



AGENDA

March 18, 2014

Town Hall BOS Meeting Room - 6:30 PM

MINUTES:

1. Approval of minutes from the 1/7/14, 2/18/14 and 2/25/14 meetings.

OLD BUSINESS:

2. **Laurie Shiffer/Classic Bay Farm – Ponemah Hill Rd – Map 54, Lot 13-2;** Major site plan to construct an indoor equestrian riding arena with attached stalls and associated site improvements.
(Tabled from 2/25/14)
3. **John Samonas/TMC CF of New England LLC – Nashua St – Map 44, Lot 11;** Major site plan for a proposed retail convenience store and gas sales with associated site improvements.
(Tabled from 2/25/14)
4. **Carol Colburn – Osgood Rd & Woodhawk Dr – Map 51, Lot 1;** Major open space subdivision creating twenty-seven (27) new residential lots.
(Tabled from 2/25/14 meeting)

NEW BUSINESS:

5. **Dorothy Lorden Family Trust & Frederick Lorden Rev Trust/Great Bridge Properties – Capron Rd & Nashua St – Map 43, Lots 55 & 57;** Public Hearing for a major site plan to construct a three (3) building apartment complex with associated site improvements and; a waiver request from the Milford Development Regulations, Section 6.05.1:D, *Off street parking*.
(Tabled from 2/25/14)

OTHER BUSINESS:

Future meetings:

03/25/14 Worksession
04/01/14 Worksession
04/08/14 Worksession
04/15/14 Regular Meeting
04/22/14 Worksession

The order and matters of this meeting are subject to change without further notice.

**Town of Milford
Planning Board Minutes
February 18, 2014**

Electronic Vote: Janet Langdell, Chairperson
Paul Amato
Kathy Bauer
Chris Beer
Steve Duncanson
Judy Plant
Susan Robinson, Alternate

Secretary: Jodie Levandowski

Motion to Approve: _____

Seconded: _____

Signed: _____

Date: _____

Due to inclement weather and anticipated road conditions and the fact that some applicants and applicant representatives are coming from a distance it was determined to kept everyone safe it was in the best interest to postpone the February 18, 2014 Planning Board meeting.

Janet Langdell, Chairperson, called for an electronic vote by email dated 2/18/14.

J. Langdell made a motion to postpone the items listed on the 2/18/14 MPB agenda to 2/15.

S. Robinson seconded.

Final vote (by attached emails):

S. Robinson – yes; C. Beer – yes; S. Duncanson– yes; J. Plant– yes; K. Bauer – yes; J. Langdell – yes; P. Amato - No.

Majority voting in favor of postponing tonight's meeting to Tues 2/25 with one No vote.

1 MILFORD PLANNING BOARD PUBLIC HEARING ~ DRAFT ~
2 February 25, 2014 Board of Selectmen's Meeting Room, 6:30 PM

3
4 Present:

5 **Members:**

6 Janet Langdell, Chairperson

7 Kathy Bauer

8 Chris Beer

9 Steve Duncanson

10 Judy Plant

11 Tom Sloan, Vice Chairman

12 Susan Robinson, Alternate member

Staff:

Jodie Levandowski, Town Planner

Bill Parker, Community Development Director

Shirley Wilson, Recording Secretary

Nick Giakus, Videographer

Excused:

P. Amato

13
14
15 **MINUTES:**

16 1. Approval of minutes from the 1/7/14 and 1/21/14 meetings.

17
18 **NEW BUSINESS:**

19 2. **Thomas Lorden – Off Crestwood Ln – Map 49, Lot 2;** Review and recommendation for a parcel
20 without frontage on a Class V road or better.

21 (*Ref: ZBA case #2014-02*)

22
23 3. **Share Outreach, Inc – Columbus Ave – Map 25, Lot 126;** Public Hearing for a site plan amendment
24 to construct a 2,300SF addition with associated site improvements and; waiver requests from the Milford
25 Development Regulations, Section 5.04.KK, *Landscaping Plan* and Section 5.04.LL, *Stormwater*
26 *Management Plan*.

27
28 4. **Laurie Shiffer/Classic Bay Farm – Ponemah Hill Rd – Map 54, Lot 13-2;** Public Hearing for a major site
29 plan to construct an indoor equestrian riding arena with attached stalls and associated site improvements.

30 (*Fieldstone Land Consultants PLLC*)

31
32 5. **John Samonas/TMC CF of New England LLC – Nashua St – Map 44, Lot 11;** Public Hearing
33 for a major site plan for a proposed retail convenience store and gas sales with associated site
34 improvements.

35 (*MHF Design Consultants*)

36
37 6. **Dorothy Lorden Family Trust & Frederick Lorden Rev Trust/Great Bridge Properties –**
38 **Capron Rd & Nashua St – Map 43, Lots 55 & 57;** Public Hearing for a major site plan to construct a
39 three (3) building apartment complex with associated site improvements and; a waiver request from the
40 Milford Development Regulations, Section 6.05.1:D, *Off street parking*.

41 (*Keach Nordstrom Associates*)

42
43 7. **St. Joseph Hospital et.al./ Milford Medical Center – Nashua St – Map 31, Lots 32 & 32-1 and Map 32,**
44 **Lot 1;** Public Hearing for a lot line adjustment involving three (3) lots.

45 (*Meridian Land Services*)

46
47 **OLD BUSINESS:**

48 8. **St. Joseph Hospital et.al./ Milford Medical Center – Nashua St – Map 31, Lots 32 & 32-1 and Map 32,**
49 **Lot 1;** Major site plan to construct a new medical facility with associated site improvements and; respective
50 waiver requests from the Milford Zoning Ordinance, Article VI, Section 6.05.0; *Nashua and Elm Street*
51 *Corridor District*, in accordance with the Milford Development Regulations, Section 5.020.

52 (*Tabled from 1/21/14 meeting*)

53
54 9. **Carol Colburn – Osgood Rd & Woodhawk Dr – Map 51, Lot 1;** Major open space subdivision creating
55 twenty-seven (27) new residential lots.

56 (*Tabled from 1/21/14 meeting*)

57
58

59 Chairperson Langdell called the meeting to order at 6:35PM noting that this meeting is a result of the
60 postponement of the 2/18/14 meeting due to inclement weather. She then explained the ground rules for the
61 public hearing, introduced the Board and Staff, and read the agenda into the record.
62

63 **MINUTES:**

64 S. Duncanson made a motion to approve the minutes from the 1/21/14 meeting. K. Bauer seconded. C. Beer
65 abstained and all else voted in favor. S. Duncanson made a motion to table approval of the minutes from the
66 1/7/14 meeting. C. Beer seconded and all else voted in favor.
67

68 **NEW BUSINESS:**

69 **Dorothy Lorden Family Trust & Frederick Lorden Rev Trust/Great Bridge Properties – Capron Rd &**
70 **Nashua St – Map 43, Lots 55 & 57;** Public Hearing for a major site plan to construct a three (3) building
71 apartment complex with associated site improvements and; a waiver request from the Milford Development
72 Regulations, Section 6.05.1:D, *Off street parking*.
73

74 Chairperson Langdell read correspondence from Anthony Basso, Keach Nordstrom, Inc. dated 2/18/14.
75

76 S. Duncanson made a motion to table the application to the March 18, 2014 meeting per the applicants' request.
77 C. Beer seconded and all in favor.
78

79 **Carol Colburn – Osgood Rd & Woodhawk Dr – Map 51, Lot 1;** Major open space subdivision creating
80 twenty-seven (27) new residential lots.
81

82 Chairperson Langdell read correspondence from Randy Haight on behalf of Carole Colburn, dated 2/25/14.
83

84 C. Beer made a motion to table the application to the 3/18/14 meeting. S. Duncanson seconded and all in favor.
85

86 **Thomas Lorden – Off Crestwood Ln – Map 49, Lot 2;** Review and recommendation for a parcel without
87 frontage on a Class V road or better
88 *Abutters were not notified, but Thomas Quinn was present.*
89

90 Chairperson Langdell recognized:
91 Wil Sullivan, Cheever & Sullivan, PA
92

93 W. Sullivan stated that the applicant is scheduled to meet with the Board of Selectmen on 3/10/14 and we are
94 asking the Planning Board to review and comment our request, pertaining to ZBA Case 2014-02 to build a house
95 on a lot without the required frontage. He distributed a plan showing the approximate location of the proposed
96 building and said it's an odd statute because there are no driveway limitations in town and this is not uncommon,
97 he's been doing these for decades. There are numerous driveways that are much longer than this one. This
98 easement is about 50ft in length going from Crestwood Dr, a town road, to the building envelope and emergency
99 services will have full access. J. Langdell referenced the staff memo from Bill Parker dated 2/18/14.
100

101 S. Duncanson referenced the statute and town counsel's comments and said he was not sure how we can approve
102 this. We will be setting precedence for every back lot in town without frontage on a road. W. Sullivan read
103 section 674:41.1(d) and stated that if you couldn't build then it would be a taking. J. Langdell clarified that
104 although the statute may not be very clear, this is allowed, with the process of review and there are a number of
105 cases in Town that have been approved by the ZBA. It just recently came to light from Attorneys Sullivan and
106 Quinn that our process was missing this step. B. Parker added that any owner of a land locked property would be
107 allowed to do this and would have to go through this same process; ZBA, Planning Board and the Board of
108 Selectmen. The first step is always the ZBA and Mr. Lorden has received their approval, actually twice. S.
109 Duncanson then inquired about the easement and if it would get lost with future land sales or possible future
110 subdivision of the fifteen (15) acre lot. J. Langdell said that any future subdivision or change in use would have
111 to come before the Planning Board and there are many other pieces that would come into play. W. Sullivan stated
112 that the easement has already been recorded. C. Beer read from the recorded driveway easement document and

113 verified that the driveway is bound to the property, not to the individual. K. Bauer added that the ZBA granted
114 the variance for a single family residence.

115
116 T. Sloan inquired about the topography of the property and asked if the driveway grade would be greater than 6%.
117 R. Reindeau said the driveway apron has been constructed to Town standards. He only inspects 15-20 ft from the
118 street and does not know what the grade further in is, but didn't really see any issues with topography. W.
119 Sullivan stated that the applicant wouldn't put money into a driveway they couldn't build. T. Sloan suggested
120 that we could indicate to the BOS that the house be located so that the driveway not have greater than a 6% slope.
121 J. Langdell said it would have to be constructed to all the regulations and specifications we have and within all
122 our existing processes. J. Levandowski added that the Building Department and DPW coordinate with each other
123 when it comes to issuance of building permits and all departments' requirements need to be met before a C/O is
124 issued.

125
126 C. Beer made a motion that this Board has reviewed this request and there are no conditions that would preclude
127 issuance of a building permit. J. Plant seconded. T. Sloan abstained as he was not present for the entire
128 presentation and all else voted in favor. The Chairperson will craft correspondence for the Board of Selectmen
129 stating that the Planning Board has reviewed the request and has no concerns relative to issuing a building permit
130 on this property.

131
132 **Share Outreach, Inc. – Columbus Ave – Map 25, Lot 126;** Public Hearing for a site plan amendment to
133 construct a 2,300SF addition with associated site improvements and; waiver requests from the Milford
134 Development Regulations, Section 5.04.KK, *Landscaping* and Section 5.04.LL, *Stormwater Management*.
135 *No abutters were present.*

136
137 Chairperson Langdell recused herself.

138
139 Vice Chairman Sloan recognized:
140 Bob Moulton, Board of Directors, Share Outreach, Inc.
141 Christine Janson, Executive Director, Share Outreach, Inc.
142 Cynthia Dokmo, Chairperson of Board of Directors for Share Outreach, Inc.

143
144 T. Sloan read the notice and background into the record. J. Levandowski stated that the application was complete.
145 C. Beer made a motion to accept the application. S. Duncanson seconded and all in favor. S. Wilson read the
146 abutters list into the record. C. Beer made a motion that this application did not pose potential regional impact. S.
147 Duncanson seconded and all in favor.

148
149 J. Levandowski clarified that the background pertaining the Elm Street Corridor District in the staff memo was
150 based on the initial review of the plans and upon further review with staff, it was determined that this is an
151 existing non-compliant site and the addition will be over existing impervious area. She then referenced the memo
152 from Bill Parker dated 2/19/14.

153
154 B. Moulton presented plans dated 1/27/14 and gave an overview of the project. The primary reason for this
155 addition is because we are running out of space. We've been in the building for five years and we've seen a
156 significant increase in the amount of clients we serve each month. We need additional office space and would
157 like to increase the size of our food pantry.

158
159 T. Sloan inquired if the site met the 30% requirements. B. Moulton replied that he is not sure if that gravel area is
160 considered pervious or impervious, but it's dirt now and he can check on the calculations. K. Bauer asked what if
161 it doesn't meet the requirements; can that be a condition of approval? J. Levandowski added even if the gravel
162 parking area is removed from the calculations, the overall open space on the site will not be changing in regards to
163 the addition. It will be constructed over impervious area so the open space will not be decreasing. It is a pre-
164 existing site and these conditions already exist. T. Sloan asked if the addition would be beautified with any
165 foundation plantings. B. Moulton said you can't really see the site from the road but that's certainly something
166 we could look at; however, no determination has been made yet.

167 B. Moulton reviewed the waiver requests and stated that there will be no change to the landscaping or stormwater
168 runoff with this addition. It is a straightforward request.

169 Vice Chair Sloan opened the hearing to the public for comment.
170

171 C. Janson said we have been in that building for five years and the Knights of Columbus hall was not designed for
172 what we do there. Currently, our clients come from five communities and about 85% are from Milford. We don't
173 have confidential space for clients so we're trying creating more appropriate office space for the work that we do.
174 We will also be expanding the pantry as our client base is expanding and we would like to serve them better. This
175 is the most cost effective way to do that and this site will actually look better with the addition. We appreciate the
176 Board considering this.

177
178 The public portion of the meeting was closed
179

180 S. Robinson agreed with the idea of plantings which will also benefit the clients. K. Bauer also agreed and said
181 although it is not essential, floral plantings would be a good idea. Maybe funds could be raised for that.
182

183 S. Duncanson made a motion to grant a waiver from Section 5.04.KK of the Milford Development Regulations.
184 C. Beer seconded for discussion. J. Levandowski noted that the applicant can still work with staff to beautify the
185 site even with the waiver request granted. T. Sloan said the applicant seemed open to the consideration to do
186 some floral plantings or some type of landscaping improvements and the improvement to the site with the
187 addition would outweigh any requirements for landscaping. B. Moulton added that a couple of bushes may have
188 to be moved or replaced as a result of the addition but will that will be done. A vote was called and all in favor.
189

190 K. Bauer read the letter from Conservation Commission dated 2/24/14. T. Sloan asked if there would be any
191 repaving on the site. B. Moulton replied not at this time. T. Sloan said the Conservation Commission comments
192 have just alluded to the gravel area being permeable.
193

194 C. Beer inquired if a waiver from the stormwater requirements was needed if this site was under the threshold per
195 comments from the Environmental Coordinator and discussion regarding drainage on the existing site followed.
196

197 S. Duncanson made a motion that a waiver from Milford Development Regulations, Section 5.04.LL Stormwater
198 would not be required. K. Bauer seconded and all in favor.
199

200 S. Duncanson made a motion to approve the application. C. Beer seconded and all in favor.
201

202 **Laurie Shiffer/Classic Bay Farm – Ponemah Hill Rd – Map 54, Lot 13-2;** Public Hearing for a major site plan
203 to construct an indoor equestrian riding arena with attached stalls and associated site improvements.

204 *Abutters present:*

205 *Mark Johansen, Ponemah Hill Rd-Milford*

206 *John & Carol Hopfenspirger, Ponemah Hill Rd-Amherst*
207

208 Chairperson Langdell recognized:

209 Chris Guida, Fieldstone Land Consultants, PLLC

210 Laurie Shiffer, Classic Bay Farms

211 John Griffin, Jr. PLLC, Griffin Law Offices
212

213 J. Langdell read the notice into the record and stated that the application was complete per the staff memo. S.
214 Duncanson made a motion to accept the application. C. Beer seconded and all in favor. C. Beer made a motion
215 that this application did not pose potential regional impact. S. Duncanson seconded for discussion and said he
216 believed there was regional impact as it borders Amherst and the shared driveway is in partly in Amherst. J.
217 Levandowski read RSA 36:55. C. Beer and S. Duncanson withdrew their motions. S. Duncanson made a motion
218 that this application did pose potential regional impact to Amherst. C. Beer seconded. K. Bauer, S. Robinson, T.
219 Sloan, J. Plant, C. Beer and S. Duncanson voted in the affirmative. J. Langdell voted in the negative. The motion
220 carried by a vote of 6-1. S. Wilson read the abutters list into the record.
221

222 C. Guida presented plans dated 2/21/14 and described the property and proposed project. We are proposing to
223 raze the existing barn and replace it with a new barn so that the stables, tack room, riding ring and office will all
224 be in one location. Ms. Shiffer has lived there for twenty years and has owned horses the whole time, so the use
225 is staying the same. L. Shiffer explained that she houses anywhere between 8-14 horses but will always have
226 eight. she has ten horses now and would like to keep it at that. C. Guida stated that the proposed indoor riding
227 arena is an acceptable and permitted use in the zone. All construction will be done in the existing open field with
228 a few minor modifications to the site. There will be no land clearing and not much change to the site except that
229 the new building will be larger.

230
231 C. Guida reviewed the comments from the staff memo dated 2/25/14.
232 – We've met with the Fire Department and this plan was revised, based on Captain Smedick's requirements to
233 show the required access to three sides of the building; he had no other comments on this proposal. The 20ft
234 wide accesses will be 25ft away from the building and the materials will support the weight of fire apparatus.
235 J. Levandowski added that Captain Smedick would like to review the revised plan.
236 – Fred Elkind has made some comments regarding the submitted a stormwater management plan. We've made
237 a few minor modifications and are working with him to finalize the remaining details.
238 – A note has been added that all lighting will be downcast which will be localized wall mounted sconces.
239 – A note has been added that Ponemah Hill Rd is a scenic road.
240 – A note has been added that no manure will be stored on site. The manure goes into a trailer parked in the barn
241 and is trucked off site and composted.
242 – The snow storage area was shown.
243 – A detention basin was created to prevent any additional runoff and there will be no increase from what is
244 there now.
245 – No other impervious areas or disturbance is proposed.
246 – There will be no impact to buffer or the wetlands.
247 – No waivers are being requested.
248 – The comments from Annmarie Pinal Turcotte pertaining to traffic, parking and aesthetics have been
249 addressed. We have added a note to the plan that there will be no parking on the common driveway. Ms.
250 Shiffer has specifically chosen a building that is in keeping with aesthetics of a rural residential area. It was
251 really important to go with a wood frame, the asphalt roofing and siding to keep in character with the
252 neighborhood and it would be a great asset to property.

253
254 J. Langdell inquired about the parking. C. Guida said there is very little need for a large parking area. Three (3)
255 spaces would be more than adequate. If there were a need for more, she could use the area near the old barn as
256 most of the corrals will be torn down and reconfigured outside the new stalls. L. Shiffer described the current
257 conditions and stated that the wood fencing will all be coming down. The perimeter of the property will be
258 fenced in using triple-crown fencing and the paddocks will be reconfigured to again allow for in/out access for
259 fire purposes. The fields will remain for the horses. K. Bauer said she was not comfortable with a site plan that
260 only shows three (3) spaces. J. Shiffer then described the current operations. I am the trainer, instructor, coach
261 and the property caretaker and she parks at her house. There is one person who helps out and she will park by the
262 new building. There isn't a need for delineated parking and I'm trying to keep this rural. I teach private (1-2
263 person) classes on Monday, Wednesday and Friday so there may be an overlap of one or two clients, but I can put
264 up a sign for parking. I professionally compete throughout the east coast, so most of the horses are mine and I
265 teach using lesson horses. The whole purpose for doing just ten (10) stalls is to limit the future use. With ten (10)
266 horses, you only have the potential of ten (10) lessons per day. To have a big business, you have to have a lot of
267 horses.

268
269 S. Duncanson inquired if there would be events on the property with a 10,000SF riding arena and where would
270 multiple horse trailers park. J. Shiffer said she had no intention of holding events. S. Duncanson said there could
271 be with the next owner. J. Levandowski said one of the benefits of having a site plan for a facility such as this is
272 that a specific note could be added to the plan that would reference the equestrian use of the site and limit it to the
273 current use, prohibit assembly or require future uses come back for approval. L. Shiffer said that the state statute
274 for agriculture doesn't call out horse shows. She then explained the existing trailer parking. J. Langdell clarified
275 that this Board is just trying to determine there is sufficient room for what is being proposed.

276 J. Griffin, attorney for the applicant, stated that there will be no change in use; it is a continuation of what she's
277 currently doing and she has no intention of having events on the property. Perhaps a limitation on the number of
278 horses or trailers might be a good way to conditionally allow the use and retain some control. L. Shiffer said five
279 (5) trailers would be a reasonable number to be able to evacuate the property due to fire or to trailer her horses to
280 a show.

281
282 K. Bauer brought up the Building comments from the staff memo. J. Langdell stated that there has been much
283 discussion in the office on this matter. J. Levandowski explained that the application was originally submitted for
284 a private facility and all comments were based on a private use building. It has since been brought to our
285 attention that there will be public entering the building, so these comments no longer apply. It is an agricultural
286 use but different building codes will come in play. The referenced sixty (60') ft access pertained to strict
287 requirements for agricultural exemptions for private use relative to the building code. This site plan is compliant
288 for a commercial use. J. Griffin explained that this is not classified as a commercial use, but it is a commercial
289 component of an agricultural use allowed by the Zoning Ordinance and the State RSA's promoting agricultural
290 use. We will work with Code Enforcement to address all construction issues as part of the building permit
291 process. J. Langdell said to approve this site plan, the Board has to know if the 60ft access is needed or not. C.
292 Guida added that the architect is currently getting a written determination from the ICC Code Officials and
293 explained the 60ft floor to sky requirement is a fire protection issue so that another structure doesn't catch fire.
294 With that being said, we don't have anything within 60ft of this building. If it were to ever be an issue, we could
295 cut some trees and create a sixty (60') ft area. J. Levandowski ended a brief discussion by saying that any
296 changes to the site plan would be submitted at the next meeting.

297
298 Chairperson Langdell opened the public hearing.

299
300 J. Hopfenspirger said he has been here equally as long as Laurie, for twenty years. We have a great relationship
301 and everything has been good. I've been happy with the way things have been run over there and she does a nice
302 job. This is a very well done building; however, this is impacting us quite a bit. It is a 14,000SF building right
303 outside our back door which is a concern and compounded with what he is hearing now. He is confused with all
304 the terms being thrown around; residential, agricultural, and commercial. This is a residential area, period and I
305 have a problem with commercial. I know what Ms. Shiffer does and I am fine with that, if it's commercial I am
306 concerned about the next owner. This has to be resolved before you can say yes to approve this. Also, is there a
307 landscaping provision? This building is 153 ft long and it will be right where I used to look out onto a beautiful
308 pastoral area. I planted some trees that will work great in the summer, but could there be some evergreen
309 plantings like hemlocks or arborvitaes that would grow to give some additional buffer. This is an appealing
310 looking building and they've done a tasteful job but it will be 153ft long, so there has to be some type of provision
311 for us as abutters to have a buffer plan that helps mitigate or reduce the impact. J. Levandowski reiterated that use
312 of the site has been and will be classified as agricultural or farming. Only the building construction will be
313 classified as commercial because the commercial building code is triggered when there is public entering a
314 building. J. Hopfenspirger said commercial makes him nuts and reiterated his concerned about the next owner.
315 He spoke with Laurie and she said nothing's changing, but things are changing and it will be a huge change. S.
316 Duncanson referred to page 5 of the staff memo and said he can see a need for buffer along the front edge.

317
318 M. Johanson brought up the 60ft buffer around the building and said that if you cut the trees for that buffer, it
319 clearly opens our property to the back of this building. S. Duncanson said there was a lot of land, at least 250 ft,
320 between the abutter and the building so the 60 ft buffer shouldn't have an impact. J. Johanson said it will.

321
322 Chairperson Langdell closed the public portion of the meeting.

323
324 J. Langdell read correspondence from an abutter, Annmarie Pinal Turcotte dated 2/15/14 and the memo from
325 Conservation Commission dated 2/24/14. Since this application is coming back for regional impact, it will allow
326 time for the applicant to work with staff to get a final stormwater report, address the landscaping buffering along
327 the roadway and get resolution to the 60 ft buffer. J. Griffin said we will come back next month with a
328 landscaping/buffer plan, we will address all the abutters' concerns, we will come back with some parameters for
329 wording that will ensure future use for the Board to consider as a possible condition and we will also add notes to

330 reflect the Conservation Commission's recommendations. The contractor also wanted me to point out that the
331 one-story/two-story requirements are still subject to debate and we will have an answer to that as well.
332

333 T. Sloan inquired if there would be sanitary facilities on site. C. Guida said a bathroom is proposed in the small
334 office area next to the tack room. We also have subsurface approval from the State. T. Sloan inquired about the
335 horse washing water. C. Guida said we will most likely put that into the system as well. It is designed for 300
336 gallons per day and there will be a Zabel filter system. L. Shiffer added that she is on a well so there is very strict
337 water usage. T. Sloan said the wash water might be able to go into a treatment swale. S. Duncanson said it was
338 considered gray water and it could. T. Sloan inquired if the trees that were mentioned for cutting would be part of
339 the 15% basal area of the wetland and buffer. C. Guida described the area in said he would review and clarify. T.
340 Sloan also suggested that the placement of the proposed trees take the location of the horses into consideration.
341

342 C. Beer said he would prefer the applicant not move forward with a restriction that would limit the number of
343 trailers because when we have a birthday party at our house, we have more than five vehicles. J. Langdell
344 clarified that the limitation was to ensure enough parking and they will come back with some language options.
345

346 K. Bauer referenced the RSA and asked if there was a restriction on the amount of riding instruction. J.
347 Levandowski replied no. T. Sloan said it is up to the Planning Board to analyze and determine what is reasonable
348 and to somehow constrain the use. K. Bauer said the building is attractive but it is very large. T. Sloan reiterated
349 that boarding, riding lessons, and riding arena are defined as agricultural uses and agricultural use is permitted in
350 the zone. The building has to be constructed to the International Building Code according to the use of the
351 structure but it doesn't define our analysis. S. Duncanson said the commercial wording shouldn't have been used.
352 It is a matter of how the building code is interpreted. This will still be an agricultural building but with public
353 usage; it's not a commercial building. L. Shiffer explained the riding operations and said you can't safely have
354 ten (10) horses in that arena at the same time, and her insurance would not allow that. Although it sounds large, a
355 150' x 75' is the smallest indoor building she could construct to accommodate three (3) horses being ridden at the
356 same time. I went this route for my neighbors.
357

358 T. Sloan made a motion to table the application to the 3/18/14 meeting to allow for the regional impact process
359 and for the applicant to come back with a final stormwater report, a landscaping plan and the items discussed. S.
360 Duncanson seconded and all in favor.
361

362 **John Samonas/TMC CF of New England LLC – Nashua St – Map 44, Lot 11; Public Hearing for a major site**
363 **plan for a proposed retail convenience store and gas sales with associated site improvements.**
364 *No abutters were present.*
365

366 Chairperson Langdell recognized:
367 A.J. Barbato, T. M. Crowley & Associates, Inc.
368 Garrett Wood, T. M. Crowley & Associates, Inc.
369 Jason Plourde, Tighe & Bond
370 Chris Tymula, MHF Design Consultants, Inc.
371 John Smolak, Smolak & Vaughn, LLP
372

373 J. Langdell read the notice into the record and stated that the application was complete per the staff memo. C.
374 Beer made a motion to accept the application. T. Sloan seconded. S. Duncanson abstained and all else in favor.
375 T. Sloan made a motion that this application did not pose potential regional impact. C. Beer seconded S.
376 Duncanson abstained and all else in favor. S. Wilson read the abutters list into the record.
377

378 C. Tymula presented the site plan set dated 1/20/14 and gave a brief history of the site. The proposal is for a
379 4,513 SF convenience store with a gas canopy in front, housing four (4) dispenser islands and eight (8) pump
380 stations. Two new 20,000 gallon underground fuel tanks will store 40,000 gallons of diesel, premium and regular
381 fuel on site. We are providing a ten (10') ft sidewalk along the front of the building, a proposed air tower and the
382 trash will be enclosed in back. We will be increasing the green space throughout the site and actually removing
383 1,500 SF of pavement currently within the wetland buffer. There will be two full access points on the side
384 driveway and a right in and right out on 101A. There will be granite curbing and pedestrian sidewalk access to

385 the site. There will be twenty-four (24) parking spaces and one (1) ADA compliant space in front of the building.
386 We will remove the existing infrastructure on the pad-ready site but will try to reuse or relocate the utilities and
387 the pavement removal will result in an 800 SF reduction of impervious surface. We really tried to balance the site
388 from a grading and drainage perspective. He reviewed the closed drainage system that eventually discharges out
389 to the back of the site. The canopy and roof runoff will be discharged into an underground system that will go
390 into the infiltration system. All the catch basins will be designed with oil hoods.

391
392 C. Tymula reviewed the erosion control measures and staff recommendations. We have no issue replacing the silt
393 fence with hay wattles. The utility and landscaping plans were reviewed. The monument sign shown is lower
394 than originally proposed and the sign package will go through the permit process. We will not be modifying the
395 master box or transformer pad that is on site currently. The lighting plan is fully shielded and dark sky compliant.
396 All mechanicals will be screened behind the building. A photo simulation based on the Leominster site, showing
397 the canopy and building, and materials board was presented. The architectural depict a very New England style
398 building with faux dormers, columns and cultured stone around the base. There will also be a seasonal patio area,
399 a nice amenity for customers.

400
401 **Staff comments**

402 C. Tymula stated that rain gardens are not a recommended stormwater BMP for this type of use. It introduces the
403 potential for any type of spill to go right into the groundwater system. We prefer to leave the existing closed
404 system as is. There is a water quality unit that will maintain and manage the system. We have also provided a
405 comprehensive long term operation maintenance plan and manual. We have no issue with adding a row of
406 medium growth plantings per Conservation's comments.

407
408 K. Bauer brought up the MFD comments and said that the Fire Department takes the width requirements seriously
409 because with snow, 15ft could possibly be reduced to 12ft. Is this a fire requirement and can they be made 20 ft
410 as requested. C. Tymula said he didn't think there was an issue with the width; we met with DPW and
411 Community Development staff and the layout was ok'd. We did run a truck turning template for a tanker delivery
412 truck and fifteen (15') ft was more than enough room so there would also be enough room for fire apparatus. J.
413 Levandowski stated that this plan meets our Town standards and she will inquire about the 20 ft width
414 requirement. J. Plourde read the Town of Milford Driveway Regulations and said that the minimum width for all
415 driveways shall be 10 feet. R. Riendeau said he was not sure where the 20 ft requirement came about, but these
416 accesses are at an angle and fifteen (15') ft is better. J. Langdell stated we will need to get clarification from the
417 Fire Department.

418
419 **Traffic**

420 J. Plourde said this site was already reviewed and approved for a 99 Restaurant and the mitigation measures
421 associated with that development were implemented. A lot of coordination took place with the DOT because the
422 other signals to the east of this intersection, at the Stop & Shop and 101 entrance ramps, are controlled by the
423 State. This signal is under local jurisdiction, so there was a lot of coordination on the original development, and I
424 did work on that project for the permitting of that development. From a traffic engineering standpoint, we look at
425 the land use, not the tenant and this site was looked at as a high turnover sit down restaurant; a 99 Restaurant or a
426 Denny's. We looked at weekday morning and afternoon as well as Saturday midday trip generated traffic. For
427 the Cumberland Farms project, we met with staff early on to see what the Town would be looking for as far as
428 evaluation of multi-modal transportation there. We came up with an understanding and put together a trip
429 generation safety assessment. We looked at and evaluated at crash data from the Milford Police Department to
430 determine if there was a safety problem today and if so, would this exacerbate that condition. There were
431 approximately three (3) collisions per year at the intersection. Although some may have taken place inside
432 Shaw's or Walgreens' parking lots, all have been associated and grouped with the intersection. A general rule is
433 that five (5) or more per year are indicative of a safety problem and we are below that. There does not appear to
434 be a safety concern here. We follow three standards for safe sight distance; the national standard AASHTO with
435 requirements based on speed limits for stopping sight distance and intersection sight distance, the NH DOT all
436 season safe sight distance which requires 400ft of safe sight lines under all conditions, and Milford's safe sight
437 distance requirement which is 300ft. We meet all three sight distance standards. When we previously presented
438 the conceptual plan to the Board, one of the primary concerns was the proximity of the right turn in driveway to
439 the McDonald's driveway and an eastbound traveler wouldn't know which driveway to turn into. As a result, the

440 right turn in drive was moved further to the west so there wouldn't be that type of conflict. For separation we
441 have provided; 80ft from the signal to the right in driveway, 80ft between our right in and right out driveways and
442 60ft between our right out driveway and the McDonalds right in driveway. All those are in conformance with
443 Milford's 50ft spacing requirements. It is important to have a right turn exit out onto Nashua St for fuel truck
444 access. The right turn in is also important so that people will be able to find their way onto the site and he
445 referenced the Dunkin Donuts on 101A in Amherst where drivers, who are unfamiliar with the area, miss the
446 entrance because they missed the opportunity to turn in at the signaled intersection and try to go into the exit only
447 drive. There is a difference in speed limit between that example and here where it is posted at 30 mph, but we
448 prefer to take care of any safety issues during the design process. We also met with Bill Parker and Rick
449 Riendeau and NH DOT to get their input and address any concerns before we went ahead with the design.

450
451 C. Beer inquired why there was a separation for the drives and could you move the right in to meet the right out
452 and have them both at the same location. J. Plourde replied that we originally had that layout on the conceptual
453 plan but it was closer to the McDonald's property and reiterated the Board's concerns about confusion with
454 entrances. The primary reason for a right in driveway is a matter of convenience for customers and trip
455 generation from the standard database for types of trips being made. J. Langdell said Walgreens customers
456 without entrance or exit on Nashua St don't seem to have a problem. J. Plourde replied that they are different
457 uses with different generating characteristics. While Cumberland Farms will generate more traffic than
458 Walgreens, it will be from pass-by trips, or cars already in the roadway system. Walgreens traffic is generated
459 from new car trips, vehicles specifically going to Walgreens, rather than pass-by trips. Convenience stores draw
460 60% of their traffic from vehicles already in the roadway system. It's also a difference of location because if
461 Walgreens wanted to put in a driveway they would have to break open the curb where existing turn lanes are
462 situated. C. Beer asked if the intersection, as designed, can support the additional traffic from Cumberland
463 Farms? J. Plourde replied yes, with the proposed curb cuts on Nashua St. The intersection does not function
464 correctly today because there is a problem with the signal due to a faulty video camera card. Without the
465 proposed right turn out, the intersection would need more green time to process the cars and steal it from the
466 Nashua St side. The right turn in would not impact the signal operations but it would from a corridor convenience
467 operations perspective. We don't want to create any safety concerns or confusion for drivers.

468
469 J. Langdell said it would be interesting to see data from Amherst of how many times people make that
470 inappropriate turn going into the Dunkin Donuts exit. It would also have been better if you could have come in
471 sooner and McDonalds come in later so that we could have jointly coordinated this improvement to gain better
472 access management for the whole area. That said, it can't be done now. J. Plourde said Cumberland Farms is
473 very comparable to the 99 Restaurant in regards to traffic impacts to the roadway system and the traffic signal
474 operation.

475
476 S. Duncanson expressed concern with the measurements from the right out of Cumberland Farms to the right in of
477 the McDonalds driveway. He felt that was too tight because this is a complete radius and is one driveway leading
478 into another. My concern is with cars exiting Cumberland Farms and not seeing a car entering McDonalds. I
479 regularly see how crazy the Leominster Cumberland Farms gets and can see this being an issue here with drivers
480 not paying attention. J. Plourde explained the measurement on the plan.

481
482 T. Sloan added that a car intending to turn into the McDonalds drive could enter the Cumberland Farms right out
483 instead. S. Duncanson agreed saying the right in should be eliminated and the right out be moved further west.
484 J. Plourde reiterated that we did meet with Rick Riendeau, he reviewed the plans and the driveway is designed in
485 accordance with the Town's standards. J. Langdell said we have a right, based on our expertise and knowledge of
486 the community and this specific site to negotiate something different. K. Bauer agreed with Steve that heading
487 east it is too tight. J. Langdell referenced the staff comments and noted that the Building, Ambulance and Fire
488 Departments also have concern with these access points. C. Tymula said it was his understanding that the normal
489 staff roundtable session did not take place, due to all the snow, where those comments would have been taken into
490 consideration and addressed prior to the staff memo.

491
492 B. Parker added that the applicant did ask if we wanted to meet again and we said no, we didn't see a reason to
493 do so. The curb cuts exceed the Town's standards that include full vehicle movement while these are restricted to

494 one-way in and one-way out. That adds another degree of safety for both access points as well as the signal that
495 stops traffic allowing people to exit and enter.

496
497 K. Bauer asked how a customer, heading east, would navigate the site to get to the convenience store using the
498 right in from Nashua St. C. Tymula went over the plan and said there would be plenty of room to get around the
499 site without any issues from both a circulation and safety standpoint. There is 38ft from the gas dispenser bollard
500 to the curb cut; room for two car lengths.

501
502 Chairperson opened the hearing for public comment; there being none, the public portion was closed.

503
504 T. Sloan said that if the one-way ingress were eliminated, it would be less prone to accidents waiting-to-happen.
505 Another pertinent point is that this wasn't a current traffic study. J. Plourde clarified that we counted the
506 signalized intersection as well as the cars going along the shared driveway. The original traffic study was done in
507 2008. T. Sloan noted that although the speed limit is 30 mph, the speed at which vehicles travel along that
508 roadway is over 40 mph. Is it adequate to perform an analysis if you don't take the accurate speed into your
509 study? I do want a gas station there for my convenience, but my concern is that it's an overuse of the site. J.
510 Plourde said he understands the concern but to put this into perspective, the speed studies conducted along
511 Nashua St for the 99 Restaurant were done prior to the signal installation which affects the speed and flow, so all
512 conditions have been taken into consideration with this analysis. T. Sloan suggested that appropriate adjustments
513 be made to the signalization to correct the existing issue. C. Tymula said we understand the concerns of the
514 Board and also understand that the Board is not DPW and not Community Development but we felt we got the
515 guidance we needed from the Board and staff and we feel it's a safe project altogether. We've tried to do
516 everything we could to design this to make the site work for Cumberland Farms, for the Town and to alleviate
517 your concerns with traffic and safety; however, the need for this additional entry is crucial for the retail
518 component of the business and if you cut off this access point you cut off patronage. J. Langdell said there were
519 concerns clearly voiced about having a right in and right out on Nashua St when the conceptual plan was
520 originally presented to the Board, so this is not something new.

521
522 **Environmental**

523 C. Tymula explained that each of the catch basins will have an oil hood and we are not altering the existing bio
524 retention swale. Because this is a motor vehicle fueling station and it is in the groundwater protection district, it's
525 really not a good idea to infiltrate any on-site stormwater runoff; everything will go through the catch basins and
526 discharge out through the water quality inlet to be pre-treated before it reaches the wetlands. The bio retention
527 swale will take runoff from the back of the site that's not paved and any overflow from the roof runoff. Since he
528 was not involved with the original design of the bio retention swale, he does not know what volume it can handle.
529 J. Levandowski added that Fred Elkind has received and reviewed the stormwater permit application, but has not
530 seen the revisions made to date.

531
532 **Pedestrian movement**

533 J. Plourde said there has been discussion with Rick Riendeau and Bill Parker as to whether we want to cross
534 people at this location or if there is a more ideal location. A pedestrian crossing here would stop all lanes of
535 traffic and that would then impact and require improvements the State locations. J. Langdell clarified that what
536 you're saying is that there could not be a pedestrian crossing anywhere between the Stop & Shop plaza and this
537 location. J. Plourde said the Ponemah Hill Rd intersection, if signalized might be a more ideal location. J.
538 Langdell noted that there is no sidewalk on that side of the road; the existing sidewalk plan is only for the south
539 side of Nashua St going as far as Medlyn Monument. J. Plourde said ideally, it is better to cross over a shorter
540 distance through a fewer number of lanes and agrees that it is definitely a safety concern when people cross in an
541 unsignalized location.

542
543 K. Bauer said we have talked a lot about getting sidewalks along Nashua St for the reasons discussed. We also
544 have a horrendous traffic situation here anyway. I understand the needs of Cumberland Farms; however, this will
545 add to the traffic problem we have there and it sounds like no pedestrian improvements are going to be made. J.
546 Langdell clarified that there is a sidewalk in front of this site now. J. Levandowski said staff has no issue with the
547 plan other than a few revisions and notes to be added. Staff feels the applicant has made every effort to comply
548 with all town regulations and in some cases exceeds the regulations. We have no further comment.

549

Nashua St Improvements

550

C. Tymula said the driveways could be widened to meet the 20ft Fire Department request and Cumberland Farms is willing to contribute to the sidewalk fund. B. Parker spoke to past applications; Burger King did not contribute, and Walgreens, Nashua Eye and Giorgio's contributed as part of the site plan approval. Staff brought a dollar value to the Planning Board and it was made part of the conditions for approval. C. Tymula said maybe it could be a mix of some of the signal component repairs and a contribution to the sidewalk fund, totaling in the range \$5,000-\$10,000. J. Langdell said she would personally like to know what the value of that contribution and repairs would be. She is not opposed to the project or the entrance but does struggle with the Nashua St component. We could table this for that information and also get an answer from the Fire Department regarding the access width and give time for Fred Elkind to comment on the revisions.

559

560

S. Duncanson made a motion to table the application to the 3/18/14 meeting to address the concerns voiced. C. Beer seconded for discussion and said he'd rather have resolution to the access width and see if the roundtable could be scheduled and get consensus to entrance and exit configuration. I personally don't care for them, but if the Town officials are ok with them, I won't raise any objection. J. Langdell said she would like Fire and Building staff to weigh in as well. K. Bauer said we still have no answer to the crosswalk situation. J. Langdell added that she can see this as a pedestrian destination. The vote was called and all in favor.

565

566

Chairperson Langdell called for a brief recess.

567

568

St. Joseph Hospital et.al./ Milford Medical Center – Nashua St – Map 31, Lots 32 & 32-1 and Map 32, Lot 1; Public Hearing for a lot line adjustment involving three (3) lots.

569

No abutters were present:

570

571

572

- Chairperson Langdell recognized:
- Kyle Burchard, Meridian Land Services, Inc.
- Danielle Santos, Lavallee Brensinger Architects
- Bob Demers, St Joseph's Hospital
- Melissa Sears, St Joseph's Hospital
- Brad Westgate, Winer & Bennett

578

579

J. Langdell read the notice into the record and noted that the application was complete per the staff memo. C. Beer made a motion to accept the application. S. Duncanson seconded and all in favor. S. Duncanson made a motion that this application did not pose potential regional impact. C. Beer seconded and all in favor. S. Wilson read the abutters list into the record and noted that the list included all towns identified as having regional impact from the site plan application.

584

585

B. Westgate gave a brief overview of the parcels, the current conditions with the shared driveway and the ownership. This plan is to adjust three lots so that the medical center and all its parking will be entirely contained on one single parcel and the reconfigured driveway system will still be shared. The front portion of the driveway will be on town land and we have a shared access arrangement. All costs to construct and maintain the driveway up to the Kaley Park entrance will be fully borne by St. Joseph's. This application will facilitate a land exchange with the Town and the public hearings are scheduled before the Board of Selectmen for 3/10/14 and 3/24/14 with the voting to take place sometime early April. K. Burchard made the point that the site plan application is not dependent on the approval of this lot line plan. B. Westgate said the site plan contemplates an either-or scenario where the easements will change depending on the location of the driveway and parking. This plan also addresses and depicts the frontage area where there is no plan of record for the strips of land along Nashua St owned by St. Joseph's that St. Joseph's is willing to dedicate towards the Nashua Street widening.

596

597

K. Burchard presented plans dated 2/5/14 and reviewed the individual parcels to be conveyed. The previous dedications shown on prior plans were never recorded so this plan will describe that land. The primary driver for doing the parcel swap in this configuration is to avoid any conflicts with the proposed retaining wall and there is less land being exchanged than originally proposed, only 15,487 SF. The easement will grant access to all visitors of Kaley Park and the hospital in perpetuity. J. Langdell asked if the current easement for Kaley Park grants access in perpetuity now. B. Westgate said the Town's easement through the medical center was only on a

602

603 temporary basis and was never made permanent on paper. St. Joseph's has a very small permanent easement on
604 Kaley Park land and we would make all permanent without constraint. K. Burchard stated that all the easement
605 and dedications are shown on the plan. Langdell added that the permanent easements would be an improvement
606 on what we currently have. She then inquired if more than the sidewalk land was being dedicated for the
607 widening and if the sidewalk was inside of the proposed dedication strip. K. Burchard replied yes, this plan
608 shows the final ultimate location of the future sidewalk with the widening of Nashua St. When all is said and
609 done, the sidewalk running alongside the completed widening of Nashua St will all be within the right of way, for
610 the Town to maintain.

611
612 Chairperson Langdell opened the hearing to public comment; there being none, the public portion of the meeting
613 was closed. There were no other comments from the Board.

614
615 S. Duncanson made a motion that the Planning Board favorably recommend the land exchange be approved by
616 the Board of Selectmen. J. Plant seconded and all in favor. S. Duncanson made a motion to approve the
617 application, subject to the condition that the recording of the lot line adjustment plans be simultaneous with the
618 recording of the deeds that affect the land exchange and the dedicated frontage area for the potential future road
619 widening and sidewalk reconstruction. J. Plant seconded and all in favor.

620
621 **OLD BUSINESS**

622 **St. Joseph Hospital et.al./Milford Medical Center – Nashua St – Map 31, Lots 32 & 32-1 & Map 32, Lot 1;**
623 Major site plan to construct a new medical facility with associated site improvements and; respective waiver
624 requests from the Milford Zoning Ordinance, Article VI, Section 6.05.0; *Nashua and Elm Street Corridor*
625 *District*, in accordance with the Milford Development Regulations, Section 5.020. *(Tabled from 1/21/14)*

626 *No abutters were present:*

627
628 Chairperson Langdell recognized:
629 Kyle Burchard, Meridian Land Services, Inc.
630 Danielle Santos, Lavallee Brensinger Architects
631 Bob Demers, St Joseph's Hospital
632 Melissa Sears, St Joseph's Hospital
633 Brad Westgate, Winer & Bennett

634
635 J. Langdell noted that Selectmen's minutes from the 12/23/13 BOS meeting were referenced at the last meeting,
636 and after reviewing the video, it was discovered those minutes contained a misquote. They should have stated
637 that Attorney Westgate did answer a question from Chairman Daniels that at this time St. Joseph's Hospital was
638 not intending to pay for widening the road. This was detailed in correspondence from Attorney Westgate dated
639 2/4/14.

640
641 B. Westgate stated that we have been working with staff and our discussions have been detailed in the staff memo
642 dated 2/25/14.

643
644 **Architecturals**

645 D. Santos presented revised plans dated _____ that include a views from the abutter's yard on Linden St and
646 aerials as well as sample materials. She recapped the previous meetings and resulting adjustments. We have tried
647 to make the facility as compact as possible and it will pretty much be a square footage swap from what is
648 currently on the property. We did a comparison of the existing 21,000 SF building that includes the urgent care
649 center, house and barn and the new building with a 13,000 SF footprint and a 6,200 SF second floor. They will be
650 similar but each department has been better configured to serve the population. The building is broken up into
651 three parts to help break up the mass. The building height will be within requirements at 33 ½ ft based on an
652 average elevation from the base of the building. The asphalt shingles and stone will be identical to what is used at
653 the medical office building in back. The clapboard-like siding will help bring down the scale and the color will be
654 similar to give all the buildings more of a campus feel. We've revised the window arrangements. The
655 community room will be a more prominent space but still accessed through the main entrance. There will be a
656 partial 1,300 SF basement located in the back corner of the new facility that will house the mechanical room,
657 utility connections and a small office with internal and external access. There will be a solid screen vinyl fence,

658 set on a slightly higher level, to block most of the view to the MRI area and we continue to meet with the abutters
659 to refine and review any concerns. The Board reviewed all the architectural. S. Robinson noted that these
660 renderings made the building look smaller and fit more into the surrounding area.

661

662 **Waivers**

663 K. Burchard read from the 12/23/13 memo addressing the waiver requests and presented a zoning map to show
664 the property in context with the Nashua St Corridor. It is in a unique position with unique constraints that make it
665 challenging to comply with the overlay district standards. J. Langdell noted that was precisely why we put in a
666 waiver clause. K. Burchard went over the details:

667

668

668 **Transportation standards 6.05.6.B**

669 **1a. Interconnecting Drives to Adjacent Properties;** because of the residential parcels, the cemetery and the
670 wetlands it is difficult and impractical to connect a commercial property to the residential parcels and
671 cemetery. There was consensus from the Board that it was clearly not feasible here.

672

672 **1b. Limiting access points;** we comply and are not requesting a waiver.

673

673 **1c. Interior Parking Interconnection;** we are not proposing an interior parking connection and want to keep
674 separation between the physicians parking lot to the west and the visitor/patient parking to the east as well as
675 limiting access to the MRI dock area. We are keeping other interior patient parking connections. J. Langdell
676 stated that this meets the spirit of the Ordinance by connecting all the other pieces while there is clearly a
677 reason for the separation of the physicians' lot and will also help to decrease the amount traffic going onto
678 Linden St. A waiver would not be needed.

679

679 **1d. Throat Length and Stacking;** we comply.

680

680 **1e. Interconnecting Movements;** the same rationale used for interconnecting drives applies and no waiver is
681 needed.

682

682 **2a. Transit Points;** there is no master transportation plan but a waiver is needed due to the language.

683

683 **3a. Bicycle Facilities;** we don't have a route planned and feel it would be a hazard to the facilities, but we do
684 make accommodations for bicycle visitors with a bike rack at the southwest corner. There was consensus
685 from the Board to not require bicycle facilities.

686

686 **4a. Sidewalk Accessibility;** we comply and no waiver requested.

687

687 **4b. Sidewalk to roadway buffer;** it's a choice between the planted buffer or a sidewalk to roadway buffer. J.
688 Langdell said with the uniqueness and needs of this site, this design has met the spirit of the Ordinance and
689 what we're trying to accomplish.

690

690 **4c. Pedestrian Convenience;** there are sidewalks coming into the property. J. Langdell added that is a huge
691 benefit.

692

692 **4d. Pedestrian Scale;** we comply.

693

693 **4e. DPW Specs;** we comply.

694

695

695 **Site Design Standards 6.05.6.C**

696

696 **1. Natural Features;** we comply.

697

697 **2a. Parking Lots to the Rear or Side;** we do not comply due to PSNH easements and need to connect the whole
698 facility together prohibits a rear parking area. J. Langdell said there has been significant discussion and
699 details for parking since day one, so this is an area where we can grant a waiver to make it official.

700

700 **2b. Parking Prohibited Along Frontage;** same rationale.

701

701 **2c. Side Yard Parking Buffer;** same rationale.

702

702 **2d. Shared Parking Provisions;** there is a shared sidewalk between the two.

703

703 **2e. Offsite Parking;** this only applies if the land swap doesn't occur and if it doesn't occur we will still provide
704 the easements to make that happen.

705

705 **3. Build to zone;** due to the constraints we cannot do this and a waiver will be needed due to the language.

706

706 **4. Landscaping;** we comply.

707

708 S. Duncanson made a motion to grant the following waivers, from the Nashua Street Corridor Design Guidelines
709 under the Town of Milford Zoning Ordinance, Article VI, for Sections: *6.05.6:B.2a Transit Points, 6.05.6:B.2b*
710 *Transit Encouragement, 6.05.6:C.2a Parking Lots to the Rear or Side, 6.05.6:C.2b Parking Prohibited Along*
711 *Frontage, 6.05.6:C.2c Side Yard Parking Buffer, and 6.05.6:C.3 Build to Zone* with the condition that they be
712 noted on the plan. C. Beer seconded and all in favor.

713 **Board and Staff Comments**

714 Sheet SP5 shows the parking counts during the variance stages of construction. The size of the stormwater focal
715 points were increased because we monitored the ones installed at the McKelvie School in Bedford and found that
716 the manufacturer's specifications weren't enough to function properly. It is the position of the medical center and
717 the medical office building to not change anything pertaining to the addressing and keep it they are because there
718 is a lot tied to the address and there is a ripple effect. J. Levandowski said she will get clarification from Dana
719 MacAllister regarding E911 numbering. K. Burchard said the Linden St egress will remain exit only. We will get
720 the stormwater plan submittals to Fred Elkind and the State. The emergency generator will be stationed between
721 the buildings and will not be visible above the proposed six (6) ft fence. S. Duncanson asked if it meets the new
722 2014 DEP standards. K. Burchard answered he assumed so. He reviewed the site's signage on EXH-4 and noted
723 that a sign variance will be required. B. Westgate said the variance will be for the size of the sign and our goal is
724 not to exceed what is allowed if the building were on the other side of the street. There will not be any issues with
725 sight line visibility.

726
727 There was discussion staff recommendation #1. J. Langdell wanted to ensure that the review was far enough
728 along the process before she signed the final plan. B. Westgate added that staff recommendation #8 should
729 include the detailed components of the waivers, and items #11, #12, and #13 should be prefaced by "adding a note
730 to the plan" along with adding the notes.

731
732 J. Langdell said that the plans are based on ten (10) physicians and thirty-seven (37) employees; is that what is
733 currently there? K. Burchard replied yes, that is the tally today. All the available space in the medical office
734 building in the rear is being used, but not all of the front building. There are offices scattered throughout the farm
735 house and barn that are not of a consistent use but don't know what that frequency of use is. J. Langdell asked if,
736 when this building is built, there will be space for more than the current staff. K. Burchard said based on this
737 plan, no; they will be maintaining the same staff. M. Sears explained that portions of the current building are not
738 in use because they are uninhabitable, so part of the reason the new building will be the same size is because the
739 existing building doesn't meet modern day codes for room sizes and other things. It is actually undersized for the
740 current amount of patient care we deliver. To meet codes for exam room sizes and other requirements, the
741 minimum square footage to do what we're doing there happens to be fully utilized. It is a replacement facility and
742 is not built for growth. The new building will be better configured so that all the spaces will be designed and
743 constructed to deliver the specific needs of physical therapy and urgent care, instead of a converted horse barn.
744 There are actually fewer people working there from when the plans were first proposed as a result of the
745 conversion to urgent care. The second floor will not be fitted up when we open the building, it will be a
746 cold/warm shell used for storage. We have no plans for it as of yet; however, there would be no ability to expand
747 on that site once this is built, so it will be future space should we need it and reiterated that there are no plans to
748 utilize it in the near term. J. Langdell said you are creating potential for more use on site for the future which gets
749 back to all our previous discussions on traffic and traffic patterns and needs of that site going forward. M. Sears
750 added that when parts of the building became uninhabitable, we moved services offsite. There used to be an
751 orthopedic practice, a podiatry practice and a midwifery practice there, so the amount of traffic going to that
752 building is substantially less. We are seeing less patients and doing less in there than when the building was
753 totally full. If we were to something with that second floor, ten years from now, it would likely come back to
754 baseline to what it was when the building was fully utilized several years ago and it wouldn't be a net growth over
755 the highest volume of patients we've served. J. Langdell noted that the baseline was a while ago and everything
756 else around it has changed too, but we move forward.

757
758 **Nashua St widening**

759 B. Westgate said that what Melissa stated was one of the fundamental elements of our thought process that the
760 hospital not be required to pay for the widening. Historically, the hospital's position has always been that it
761 would dedicate the land, now seen on the lot line adjustment plan, to permit the widening when the Town was to
762 undertake the project. It kind of couples back to Kaley Park from the 1999 ZBA approval contemplated as a
763 condition that when the second field was fully operational, the road widening would occur and the left turn lane
764 capability would be implemented. So it would have been a Town project. Over the past week there have been
765 discussions with Rick Riendeau, Jodie and Bill as far as the timing of work. The reconfigured aprons require new
766 paving to integrate into the locust of Nashua St, so in effect, we're doing some of the paving that has to be a
767 component of the road widening and there could be a field coordination effort. If the Town's timing can work

768 with the hospital's timing to be built and open by June, 2015 then the parties can work together to do their
769 components of work in a manner that most efficiently implements the road widening. We're hopeful that makes
770 sense to the Board and that leaving it to the hands on people makes the best approach.

771
772 J. Langdell referenced the submitted draft cost breakdown for the proposed roadwork and noted that this Board
773 can strongly recommend an effort of coordination.
774

775 R. Riendeau said this is the time to do this work to save the town money; it's a plus for them and a plus for us and
776 we can do the work in budget. It will be cheaper to do this now and there other components that we can do
777 ourselves to save even more because we do have the gravel in-house. Those draft costs are based on highest cost.

778 J. Langdell said that based on the draft construction costs the Town's portion would be 62% and St. Joseph's
779 would be 38% if done now. It does show that there is participation with the widening. B. Westgate said these are
780 distinct projects but can be coordinated in the field so that there will be control over the timing.

781
782 Chairperson Langdell opened discussion to the public; there being no comments, the public portion of the meeting
783 was closed.
784

785 S. Duncanson made a motion to grant approval of the application subject to the fifteen (15) staff
786 recommendations in the staff memo dated 2/25/14 and a recommendation that there be field coordination for the
787 Nashua St widening. C. Beer seconded for discussion. There was no further discussion and all voted in favor.

788
789 The meeting was adjourned at 11:40pm.
790

791 **MINUTES OF THE FEB 25, 2014 PLANNING BOARD PUBLIC HEARING APPROVED _____, 2014**

792
793 Motion to approve: _____

794
795 Motion to second: _____

796
797 _____ Date: _____

798 Signature of the Chairperson/Vice-Chairman: _____



TOWN OF MILFORD, NH

COMMUNITY DEVELOPMENT DEPARTMENT
1 UNION SQUARE, MILFORD, NH 03055

TEL: (603)249-0620
www.milford.nh.gov

STAFF MEMO

Date: March 18, 2014
To: Town of Milford Planning Board
From: Jodie Levandowski, Town Planner
Subject: **Laurie Shiffer/Classic Bay Farm – Ponemah Hill Rd – Map 54, Lot 13-2;**
Major site plan to construct an indoor equestrian riding arena with attached stalls and associated site improvements.
(Tabled from 2/25/2014)

BACKGROUND:

The applicant is back before the Board to construct an indoor equestrian riding arena with attached stalls and associated site improvements within the Residence “R” District. In accordance with Section 5.04.1 of the Milford Zoning Ordinance, agriculture and farming are acceptable uses in the “R” District. The equestrian use is allowed by definition.

At the February 25, 2014 Planning Board Meeting the Board tabled the application until the March 18, 2014 meeting, without further abutter notification, to allow for the regional impact process and for the applicant to come back with a final stormwater report, a landscaping plan and the items discussed.

On March 10th a staff level meeting between Fire, Building, Community Development and the project team was held to discuss a building type and site layout relative to emergency access. At this time the Building and Fire Departments are awaiting a final set of building plans that may dictate a revised site layout.

REGIONAL IMPACT:

At the February 25, 2014 meeting the Planning Board made a determination of regional impact as the driveway leading to the subject property is located within the Town of Amherst. Per the state RSA 36:57 upon determination that a proposed development has a potential regional impact, the local land use board having jurisdiction shall afford the regional planning commission (Nashua Regional Planning Commission) and the affected municipalities (Amherst) the ability to provide testimony.

On March 7th the Office of Community Development received a letter from the Community Development Director of the Town of Amherst stating that the Amherst Planning Board does not believe this project has significant impact on Amherst, as long as there is a manure management plan in place to protect water resources and Stormwater which may flow into Amherst.

No regional impact response was received as of March 13, 2014 from the Nashua Regional Planning Commission. If any comments come in, Staff will let the Board know at the meeting.

STAFF RECOMMENDATIONS:

As of March 13th, staff has not received revised plans or a landscape plan as requested at the February 25th meeting. Staff recommends the Board table the application to April to allow time for the applicant to lock down a site layout and allow Staff and Department Heads to review a complete site/landscape plan and comment. At this time, Staff does not have clear information on the proposed layout of the site or landscape buffering.





Town of Amherst, New Hampshire
Office of Community Development
Building · Code Enforcement · Planning · Zoning · Economic Development
Sarah Marchant, Community Development Director

March 6, 2014

TOWN OF MILFORD RECEIVED MAR 07 2014 PB _____ ZBA _____ Office _____

Jodie Levandowski
Town Planner
Town of Milford
One Union Square
Milford, NH 03055

Re: Potential Regional Impact – Map 54, Lot 13-2 – Ponemah Hill Rd

Dear Jodie,

The Town of Amherst Planning Board reviewed the potential regional impact information for the above mentioned property. The Planning Board does not believe this project has significant impact on Amherst, as long as there is a manure management plan in place to protect water resources and Stormwater which may flow into Amherst.

Thank you for the copies of the plans and minutes. The Board appreciated that information for their review.

Respectfully,

Sarah Marchant
Community Development Director



TOWN OF MILFORD, NH

COMMUNITY DEVELOPMENT DEPARTMENT
1 UNION SQUARE, MILFORD, NH 03055

TEL: (603)249-0620
www.milford.nh.gov

STAFF MEMO

Date: March 18, 2014
To: Town of Milford Planning Board
From: Jodie Levandowski, Town Planner
Subject: **John Samonas/TMC CF of New England LLC – Nashua St – Map 44, Lot 11;**
Major site plan for a proposed retail convenience store and gas sales with associated site improvements. (*Tabled from 2/25/14*)

BACKGROUND:

The applicant is back before the Planning Board to continue their application for site improvements including the location of a new motor fuel outlet (Cumberland Farms) which includes a 4,513 SF convenience store, fuel dispensers (8 fueling positions) and an overhead canopy within the Integrated Commercial Industrial “ICI” District. In accordance with Section 5.08.1 of the Milford Zoning Ordinance, filling stations are acceptable uses in the “ICI” District.

At the February 25, 2014 Planning Board Meeting the Board tabled the application until the March 18, 2014 meeting, to allow for the applicant to address the items discussed and meet with Fire, DPW, Building, Police and Ambulance services regarding concerns with the entrance and exit configuration on Nashua Street.

The applicant has submitted a revised plan set that depicts 20 foot wide entrance and exit driveways on Nashua Street, additional traffic signage and revised stormwater control measures. The applicant has met with applicable department heads and with the additional traffic signage provided, there are no further department concerns with the proposed entrance and exit locations on Nashua Street. As noted in a memo dated March 6, 2014 from Police Chief Michael J. Viola, regarding right turns out of the shared Walgreens driveway, the Police Department will monitor the traffic related activity in this area and should there be a noticeable increase in the amount of traffic related incidents, additional enforcement and educational measure will be put in place.

(See attached correspondence from department heads, staff memo from February 18, 2014 and revised site plan)

NASHUA STREET SIDEWALK IMPROVEMENTS:

At the February 25th Planning Board meeting the applicant expressed a willingness to contribute funds for pedestrian improvements along Nashua Street. The contribution will go towards pedestrian signalization and intersection improvements at Nashua Street and existing signal, to tie in with the Nashua Street Sidewalk Project.

On March 3rd a staff level meeting between Department Heads and Cumberland's project team was held to discuss a specific dollar amount Cumberland Farm will contribute to the Nashua Street Fund. At the close of the meeting the project engineer stated that Cumberland Farms is agreeable to a contribution of \$15,000.00 to be placed in the Nashua Street Corridor Improvement Fund.

(See attached memo dated March 12, 2014 from Bill Parker, Community Development Director relative to the Nashua Street Corridor Improvements Fund)

STAFF RECOMMENDATIONS:

Staff has no issues with the plan as presented. The applicant has worked with Town Staff to address all issues and submitted additional information as requested. If the Planning Board approves the application staff recommends that the Planning Board graciously accept the proposed contribution from Cumberland Farms for the Nashua Street Improvement Fund and so noted in the Board's approval along with any recommendations the Board may have on how the contributions should be utilized. In addition, prior to final signing of the plan the following note revisions and conditions are completed:

Sheet 4- Site Plan

- Note #13 be revised to state- A sign permit shall be obtained prior to installation of all site signage.
- Note #11 be revised to state- "All water, sewer, road (including parking lot) and drainage work shall be constructed in accordance with the Town of Milford's Water Utilities Department and Public Works Department standards."
- Note be added to the plan detailing Flood Hazard information in conformance with the requirements of 6.014 SPECIAL FLOOD HAZARD AREAS.

Sheet 6- Erosion Control Plan

- Remove- Sediment Control Fence detail and replace with Hay Wattle detail.

Sheet 8- Landscaping Plan

- A note be added to the plan stating that: "The owner and their representative shall be responsible for providing, protecting and maintaining all landscaping in healthy and growing condition, and replacing it when necessary to insure continuous conformance with these guidelines. Any landscape element that dies, or is otherwise removed, shall be promptly replaced with the same, if not similar to, height or texture element as originally intended. In addition, landscaped areas shall be kept free of all debris, rubbish, weeds and tall grass."
- A note be added to the plan stating that: "An inspection of all plantings to ensure compliance with the approved landscaping plan shall be conducted prior to the issuance of a Certificate of Occupancy. Ongoing inspections of landscapes shall be conducted to ensure compliance of landscape maintenance in perpetuity."

Sheet 9- Lighting Plan

- A note be added to the plan stating that: "All outdoor lighting shall be downcast and so directed and shielded that no glare will spill out onto neighboring properties or roads."

Jodie Levandowski

From: Eric Schelberg
Sent: Monday, March 03, 2014 4:13 PM
To: Jodie Levandowski
Cc: Bill Parker
Subject: Cumberland Farms Plan Review

Jodie,

In follow-up to today's meeting regarding the Cumberland Farms Nashua St. entrance/egress concerns, the proposal to periodically review accident data at the Walgreens intersection and place, if needed, signage stating no right on red at the Walgreens exit is acceptable.

Further, placement on No U-Turn signage to westbound traffic on the Walgreens intersection median(s) should further reduce crashes related to traffic entering Cumberland Farms from Nashua St.

Thank you for the opportunity to provide input on this project.

Have a good day,

Eric

Eric Schelberg
Director

Milford Ambulance Service
66 Elm St.
Milford, NH 03055

Telephone: (603) 249-0610
Fax: (603) 249-0611

email: eschelberg@milford.nh.gov

website: www.milford.nh.gov

Jodie Levandowski

From: Jason Smedick
Sent: Monday, March 03, 2014 2:49 PM
To: Jodie Levandowski
Subject: Cumberland Farms Meeting

Jodie,

After today's meeting for the aforementioned project we have reached the following agreement with the representatives from Cumberland Farms:

1. The access and egress driveway off of Nashua Street can stay in their location but will be widened to 20'.
2. We will look at adding a no turn on red sign at the right turn lane onto Nashua St. 6 months after to completion of the facility.

Should you have any questions, please do not hesitate to contact me.

Thank you.

J

Capt. Jason A Smedick
Milford Fire Dept.
Bureau of Fire Prevention & Investigation
39 School Street
Milford, NH 03055
603-249-0680



Town of Milford

POLICE DEPARTMENT
19 Garden Street
Milford, NH 03055
603 249-0630

Michael J. Viola
Chief of Police



To: Jodi Levandowski, Office of Community Development

From: Chief Michael J. Viola

Date: March 6, 2014

Ref: Cumberland Farms Location

On Monday, March 3, 2014, a meeting was held at Town Hall to discuss any possible issues with the new Cumberland Farms location. During this meeting, representatives from Cumberland Farms gave an overview of the building plans, which included entry and exit points to the location.

Upon the review of Cumberland Farms lot and building plans and the accepted changes that were discussed at the meeting, on behalf of the Milford Police Department, I would approve this location. With the discussed changes, I believe that this location would not substantially increase any police related concerns or issues.

Some of the discussion points that were stated during this meeting were in reference to the entry and exit points to the location. On behalf of the Police Department, I believe that these areas would not be an additional safety hazard to the people who are traveling through. There will be two entry points into the location. The Nashua Street entrance, which is just east of the traffic control device located at the Walgreens Access Road, was the entry point that was discussed. With the traffic signal there, I do not feel that this would have a significant impact on drivers in regards to safety. As discussed in the meeting, the Police Department will monitor the traffic related activity in this area. Should there be a noticeable increase in traffic related incidents, additional enforcement and educational measures will be put in place. Other measures discussed include a No U turn for drivers traveling west on Nashua Street and a No Turn on Red for drivers turning east on Nashua Street from the Walgreens Access Road.

The other issue discussed was in reference to the exit point of the Cumberland Farms location and the Nashua Street entry point to the McDonalds parking lot. I do not believe that this would cause a significant traffic issue for people driving through this area. With the recent McDonalds renovations, it appears that drivers are not using the Nashua Street entrance as frequently as they have in the past. It appears that drivers are traveling to the next intersection to enter the McDonalds parking lot. This is most likely due to the drive thru renovations.

Please contact me if you have any further questions or concerns.



TOWN OF MILFORD, NH

COMMUNITY DEVELOPMENT DEPARTMENT
1 UNION SQUARE, MILFORD, NH 03055

TEL:(603)249-0620
www.milford.nh.gov

STAFF MEMO

Date: February 18, 2014
To: Town of Milford Planning Board
From: Jodie Levandowski, Town Planner
Subject: **John Samonas/TMC CF of New England LLC – Nashua St – Map 44, Lot 11;**
Public Hearing for a major site plan for a proposed retail convenience store and gas sales with associated site improvements.
(MHF Design Consultants)

BACKGROUND:

As you may recall, the applicant (TMC CF New England, LLC) was last before the Board on December 17, 2013 for a conceptual discussion for the proposed site improvements including the location of a new motor fuel outlet which includes a 4,513 SF convenience store, fuel dispensers (8 fueling positions) and an overhead canopy, located on the east end of Nashua Street, Map 44 Lot 11 (proposed 99 Restaurant site).

The existing 1.651 acre property is zoned Integrated Commercial Industrial (ICI) and currently features an existing foundation and paved parking area for a previously approved 6,630 square foot “99” restaurant. The restaurant pad and underground site utilities were installed but the building was not constructed. The property is situated on the south side of Nashua Street with existing retail uses to the east and west (Burger King to the east and Walgreens to the west).

The site for the proposed Cumberland Farms is 1.651 acres in size, and is located entirely within the Integrated Commercial-Industrial (ICI) Zone; this is an allowable use within the District. The site is also located within the Level II Groundwater Protection Overlay District and this type of facility is allowed.

SITE LAYOUT:

The specifics of the project include the construction of a 4,513 square foot New England style retail convenience store building with 4 gasoline dispensers to be located parallel with Nashua Street and under an overhead canopy along with a total of 24 parking spaces and lighting and landscape improvements.

Access to both the site and the existing Walgreens pharmacy from Nashua Street is provided by a shared driveway at a traffic signal located at the intersection with Lorden Plaza’s west driveway. The applicant is proposing a new curb cut to be added on the shared internal driveway, the existing curb cut shall remain. Based on coordination efforts with department heads, a right-turn in driveway and a

separate right-turn out driveway is proposed to be constructed on NH Route 101A. However, concerns are still present regarding potential conflicts involving the new McDonalds entrance.

TRAFFIC:

A trip generation and safety assessment report has been prepared by Tighe & Bond, Inc. The report was prepared to evaluate the traffic impacts associated with the Cumberland Farms project. The report established 5 conclusions based on the project change from a restaurant site to a mobile fueling center (see attached list of 5 conclusions).

The Full Traffic report will be available for review at the 02/18/2014 meeting and in the Office of Community Development.

DRAINAGE:

The proposed stormwater design is consistent with the previous approvals for the "99" Restaurant plan as permitted in 2008.

The total area of disturbance is approximately 55,000 square feet. There will be no disturbance of any wetland resource area and the proposal will result in a reduction of the impervious coverage by 684 square feet.

WAIVERS:

No waivers are being requested at this time.

NOTICES:

Abutter notices were sent by certified mail to all abutters on February 7, 2014

APPLICATION STATUS:

The application is complete and ready to be accepted at this time. The Board will need to make a determination of regional impact.

INTERDEPARTMENT REVIEWS:

Water Utilities:

No issues with the proposed design

Zoning Administrator:

Proposed uses are allowed in the ICI Zone- retail businesses and filling stations. Site lies within Level II Groundwater Protection Zone and the uses are allowed. All storage of regulated materials must meet all Federal, State and Local requirements.

Fire:

After review of the aforementioned plans we have the following concerns:

1. The entrance and exit off of Nashua Street are only 15' wide. They are required to be 20'.
2. We have concerns relative to having cars being able to enter and exit via these two curb cuts. The traffic in the area is already heavy and with the installation of the new McDonalds entrance we feel this has the real potential to increase traffic accidents in the area.

Building Department:

There are too many access points to Nashua St. in that area (DPW has the final word on that) but having been caught in traffic in that area it would be better to have access off the driveway already constructed that is serving Walgreens. Once a decision is made on that I'll determine if the addressing is off of Nashua St. or the short roadway (which we will name).

Ambulance Service:

Is west entrance directly off of Nashua St. necessary with entrance off of Walgreen entrance also proposed. Concern with possible crashes with vehicles exiting in an easterly direction from Walgreens and immediately approaching an entrance.

Environmental Coordinator:

The drainage from the roofs appears to be handled well in the infiltration structure. However, I have several questions regarding the remainder of the project.

1. Oil/Gas separator details are included. Are these to be installed in all catch basins?
2. There appears to be little treatment of paved-area runoff (other than the aforementioned separators). We anticipate that the 1" storm event will be infiltrated in this area as well. There is a mention of a bioretention swale, how does this function? What volume does it handle? Details?
3. The project will require an NPDES permit. The basis for the application (NOI) should be submitted as part of the Milford Stormwater Permit application. The owner is the decision maker and should be signatory to the SWPPP. Much of the required information is present in the O&M manual.
4. Can straw wattles or equal be substituted as an accepted improvement for the silt fence?
5. Can the landscaped areas be utilized as bioretention/rain garden facilities to enhance stormwater treatment?

In summary, we anticipate that a 1" rainfall will be infiltrated. All additional discharge should be subjected to "treatment" consistent with the NH AoT manual even though the project included less than 100,000 sq.ft. of disturbance.

Community Development:

The DPW Director and Community Development Director met with the site engineers on January 9th to review plans. Several departments noted concerns with the proposed Nashua Street access. As these are entrance/exit only, and blocked by a center Nashua Street median, conflicts will be minimal. Additionally, the signal will be reprogrammed allowing safe timing eastbound for exit out of the Cumberland Farms site as traffic will be stopped to allow for breaks.

The DPW Director was satisfied with the proposed circulation. The driveways on Nashua Street exceed the Town Driveway requirements for commercial curb cut distances. The easterly exit is 60' at property line from the McDonald's entrance (50' is minimum) and the entrance is 80' from the internal shared signalized drive (50' minimum). There is 80' between the entrance only and exit only. All requirements are met for distance between curb cuts.

The DPW Director has been in close communication with both Jason Plourde of Tighe & Bond and the signal company to insure signal timing is coordinated with the NHDOT signal to the east.

No comments were received as of February 13, 2014 from Public Works, Police or Assessing. The Heritage Commission and Conservation Commission's regular meeting were held after staff memos were distributed, if any comments come in, Staff will let the Board know at the meeting.

STAFF RECOMMENDATIONS:

The Board should first make a determination of regional impact. The Board should also discuss the effects the proposed project will have on traffic in the area and stormwater. The stormwater plan was revised based on the previously approved "99" restaurant site. The Board should discuss with the applicant stormwater runoff from this property and the effect it may have based on the proposed changes.

The applicant has expressed a willingness to contribute a contribution for pedestrian improvements. The Planning Board should discuss with the applicant a contribution that will go towards pedestrian signalization and intersection improvements at Nashua Street and existing signal, to tie in with the Nashua Street Sidewalk Project.

Staff has no significant issues with the application. Plans are ready for conditional approval pending final review of plan notes and landscaping plan by the Community Development Office.



PROPOSED SITE RE-DEVELOPMENT PLANS

for

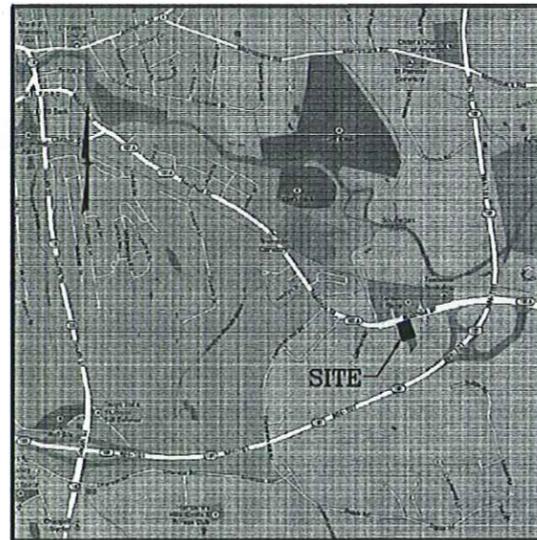
ASSESSORS MAP 44 - LOT 11

583 NASHUA STREET
MILFORD, NEW HAMPSHIRE

Prepared for:



100 CROSSING BLVD, FRAMINGHAM, MASSACHUSETTS 01702



LOCATION MAP
(NOT TO SCALE)

INDEX TO DRAWINGS

CFG1.0	TITLE SHEET
CFG2.0	EXISTING CONDITIONS PLAN
CFG3.0	DEMOLITION PLAN
CFG4.0	SITE PLAN
CFG5.0	GRADING & DRAINAGE PLAN
CFG6.0	EROSION CONTROL PLAN
CFG7.0	UTILITIES PLAN
CFG8.0	LANDSCAPE PLAN
CFG9.0	DETAIL SHEET
CFG9.1	DETAIL SHEET
CFG9.2	DETAIL SHEET
CFG9.3	SEWER DETAILS
CFG10.0	LIGHTING PLAN (LSI)
CFG10.1	FIXTURE SPECIFICATIONS SHEETS
CFG11.0	FIRE SUPPRESSION PLAN (BY OTHERS)
CFG12.0	PROPOSED CANOPY ELEVATIONS
CFG12.1	PROPOSED CANOPY DETAILS
CFG16.0	TRUCK TURN PLAN (TTP)
A1.1	FLOOR PLAN & PARTITION TYPES *
A3.1	EXTERIOR ELEVATIONS *
A3.2	EXTERIOR ELEVATIONS *
DS1.1	DOWNSPOUT LOCATION PLAN *
LI.1	EXTERIOR BUILDING LIGHTING PLAN *

* ARCHITECTURAL PLANS FOR PERMITTING USE ONLY.
CONTRACTOR TO REFER TO STAMPED, SIGNED, SEALED
PLANS LABELED "FOR CONSTRUCTION".



44 Siles Road, Suite One
Salem, New Hampshire 03079
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REVISIONS			
NO.	DATE	REV. BY	DESCRIPTION
1	3/7/14	CMT	REV SHEETS CFG3.0-9.2, CFG10.0 & CFG16.0

SQUARE FEET
ACRES
V# NEW
Store# NEW
Gas Station# NEW

583 NASHUA STREET
MILFORD, NH

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

SCALE: NTS
DATE: JANUARY 29, 2014
FILE: 3422CVR.dwg
DRAWN BY: NFM
CHECKED BY: CMT

TITLE SHEET CFG1.0

c:\Projects\0421\3422CVR.dwg Model: 3/10/14 2:06pm nfm

SCHEDULE B-2 EXCEPTIONS

- 9 RECIPROCAL EASEMENT AGREEMENT WITH COVENANTS, CONDITIONS AND RESTRICTIONS DATED 4/3/07 AND RECORDED IN BOOK 7831 PAGE 2546 AFFECTS THE SURVEY TRACT AND THE PLOTTABLE EASEMENTS ARE SHOWN HEREON.
- 10 EASEMENT TO THE TOWN OF MILFORD DATED 4/30/07 AND RECORDED IN BOOK 7831 PAGE 2566 AFFECTS THE SURVEY TRACT AND IS SHOWN HEREON.
- 11 EASEMENT AS RECITED IN DEED TO THE STATE OF NEW HAMPSHIRE DATED 6/29/70 AND RECORDED IN BOOK 2085 PAGE 495 DOES NOT AFFECT THE SURVEY TRACT.
- 12 COMMISSIONERS RETURN OF HIGHWAY LAYOUT RECORDED IN BOOK 2088 PAGE. DOES NOT AFFECT THE SURVEY TRACT.
- 13 NOTICE OF OPTION TO PURCHASE DATED 4/3/07 AND RECORDED IN BOOK 7831 PAGE 2570 DOES NOT AFFECT THE SURVEY TRACT. THE AREA SOUTH OF THE POND WAS SUBDIVIDED OFF AS SHOWN ON THE SUBDIVISION PLAN RECORDED AS #37742.

PLAN REFERENCES:

- 1) PLAN OF PROPOSED FEDERAL AID PRIMARY PROJECT, No. F-FG 010-1(2), N.H. PROJECT No. P-2419.
- 2) RIGHT OF WAY PLANS OF PROPOSED FEDERAL AID PRIMARY PROJECT, N.H. PROJECT No. P-7259B.
- 3) HILLSBOROUGH COUNTY REGISTRY OF DEEDS (H.C.R.D.) PLAN #35380.
- 4) H.C.R.D. PLAN #35379.
- 5) H.C.R.D. PLAN #35378.
- 6) H.C.R.D. PLAN #34284.
- 7) H.C.R.D. PLAN #30832.
- 8) H.C.R.D. PLAN #26976.
- 9) H.C.R.D. PLAN #26926.
- 10) H.C.R.D. PLAN #25346.
- 11) H.C.R.D. PLAN #688.
- 12) GRADING AND DRAINAGE PLAN (LOT 44-13-1), PREPARED FOR ARISTA DEVELOPMENT, LLC; SCALE: 1"=20'; REVISION DATE 6/5/07; PREPARED BY TF MORAN, INC..
- 13) UTILITY PLAN (LOT 44-13-1), PREPARED FOR ARISTA DEVELOPMENT, LLC; SCALE: 1"=20'; REVISION DATE 6/19/07; PREPARED BY TF MORAN, INC..
- 14) NINETY NINE, NASHUA STREET (NH ROUTE 101A), MILFORD, NEW HAMPSHIRE, SITE PLANS, APRIL 16, 2008; PREPARED BY APPLIED ENGINEERING, INC..
- 15) H.C.R.D. PLAN #37742.
- 16) AS-BUILT UTILITIES PLAN, TAX MAP 44 LOT 11, PREPARED FOR SAMONAS REALTY; SCALE: 1"=20'; DATE: OCTOBER 7, 2008; PREPARED BY TF MORAN, INC..
- 17) SITE PLAN, TAX MAP F7 LOT 44-10, PREPARED FOR MCDONALD'S USA, LLC; SCALE: 1"=20'; DATE: 2/18/13, LAST REVISED 9/4/13; PREPARED BY BOHLER ENGINEERING.

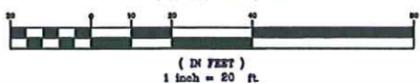
ENCROACHMENTS:

- OVERHEAD WIRES FROM UTILITY POLE "PSNH 77" TO UTILITY POLE "PSNH 78-1" ENCROACH ONTO THE SURVEY TRACT 3".
- TRAFFIC CONTROL CABINET ENCROACHES OUTSIDE OF THE EASEMENT BY 5".
- TRANSFORMER PAD WITH CONDUIT AND ELECTRICAL LINES.
- CONCRETE SIDEWALK ON WESTERLY SIDE OF SURVEY TRACT.
- LANE USE SIGN ON WESTERLY SIDE OF SURVEY TRACT.
- FIRE HYDRANT

LEGEND

○ IRON ROD FOUND	△ SPIKE FOUND
○ UTILITY POLE	— VERTICAL GRANITE CURB
○ SEWER MANHOLE	— SLOPED GRANITE CURB
○ DRAIN MANHOLE	— VERTICAL CONCRETE CURB
○ CATCH BASIN	— DOUBLE SOLID YELLOW LINE
○ FIRE HYDRANT	— SINGLE SOLID WHITE LINE
○ WATER VALVE	— BROKEN WHITE LINE
○ WATER SHUT OFF	— QUADRANT
○ GAS VALVE	— WATER LINE
○ SIGN	— UNDERGROUND ELECTRIC & TELEPHONE
○ CLEAN OUT	— GAS LINE
○ STONEWALL	— OVERHEAD SERVICE WIRES
○ CHAINLINK FENCE	— WETLAND LINE
○ WIRE FENCE	— NH NEW HAMPSHIRE HIGHWAY BOUND

GRAPHIC SCALE

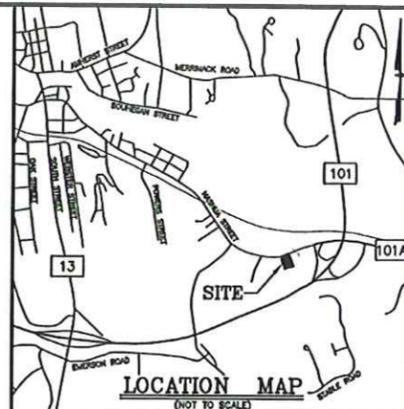


OWNER OF RECORD:

JOHN SAMONAS
157 NEWINGTON ROAD
GREENLAND, NH 03840
BOOK 7831 PAGE 2538

NO.	DESCRIPTION	DATE	BY	DATE
1	MISC. REVISIONS	1/20/14	CMF	
	REVISIONS			

SCALE: 1"=20' DATE: DECEMBER 10, 2013
DRAWN BY: JWW CHECKED BY: CMF PROJECT NO. 342213 SHEET NO. CFG2.0



METES & BOUNDS DESCRIPTION

A certain tract or parcel of land situate in the State of New Hampshire, County of Hillsborough and Town of Milford located on the southerly side of New Hampshire Route 101A (a.k.a. Nashua Street) and shown as NEW 44-11 on a plan entitled Tax Map 44 Lot 11 Subdivision Plan 583 Nashua Street (NH Route 101A) Milford, New Hampshire owned by and prepared for John Samonas; Scale: 1"=50'; Date: February 19, 2012 (revised to 4/22/13) by TFM, Inc. and recorded in the Hillsborough County Registry of Deeds as plan #37742 and being more particularly bounded and described as follows:

Beginning at an iron rod on the southerly sideline of said New Hampshire Route 101A, thence;
Partly by said New Hampshire Route 101A and by land now or formerly of Cole Mt Milford NH, LLC S00°28'20"E seventy-one and sixty-five hundredths (71.65) feet to a point, thence;
Continuing by land now or formerly of said Cole Mt Milford NH, LLC S03°22'24"E two hundred sixty and thirteen hundredths (260.13) feet to a point at other land now or formerly of John Samonas, thence;

By land now or formerly of said John Samonas N85°22'51"W two hundred thirty-nine and forty-three hundredths (239.43) feet to a point at land now or formerly of Walgreen Eastern Co., Inc., thence;

By land now or formerly of said Walgreen Eastern Co., Inc. N01°15'24"E three hundred six and three hundredths (306.03) feet to a spike on said southerly sideline of Route 101A ;

Thence by said southerly sideline of New Hampshire Route 101A N85°23'09"E two hundred sixteen and thirteen hundredths (216.13) feet to the point of beginning.

Said tract or parcel of land contains 71,940 square feet or 1.651 acres more or less.

NOTES:

- 1) THE SURVEY TRACT IS LOCATED WITHIN A LEVEL II PROTECTION AREA OVERLAY DISTRICT PER THE TOWN OF MILFORD ZONING ORDINANCE, ARTICLE VI, SECTION 6.01, GROUNDWATER PROTECTION.
- 2) THE SURVEY TRACT IS LOCATED IN ZONE 'C', AREA OF MINIMAL FLOODING, AS SHOWN ON THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL #330096 0478 D FOR THE TOWN OF MILFORD, NEW HAMPSHIRE, EFFECTIVE DATE: SEPTEMBER 25, 2009.
- 3) LOCATION OF UNDERGROUND UTILITIES IS APPROXIMATE ONLY. ADDITIONAL UNDERGROUND UTILITIES OTHER THAN THOSE SHOWN MAY BE ENCOUNTERED.
- 4) REFER TO FIRST AMERICAN TITLE INSURANCE COMPANY COMMITMENT FOR TITLE INSURANCE #NCS-631813-H0U1, EFFECTIVE: SEPTEMBER 23, 2013.
- 5) THIS PLAN IS THE RESULT OF A FIELD SURVEY MADE BY THIS OFFICE IN OCTOBER 2013.

CERTIFICATION:

TO: CUMBERLAND FARMS GROUP AND FIRST AMERICAN TITLE INSURANCE COMPANY
THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS IN 2011, AND INCLUDES ITEMS 2, 3, 4, 7A, 7B, 8, 9, 10, 11B, 12, 13 AND 19 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON OCTOBER 10, 2013.



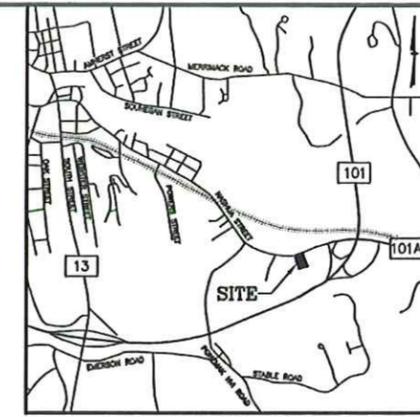
CHRISTOPHER TRUOG - REG. #734
DATE OF PLAN OR MAP: DECEMBER 10, 2013

EXISTING CONDITIONS PLAN

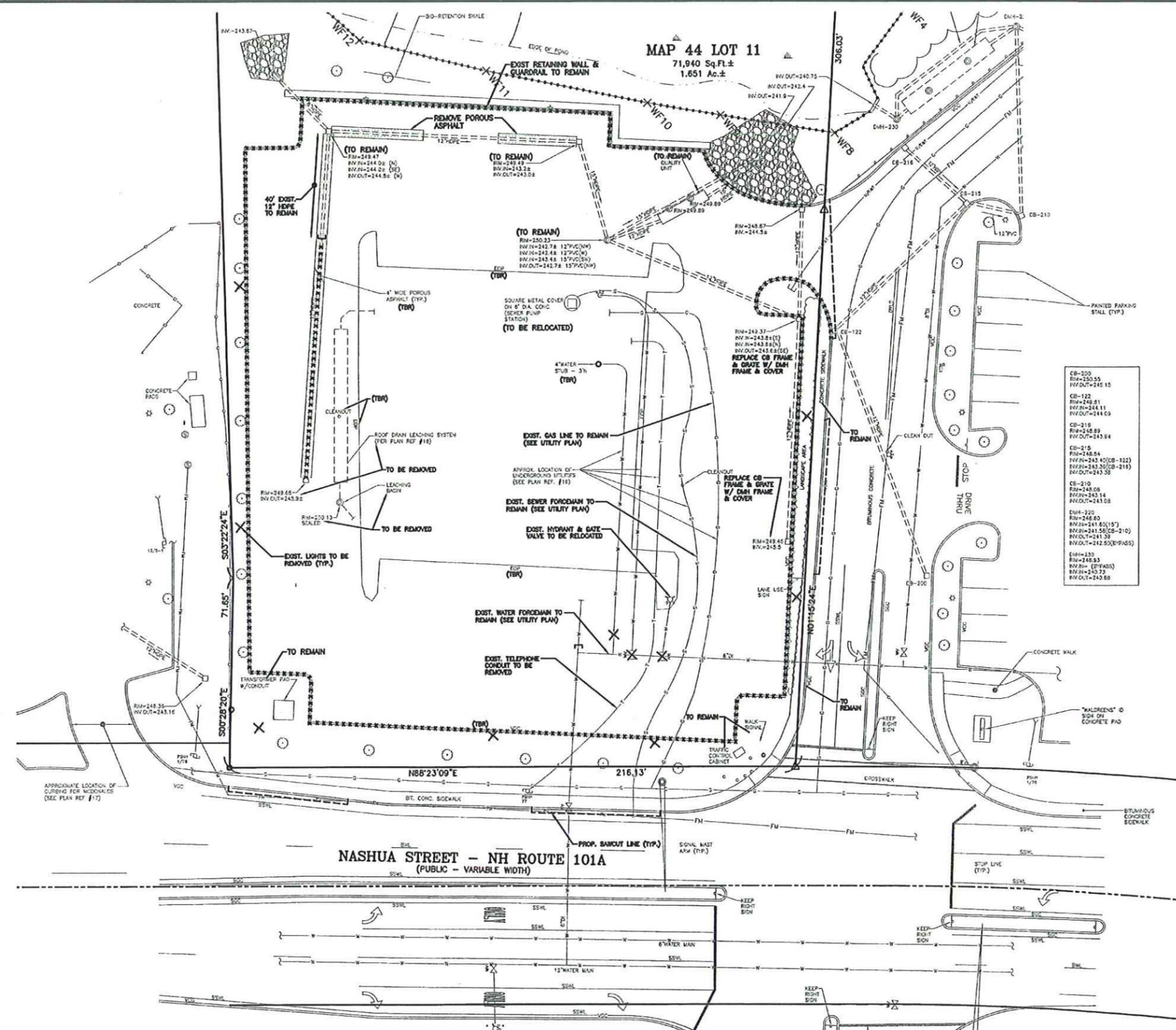
ASSESSORS MAP 44 - LOT 11
583 NASHUA STREET (ROUTE 101A)
MILFORD, NEW HAMPSHIRE
PREPARED FOR:
CUMBERLAND FARMS GROUP
100 CROSSING BOULEVARD
FRAMINGHAM, MASSACHUSETTS 01702



MAP 44 LOT 11
71,940 Sq. Ft. ±
1.651 Ac. ±



LOCATION MAP
(NOT TO SCALE)



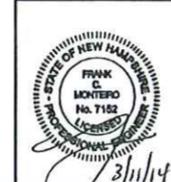
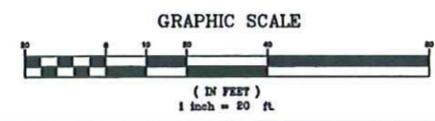
NOTES:

- 1) A DEMOLITION PERMIT MUST BE OBTAINED FROM THE TOWN OF MILFORD PRIOR TO COMMENCEMENT OF WORK. ALL EXISTING UTILITY DISCONNECTIONS MUST BE COORDINATED WITH RESPECTIVE UTILITY COMPANIES.
- 2) ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN STRICT ADHERENCE TO ALL FEDERAL, STATE AND LOCAL REGULATIONS. CONTRACTOR TO INSTALL EROSION CONTROL DEVICES IN ACCORDANCE WITH GRADING & DRAINAGE PLAN PRIOR TO BEGINNING DEMOLITION ACTIVITIES.
- 3) PROCEED WITH DEMOLITION IN A SYSTEMATIC MANNER, FROM THE TOP OF THE STRUCTURE(S) TO THE GROUND.
- 4) DEMOLISH CONCRETE IN ALL SECTIONS.
- 5) BREAK UP CONCRETE SLABS-ON-GRADE, UNLESS OTHERWISE DIRECTED BY THE CONSTRUCTION MANAGER.
- 6) CONDUCT ALL DEMOLITION OPERATIONS IN A MANNER THAT WILL PREVENT INJURY, DAMAGE TO STRUCTURES, ADJACENT BUILDINGS AND ALL PERSONS.
- 7) REFRAIN FROM USING EXPLOSIVES WITHOUT PRIOR WRITTEN CONSENT OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES.
- 8) CONDUCT DEMOLITION SERVICES IN SUCH A MANNER TO INSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS AND OTHER ADJACENT FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS OR OTHER OCCUPIED FACILITIES WITHOUT PRIOR WRITTEN PERMISSION OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES. PROVIDE ALTERNATIVE ROUTES AROUND CLOSED OR OBSTRUCTED TRAFFIC WAYS IF REQUIRED BY APPLICABLE GOVERNMENTAL REGULATIONS.
- 9) USE WATERING, TEMPORARY ENCLOSURES AND OTHER SUITABLE METHODS, AS NECESSARY TO LIMIT THE AMOUNT OF DUST AND DEBRIS RISING AND SCATTERING IN THE AIR. CLEAN ADJACENT STRUCTURES AND IMPROVEMENTS OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. RETURN ALL ADJACENT AREAS TO THE CONDITIONS EXISTING PRIOR TO THE START OF WORK.
- 10) ACCOMPLISH AND PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE UNAUTHORIZED ENTRY OF PERSONS AT ANY TIME.
- 11) COMPLETELY FILL BELOW GRADE AREAS AND VOIDS RESULTING FROM THE DEMOLITION OF STRUCTURES AND FOUNDATIONS WITH SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. STONES USED WILL NOT BE LARGER THAN 6 INCHES IN DIMENSION. MATERIAL FROM DEMOLITION MAY NOT BE USED AS FILL. PRIOR TO PLACEMENT OF FILL MATERIALS, UNDERTAKE ALL NECESSARY ACTION IN ORDER TO INSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROZEN MATERIAL, TRASH, DEBRIS. PLACE FILL MATERIAL LAYERS NOT EXCEEDING 6 INCHES IN LOOSE DEPTH AND COMPACT EACH LAYER AT PLACEMENT TO 95% OPTIMUM DENSITY, GRADE SURFACE TO MEET ADJACENT CONTOURS AND TO PROVIDE SURFACE DRAINAGE.
- 12) REMOVE FROM THE DESIGNATED SITE, AT THE EARLIEST POSSIBLE TIME, ALL DEBRIS RUBBISH, SALVAGEABLE ITEMS, HAZARDOUS AND COMBUSTIBLE MATERIALS. REMOVED MATERIALS MAY NOT BE STORED, SOLD OR BURNED ON SITE. REMOVAL OF HAZARDOUS AND COMBUSTIBLE MATERIALS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROCEDURES AS AUTHORIZED BY THE FIRE DEPARTMENT OR OTHER APPROPRIATE REGULATORY AGENCIES AND DEPARTMENTS.
- 13) DISCONNECT, SHUT OFF AND SEAL ALL UTILITIES SERVING THE STRUCTURE(S) TO BE DEMOLISHED BEFORE THE COMMENCEMENT OF THE DESIGNATED DEMOLITION. MARK FOR POSITION ALL UTILITY DRAINAGE AND SANITARY LINES AND PROTECT ALL ACTIVE LINES. CLEARLY IDENTIFY BEFORE THE COMMENCEMENT OF DEMOLITION SERVICES THE REQUIRED INTERRUPTION OF ACTIVE SYSTEMS THAT MAY AFFECT OTHER PARTIES, AND NOTIFY ALL APPLICABLE UTILITY COMPANIES TO INSURE THE CONTINUATION OF SERVICE.
- 14) PROTECT EXISTING DRAINAGE SYSTEM(S) AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING DURING CONSTRUCTION. SEE DETAIL SHEETS FOR EROSION CONTROL DEVICES.
- 15) ALL WORK WITHIN ROADWAY RIGHT-OF-WAYS TO CONFORM TO MILFORD AND NH DOT STANDARDS.
- 16) THE LIMITS OF WORK SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO THE START OF CONSTRUCTION OR SITE CLEARING.
- 17) IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO NOTIFY "DIG SAFE" (1-888-344-7233) 72 HOURS PRIOR TO ANY EXCAVATION ON THIS SITE. CONTRACTOR SHALL ALSO NOTIFY LOCAL WATER DEPARTMENT TO MARK OUT THEIR UTILITIES.
- 18) NOTES ON THIS PLAN THAT READ "TBR" REPRESENT FEATURES TO BE REMOVED. ANY FEATURES NOT LABELED "TBR" OR "TO BE REMOVED" SHALL BE CONSIDERED EXISTING TO REMAIN.

CB-200	RM=250.55
RM=250.55	RV.OUT=245.15
CB-122	RM=248.81
RM=248.81	RV.IN=244.11
RV.IN=244.11	RV.OUT=244.09
CB-216	RM=248.89
RM=248.89	RV.OUT=245.84
CB-215	RM=248.84
RM=248.84	RV.IN=243.40(CB-122)
RV.IN=243.40(CB-122)	RV.IN=243.30(CB-216)
RV.IN=243.30(CB-216)	RV.OUT=243.30
CB-210	RM=248.08
RM=248.08	RV.IN=243.14
RV.IN=243.14	RV.OUT=243.08
DM-220	RM=248.80
RM=248.80	RV.IN=241.80(15')
RV.IN=241.80(15')	RV.IN=241.58(CB-210)
RV.IN=241.58(CB-210)	RV.OUT=241.39
RV.OUT=241.39	RV.OUT=242.82(EP-PASS)
DM-130	RM=248.83
RM=248.83	RV.IN=240.73
RV.IN=240.73	RV.OUT=240.88

LEGEND

- | | | | |
|---|-------------------|--------|--------------------------|
| ○ | UTILITY POLE | — | OVERHEAD SERVICE WIRES |
| ⊙ | DRAIN MANHOLE | — | DOUBLE SOLID YELLOW LINE |
| ⊙ | SEWER MANHOLE | — | SINGLE SOLID WHITE LINE |
| ⊙ | TELEPHONE MANHOLE | — | SIGN |
| ⊙ | CATCH BASIN | ⊙ | OBSERVATION WELL |
| — | WATER LINE | — | TREELINE |
| ⊙ | WATER VALVE | — | CONTOUR ELEVATION |
| ⊙ | FIRE HYDRANT | — | GAS LINE |
| ⊙ | GAS VALVE | (TBR) | TO BE REMOVED |
| — | | XXXXXX | TO BE REMOVED |



REVISIONS			
NO.	DATE	REV. BY	DESCRIPTION
1	3/7/14	CMT	REV'S PER TOWN REVIEW

SQUARE FEET	
ACRES	
V# NEW	
Store# NEW	
Gas Station# NEW	

CROWLEY & ASSOCIATES

MHF Design Consultants, Inc.

44 Siles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

583 NASHUA STREET
MILFORD, NH

Cumberland FARMS

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMMINGHAM, MA 01702

SCALE: 1"=20'
DATE: JANUARY 20, 2014
FILE: 3422SP.dwg
DRAWN BY: NFM
CHECKED BY: CMT

DEMOLITION PLAN

CFG3.0

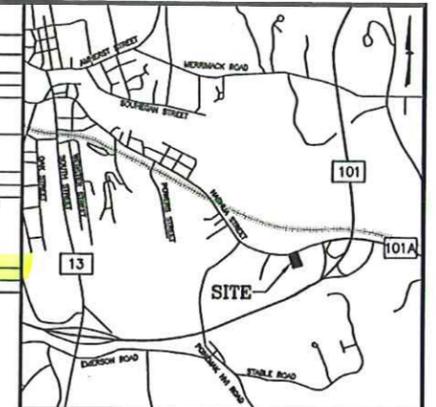
LEGEND

- IRON PIN FOUND
- CONCRETE BOUND FOUND
- △ RAILROAD SPIKE FOUND
- DRILL HOLE FOUND
- VERTICAL GRANITE CURB
- SLOPED GRANITE CURB
- BITUMINOUS CONCRETE LIP CURBING
- BITUMINOUS CONCRETE BERM
- OVERHEAD SERVICE WIRES
- DOUBLE SOLID YELLOW LINE
- SINGLE SOLID WHITE LINE
- BROKEN WHITE LINE
- ⊕ SIGN
- ⊕ OBSERVATION WELL
- ⊕ TEST PIT
- ⊕ TEST BORING
- ⊕ PERCOLATION TEST
- TREELINE
- UTILITY POLE
- DRAIN MANHOLE
- SEWER MANHOLE
- TELEPHONE MANHOLE
- CATCH BASIN
- WATER LINE
- WATER VALVE
- FIRE HYDRANT
- GAS VALVE
- GAS LINE
- UNDERGROUND TELEPHONE LINE
- UNDERGROUND ELECTRIC AND TELEPHONE
- WETLAND LINE
- SPOT ELEVATION
- CONTOUR ELEVATION

MAP 44 LOT 11
71,940 Sq.Ft. ±
1.851 Ac. ±

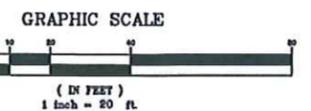
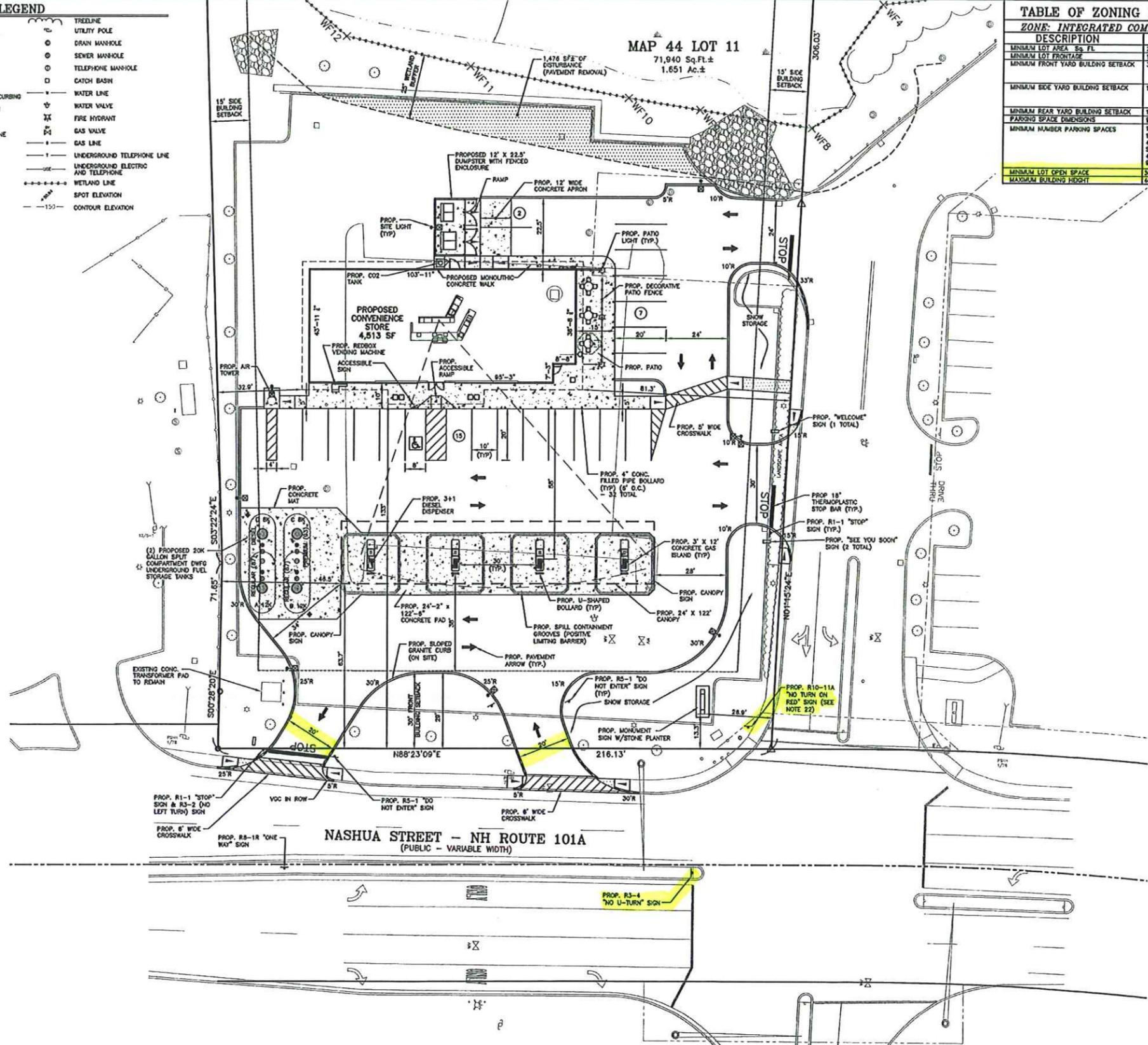
TABLE OF ZONING REGULATIONS - MILFORD, NH

DESCRIPTION	REQUIRED	PROVIDED
MINIMUM LOT AREA - SQ. FT.	20,000 SF	71,940 SF
MINIMUM LOT FRONTAGE	110'	216.13'
MINIMUM FRONT YARD BUILDING SETBACK	30'	141.2' (BUILDING) 133.0' (BUILDING OVERHANG) 83.7' (CANOPY)
MINIMUM SIDE YARD BUILDING SETBACK	15'	35.0' (BUILDING) 32.0' (BUILDING OVERHANG) 46.5' (CANOPY)
MINIMUM REAR YARD BUILDING SETBACK	15'	122.8'
PARKING SPACE DIMENSIONS	9'x15'	10'x20'
MINIMUM NUMBER PARKING SPACES	1 SPACE/EMPLOYEE PLUS 4 EMPLOYEES/MAX SHIFT = 4 SPACES 4,513 SF/1,000 SF = 8 SPACES 8 SPACES REQUIRED	24 SPACES (EXCLUDING 8 SPACES AT FUEL ISLANDS)
MINIMUM LOT OPEN SPACE	30%	51.8% (37,300 SF)
MAXIMUM BUILDING HEIGHT	40'	32'-10"



NOTES:

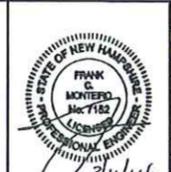
- 1) THE BOUNDARY INFORMATION SHOWN HEREON FOR MAP 44 LOT 11 IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE IN OCTOBER 2013.
- 2) ASSESSORS MAP 44 LOT 11.
- 3) ZONING DISTRICT: INTEGRATED COMMERCIAL-INDUSTRIAL "IC" DISTRICT
- 4) THE SURVEY TRACT IS LOCATED WITHIN A LEVEL II PROTECTION AREA OVERLAY DISTRICT PER THE TOWN OF MILFORD ZONING ORDINANCE, ARTICLE VI, SECTION 6.01, GROUNDWATER PROTECTION.
- 5) LOT AREA = 71,940 Sq.Ft. = 1.851 Ac. ±
- 6) EXISTING USE: DEVELOPED PAD SITE (FORMERLY APPROVED 99'S RESTAURANT)
- 7) PROPOSED USE: RETAIL MOTOR FUEL OUTLET WHICH INCLUDES A 4,513 SF CONVENIENCE STORE AND A FUEL DISPENSING AREA WITH 4 DISPENSERS (8 FUELING POSITIONS) AND AN OVERHEAD CANOPY.
- 8) ALL BUILDINGS AND SITE CONSTRUCTION SHALL COMPLY WITH THE RULES AND REGULATIONS OF THE AMERICANS WITH DISABILITIES ACT (ADA) AS PUBLISHED IN THE FEDERAL REGISTER, VOL. 65, NO. 144, DATED JULY 26, 1991, REVISED JULY 23, 2004.
- 9) THE LOCATIONS OF EXISTING SUBSURFACE UTILITIES SHOWN ON THIS PLAN WERE COMPILED FROM AVAILABLE RECORD DRAWINGS AND ARE NOT WARRANTED TO BE CORRECT. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING SUBSURFACE UTILITIES PRIOR TO PERFORMING ANY WORK.
- 10) WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR.
- 11) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIGSAFE 1-888-344-7233 PRIOR TO ANY EXCAVATION.
- 12) ALL CONSTRUCTION SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF MILFORD PUBLIC WORKS DEPARTMENT AND THE STATE OF NEW HAMPSHIRE.
- 13) ALL CONSTRUCTION SHALL CONFORM TO THESE PLANS AND THE STANDARD CONSTRUCTION DRAWINGS AS SUPPLIED BY CUMBERLAND FARMS.
- 14) A SIGN PERMIT SHALL BE OBTAINED PRIOR TO INSTALLATION.
- 15) THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THESE PLANS, PREPARED BY MHP DESIGN CONSULTANTS, INC., DO NOT EXTEND TO OR INCLUDE SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR THEIR EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF THE SURVEYOR AND/OR ENGINEER AS INCLUDED IN THE PLAN SET DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE AND/OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND/OR LOCAL REGULATIONS.
- 16) ALL UNDERGROUND STORAGE TANKS, PRODUCT PIPING AND VENT LINES SHALL COMPLY WITH CURRENT STATE AND E.P.A. REGULATIONS.
- 17) ALL SNOW SHALL BE STORED IN THE AREA(S) DEPICTED ON THIS PLAN AS SNOW STORAGE AREAS. IN THE EVENT THAT THE AREA(S) APPROVED FOR SNOW STORAGE BECOME FULL, THE OWNER SHALL REASONABLY REMOVE EXCESS SNOW FROM THE SITE, AND SHALL NOT ALLOW SNOW TO BE STORED WITHIN PARKING LOTS OR TRAVEL AISLES.
- 18) ALL WASTE MATERIALS AND RECYCLABLES SHALL BE CONTAINED WITHIN THE BUILDING(S) OR APPROVED STORAGE FACILITIES AND SHALL NOT BE OTHERWISE STORED ON PROPERTY. DUMPSTERS SHALL BE EMPTIED BETWEEN THE HOURS OF 7:00 AM AND 5:00 PM.
- 19) ALL WASTE MATERIALS AND RECYCLABLES SHALL BE CONTAINED WITHIN THE BUILDING(S) OR APPROVED STORAGE FACILITIES AND SHALL NOT BE OTHERWISE STORED ON THE PROPERTY.
- 20) EXISTING IMPERVIOUS COVERAGE = 34,892 SF
PROPOSED IMPERVIOUS COVERAGE = 34,640 SF
- 21) AS-BUILT PLANS SHALL BE DELIVERED TO THE BUILDING DEPARTMENT PRIOR TO A CERTIFICATE OF OCCUPANCY BEING ISSUED.
- 22) PROPOSED "NO RIGHT ON RED" SIGN TO BE INSTALLED AT THE DISCRETION OF THE MILFORD POLICE DEPARTMENT.



OWNER'S AUTHORIZATION
AS THE OWNER OF MAP 44 LOT 11, I HEREBY ACKNOWLEDGE THE PROPOSED DEVELOPMENT DEPICTED HEREON.

John James
OWNER'S SIGNATURE

1/18/14
DATE



NO.	DATE	REV. BY.	DESCRIPTION
1	3/7/14	CMT	REVS PER TOWN REVIEW

SQUARE FEET
ACRES
V# NEW
Store# NEW
Gas Station# NEW

MHP Design Consultants, Inc.
44 Sites Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhpdesign.com

**583 NASHUA STREET
MILFORD, NH**

Cumberland Farms Inc.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

SCALE: 1"=20'
DATE: JANUARY 20, 2014
FILE: 34225P.dwg
DRAWN BY: CCC
CHECKED BY: CMT

SITE PLAN **CFG4.0**

F:\Projects\040\42213\34225P.dwg, SP, 3/10/14, 2:12pm, ntm

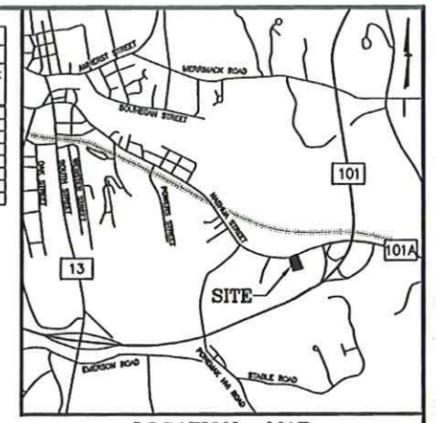


MAP 44 LOT 11
71,940 Sq.Ft.±
1.651 Ac.±

DRAINAGE STRUCTURES

- CB-11
RM=249.85
INV./OUT.=248.75
- CB-2
RM=248.55
INV./OUT.=248.27
- CB-3
RM=250.42
INV./OUT.=248.42
- CB-4
RM=250.06
INV./OUT.=245.20
- LOW PROFILE
FRAME & GRATE
& TOP SLAB
- DMH-1
RM=251.50
INV./OUT.=245.50 (PROP.)
INV./OUT.=245.5± (DOST.)
- DMH-2
RM=250.60
INV./OUT.=244.80 (PROP.)
INV./OUT.=244.5± (DOST.)
- DMH-3
RM=250.10
- DMH-4
RM=250.50
INV./OUT.=244.50(OCS-1)
INV./OUT.=244.8±(DOST.)

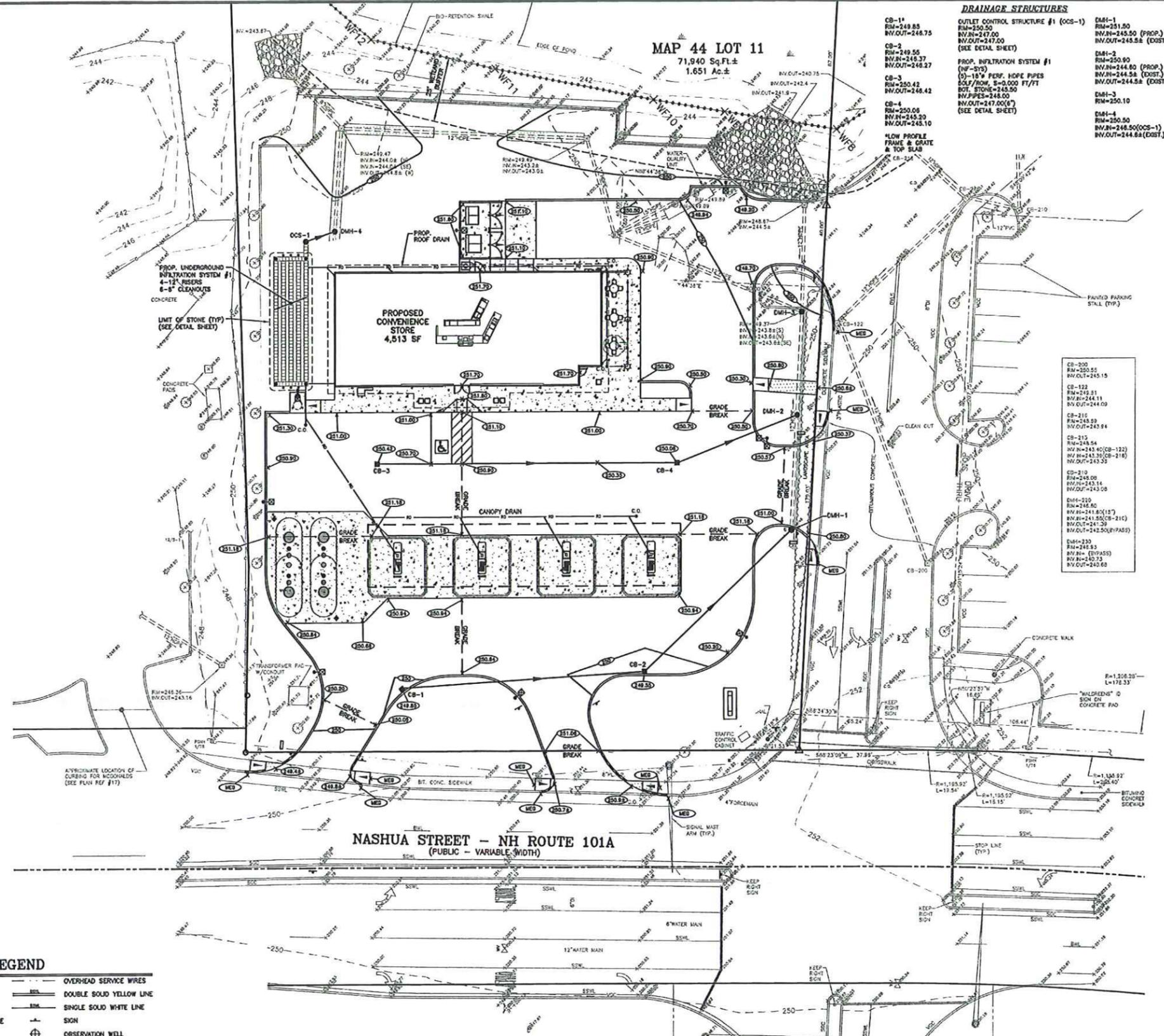
DRAINAGE PIPE SCHEDULE					
FROM:	PIPE SIZE	TYPE OF PIPE	APPROX. PIPE LENGTH (feet)	SLOPE OF PIPE (ft./ft.)	TO:
STRUCTURE NUMBER	(inches)			(ft./ft.)	STRUCTURE NUMBER
CB-1	12	HDPE	95	0.005	CB-2
CB-2	12	HDPE	77	0.010	DMH-1
CB-3	12	HDPE	108	0.012	CB-4
CB-4	12	HDPE	50	0.010	DMH-2
INF. SYS.	12	HDPE	8	0.000	OCS-1
OCS-1	8	PVC	10	0.050	DMH-4



LOCATION MAP
(NOT TO SCALE)

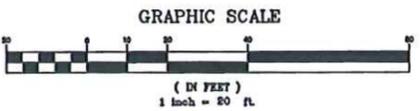
NOTES:

- 1) ALL SITE DRAINAGE PIPE SHALL BE CORRUGATED HIGH-DENSITY POLYETHYLENE PIPE WITH STANDARD JOINTS, DUAL-WALL, SMOOTH INTERIOR, AS MANUFACTURED BY ADS, INC., OR APPROVED EQUAL, UNLESS OTHERWISE NOTED ON PLAN.
- 2) ALL ROOF AND CANOPY DRAIN PIPE SHALL BE 6" PVC(SDR-35).
- 3) ELEVATIONS ARE BASED ON NVD 1929 DATUM.
- 4) ALL PROPOSED ELEVATIONS AS SHOWN ARE BOTTOM OF CURB ELEVATIONS, UNLESS OTHERWISE NOTED.
- 5) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- 6) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND INSTALLATIONS SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 7) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DFW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- 8) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
- 9) THIS SITE WILL REQUIRE A USEPA NPDES PERMIT FOR STORMWATER DISCHARGE FOR THE SITE CONSTRUCTION SINCE THE DISTURBANCE EXCEEDS ONE ACRE (ACTUAL DISTURBANCE = 55,000 SF). THE CONSTRUCTION SITE OPERATOR SHALL DEVELOP AND IMPLEMENT A CONSTRUCTION STORM WATER POLLUTION PREVENTION PLAN (SWPPP), WHICH SHALL REMAIN ON SITE AND MADE ACCESSIBLE TO THE PUBLIC. A COMPLETED NOTICE OF TERMINATION (NOT) SHALL BE SUBMITTED TO NPDES PERMITTING AUTHORITY WITHIN 30 DAYS AFTER EITHER OF THE FOLLOWING CONDITIONS HAVE BEEN MET: FINAL STABILIZATION HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH THE PERMITTEE IS RESPONSIBLE; OR ANOTHER OPERATOR/PERMITTEE HAS ASSUMED CONTROL OVER ALL AREAS OF THE SITE THAT HAVE NOT BEEN FINALLY STABILIZED.
- 10) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO NHDOT AND MILFORD DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- 11) ALL ADA ACCESSIBLE WALKWAYS CANNOT EXCEED 8% RUNNING SLOPE AND 2% CROSS SLOPE. RAMP'S CANNOT EXCEED 8.33% RUNNING SLOPE AND 2% CROSS SLOPE. AND NO PARKING STALLS AND ACCESS AISLES CANNOT EXCEED 2% SLOPE IN ANY DIRECTION. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.
- 12) SEE UTILITIES PLAN FOR DETAILED UTILITY LAYOUT.
- 13) CONTRACTOR IS TO FIELD ADJUST GRADES FOR THE TANK PAD TO SHED WATER.



LEGEND

- | | | | |
|-------|---------------------|------|--------------------------|
| ○ | UTILITY POLE | — | OVERHEAD SERVICE WIRES |
| ⊙ | DRAIN MANHOLE | — | DOUBLE SOLID YELLOW LINE |
| ⊙ | SEWER MANHOLE | — | SINGLE SOLID WHITE LINE |
| ⊙ | TELEPHONE MANHOLE | — | SIGN |
| □ | CATCH BASIN | ⊕ | OBSERVATION WELL |
| — | WATER LINE | — | TREELINE |
| — | WATER VALVE | — | CONTOUR ELEVATION |
| — | FIRE HYDRANT | — | GAS LINE |
| — | GAS VALVE | — | MEET EXISTING GRADE |
| — | PROP. CLEAHOUT | — | PROP. SPOT ELEVATION |
| CB-1 | PROP. CATCH BASIN | — | PROP. CONTOUR ELEVATION |
| DMH-1 | PROP. DRAIN MANHOLE | — | PROP. SILT FENCE |
| SMH-1 | PROP. SEWER MANHOLE | T.W. | TOP OF WALL ELEV. |
| | | B.W. | BOTTOM OF WALL ELEV. |
| | | — | PROP. GATE VALVE |



REVISIONS			
NO.	DATE	REV. BY	DESCRIPTION
1	3/7/14	CMT	REV'S PER TOWN REVIEW

SQUARE FEET	ACRES
V# NEW	Store# NEW
Gas Station# NEW	

T.M. CROWLEY & ASSOCIATES

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44 Giles Road, Suite One
Salem, New Hampshire 03079
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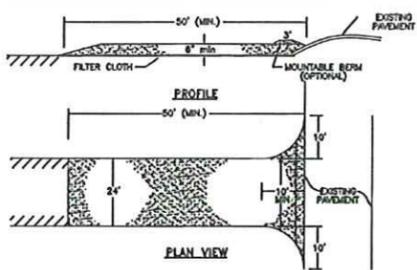
583 NASHUA STREET MILFORD, NH

Cumberland FARMS

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

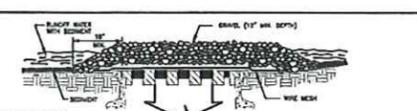
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CHECKED BY: CMT

GRADING & DRAINAGE PLAN CFG5.0



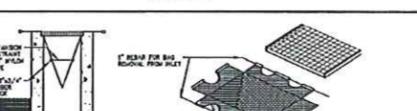
1. STONE FOR A STABILIZED CONSTRUCTION ENTRANCE SHALL BE 1 TO 2 INCH STONE, RECLAIMED STONE, OR RECYCLED CONCRETE EQUIVALENT.
2. THE LENGTH OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 50 FEET, EXCEPT FOR A SINGLE RESIDENTIAL LOT WHERE A 30 FOOT MINIMUM WOULD APPLY.
3. THE THICKNESS OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 8 INCHES.
4. THE WIDTH OF THE ENTRANCE SHALL NOT BE LESS THAN THE FULL WIDTH OF THE ENTRANCE WHERE INGRESS OCCURS OR 10 FEET, WHICHEVER IS GREATER.
5. GEOTEXTILE FILTER CLOTH SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE. FILTER CLOTH IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENCE LOT.
6. ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARD THE CONSTRUCTION ENTRANCE SHALL BE FILTERED THROUGH THE ENTRANCE. IF PAVING IS NECESSARY, A BEDM WITH 6:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PPE.
7. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY WEARABLES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.
8. WHEELS SHALL BE CLEANED TO REMOVE MUD PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

STABILIZED CONSTRUCTION ENTRANCE DETAIL
NOT TO SCALE



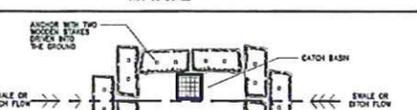
CONSTRUCTION SPECIFICATIONS:
A. FILTERED WATER SHALL BE COLLECTED IN A CATCH BASIN OR OTHER DEVICE THAT IS MAINTAINED AND CLEANED AS NECESSARY. THE CATCH BASIN SHALL BE SIZED TO HOLD THE VOLUME OF WATER THAT IS EXPECTED TO BE COLLECTED. THE CATCH BASIN SHALL BE SIZED TO HOLD THE VOLUME OF WATER THAT IS EXPECTED TO BE COLLECTED. THE CATCH BASIN SHALL BE SIZED TO HOLD THE VOLUME OF WATER THAT IS EXPECTED TO BE COLLECTED.

STONE INLET PROTECTION DETAIL-ON SITE
NOT TO SCALE



CONSTRUCTION SPECIFICATIONS:
A. SILT SACKS SHALL BE PLACED ON A CONCRETE CURB OR OTHER DEVICE THAT IS MAINTAINED AND CLEANED AS NECESSARY. THE CURB SHALL BE SIZED TO HOLD THE VOLUME OF WATER THAT IS EXPECTED TO BE COLLECTED. THE CURB SHALL BE SIZED TO HOLD THE VOLUME OF WATER THAT IS EXPECTED TO BE COLLECTED.

SILT SACK DETAIL-ON OR OFF SITE
NOT TO SCALE



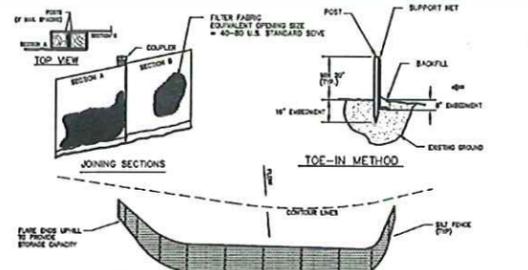
CONSTRUCTION SPECIFICATIONS:
A. THE BARRIER SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY WEARABLES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.

LOW POINT SEDIMENTATION CONTROL BARRIER
NOT TO SCALE



CONSTRUCTION SPECIFICATIONS:
A. THE STOCKPILE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY WEARABLES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.

MATERIALS STOCKPILE DETAIL
NOT TO SCALE



CONSTRUCTION SPECIFICATIONS:
A. THE FENCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY WEARABLES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.

SEDIMENT CONTROL FENCE
NOT TO SCALE

NOTES:
1. WHEN TWO SECTIONS OF FENCE JOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY 8 INCHES, FOLDED AND STAPLED.
2. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGED" ABOVE THE 12 INCH FENCE.
3. ENDS OF THE FENCE SHALL BE PLACED UP CONTOUR OF THE FLOW TO TRAP SEDIMENT.

TEMPORARY EROSION CONTROL MEASURES:

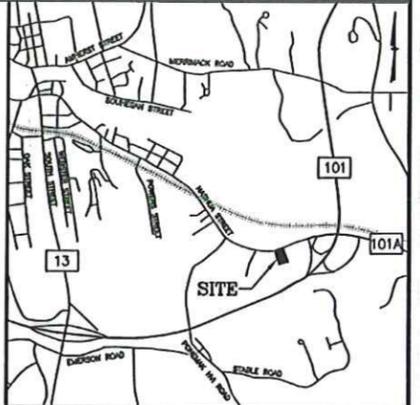
1. THE SMALLEST PRACTICAL AREA OF LAND SHALL BE EXPOSED AT ANY ONE TIME. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE.
2. HAY BALE BARRIERS AND SEDIMENT TRAPS SHALL BE INSTALLED AS REQUIRED. BARRIERS AND TRAPS ARE TO BE MAINTAINED AND CLEANED UNTIL ALL SLOPES HAVE A HEALTHY STAND OF GRASS.
3. BAILED HAY AND MULCH SHALL BE MORNINGS OF ACCEPTABLE HERBACEOUS GROWTH, FREE FROM INSECTS, WEEDS OR WOODY STEMS, AND SHALL BE DRY. NO SALT OR FERTILIZER SHALL BE USED.
4. FILL MATERIAL SHALL BE FREE FROM STUMPS, WOOD, ROCKS, ETC.
5. STOCKPILED MATERIALS SHALL BE PLACED ONLY IN AREAS SHOWN ON THE PLANS. STOCKPILES SHALL BE PROTECTED BY SILTATION FENCE AND MULCH. MULCH SHALL BE PLACED OR EXPOSED OFF SITE.
6. ALL DISTURBED AREAS SHALL BE LOADED AND SEEDING A MINIMUM OF 4 INCHES OF LOAM SHALL BE INSTALLED AND SEEDING AS SPECIFIED.
7. AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED THE TEMPORARY EROSION CONTROL MEASURES ARE TO BE REMOVED.
8. PAVED ROADWAYS MUST BE KEPT CLEAN AT ALL TIMES.
9. ALL CATCH BASIN INLETS WILL BE PROTECTED WITH LOW POINT SEDIMENTATION BARRIER.
10. ALL STORM DRAINAGE OUTLETS WILL BE STABILIZED AND CLEANED AS REQUIRED, BEFORE THE DISCHARGE POINTS BECOME OPERATIONAL.
11. ALL EXCAVATION OPERATIONS MUST DISCHARGE DIRECTLY INTO A SEDIMENT FILTER AREA.
12. JUTE MATTING OR APPROVED EQUIPMENT SHALL BE PROVIDED ON ALL SLOPES GREATER THAN 3:1.

CONSTRUCTION SEQUENCE NOTES:

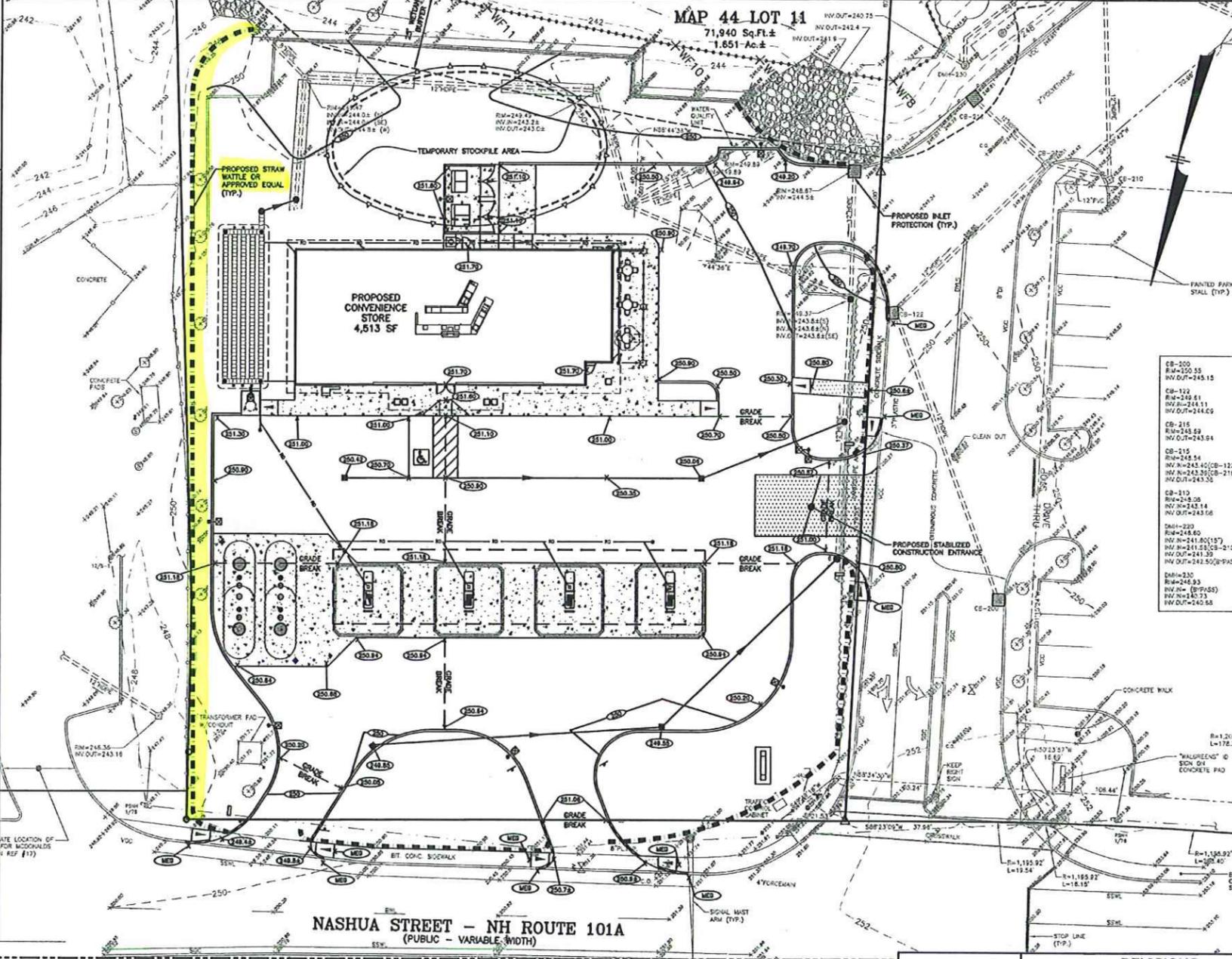
1. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY ON-SITE CONSTRUCTION AS SHOWN. ADDITIONAL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED AS SOON AS PRACTICAL.
2. CONSTRUCT TEMPORARY STABILIZED CONSTRUCTION ENTRANCE AS SHOWN ON THIS SHEET.
3. CUT AND STUMP AREAS OF PROPOSED CONSTRUCTION.
4. REMOVE AND STOCKPILE TOPSOIL. STOCKPILE SHALL BE SEEDING TO PREVENT EROSION.
5. CONSTRUCT ROADWAYS AND PERFORM SITE GRADING, PLACING HAY BALES AND SILTATION FENCES AS REQUIRED TO CONTROL SOIL EROSION. STABILIZE ROAD, PARKING LOTS AND CUT/FILL SLOPES WITHIN 72-HOURS OF ACHIEVING FINISH GRADES.
6. INSTALL UNDERGROUND UTILITIES AND DRAINAGE SYSTEM.
7. BEGIN TEMPORARY AND PERMANENT SEEDING AND MULCHING. ALL CUT AND FILL SLOPES SHALL BE SEEDING OR MULCHING WITHIN 72-HOURS OF ACHIEVING FINISH GRADES.
8. DAILY, OR AS REQUIRED, CONSTRUCT, INSPECT, AND IF NECESSARY, RECONSTRUCT TEMPORARY BERM, DRAIN, DITCHES, SILT FENCES AND SEDIMENT TRAPS INCLUDING MULCHING AND SEEDING. AT A MINIMUM, INSPECT EROSION CONTROLS WEEKLY AND AFTER EVERY 1/2" OF RAINFALL. BEGIN EXCAVATION FOR CONSTRUCTION OF BUILDINGS.
9. FINISH PAVING ALL ROADWAYS AND DRIVEWAYS.
10. COMPLETE PERMANENT SEEDING AND LANDSCAPING.
11. AFTER GRADING HAS BEEN FULLY COMPLETED IN ALL SEEDED AREAS, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.
12. APPLICATION OF GRASS SEED, FERTILIZERS AND MULCH SHALL BE ACCOMPLISHED BY BROADCAST SEEDING OR HYDROSEEDING AT THE RATES OUTLINED BELOW:
Location: 150 lbs./1,000 square feet
Fertilizer: 13-18 lbs./1,000 sq. ft.
Mulch: 4-6 inches approximately 3 tons/acre unless erosion control matting is used.
Permanent Seed Mix
Cresting Seed Mix
Winter Mix (Nov. 15 - Sept. 15)
Date (No later than May 15)
TOTAL: 112
13. NEWLY GRADED AREAS REQUIRING SLOPE PROTECTION OUTSIDE OF NORMAL SEEDING SEASON SHALL RECEIVE HAY MULCH AT THE APPROXIMATE RATE OF 100 MORE THAN 3 TONS PER ACRE.
14. THE CONTRACTOR AND DEVELOPER MUST MANAGE THE PROJECT TO MEET THE REQUIREMENTS AND INTENT OF RSA 430:53 AND ACR 3800 RELATIVE TO INVASIVE SPECIES.
15. FUGITIVE DUST MUST BE CONTROLLED IN ACCORDANCE WITH EMV-A 1000.

NOTES:

- 1) SEE DETAIL SHEETS FOR ADDITIONAL NOTES, DETAILS AND EROSION CONTROL INFORMATION.

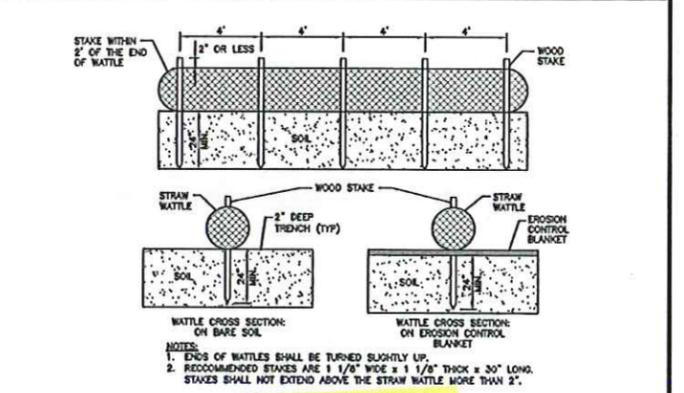


LOCATION MAP
NOT TO SCALE



EROSION CONTROL NOTES:

1. THE EROSION CONTROL PROCEDURES SHALL CONFORM TO THE NH STORMWATER MANUAL, VOLUME 3, EROSION & SEDIMENT CONTROLS DURING CONSTRUCTION, DECEMBER 2008.
2. DURING CONSTRUCTION AND THROUGHOUT EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED. THE SMALLEST PRACTICAL AREA OF LAND SHOULD BE EXPOSED AT ANY ONE TIME DURING DEVELOPMENT. WHEN LAND IS EXPOSED DURING DEVELOPMENT, THE EXPOSURE SHOULD BE KEPT TO THE SHORTEST PRACTICAL PERIOD OF TIME AS APPROVED BY THE ENGINEER. LAND SHOULD NOT BE LEFT EXPOSED DURING THE WINTER MONTHS.
3. LIMIT OF MAXIMUM AREA OF EXPOSED SOIL AT ANY ONE TIME TO LESS THAN 3 ACRES. THE EXPOSED AREA THAT IS BEING ACTIVELY WORKED DURING WINTER IS TO BE LESS THAN 3 ACRES DURING THE WINTER SEASON.
4. ALL PERMANENT STORM WATER STRUCTURES SHALL BE STABILIZED PRIOR TO DIRECTING FLOW INTO THEM. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:
A) BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED.
B) A MINIMUM OF 65 PERCENT VEGETATED GROWTH HAS BEEN ESTABLISHED.
C) A MINIMUM OF 3 INCHES OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIP-RAP HAS BEEN INSTALLED.
D) OR, EROSION CONTROL BARRIERS HAVE BEEN PROPERLY INSTALLED.
5. SILT FENCE SHALL BE INSTALLED AND MAINTAINED DURING AND AFTER DEVELOPMENT TO REMOVE SEDIMENT FROM RUNOFF WATER AND FROM LAND UNDERGOING DEVELOPMENT. WHERE POSSIBLE, NATURAL BARRIERS SHOULD BE UTILIZED AND LEFT OPEN TO REMOVE EXCESS SURFACE WATER. SILT FENCE TO BE MAINTAINED AND CLEANED UNTIL ALL SLOPES HAVE A HEALTHY STAND OF GRASS.
6. ALL DISTURBED AREAS AND SLOPES WHICH ARE FINISHED GRADING, WITH NO FURTHER CONSTRUCTION TO TAKE PLACE, SHALL BE LOADED AND SEEDING WITHIN 72 HOURS AFTER FINAL GRADING. A MINIMUM OF 4" OF LOAM SHALL BE INSTALLED WITH NOT LESS THAN ONE POUND OF SEED PER 50 SQUARE YARDS OF AREA. THE SEED MIX SHALL BE AS DESIGNATED BELOW.
7. ANY DISTURBED AREAS WHICH ARE TO BE LEFT TEMPORARILY, AND WHICH WILL BE REGRADED LATER DURING CONSTRUCTION SHALL BE MACHINE HAY MULCHED AND SEEDING WITH RYE GRASS TO PREVENT EROSION. THE MAXIMUM LENGTH OF TIME FOR THE EXPOSURE OF DISTURBED SOILS SHALL BE 45 DAYS. HAY OR STRAW MULCH SHALL BE APPLIED TO ALL FRESHLY SEEDING AREAS AT THE RATE OF 3 TONS PER ACRE. BALES SHALL BE UNFOLDED, AIR DRIED, AND FREE FROM WEED, SEEDS AND ANY COARSE MATERIAL.
8. DURING GRADING OPERATIONS INSTALL HAY BALE BARRIERS ALONG TOE OF SLOPE OF FILL AREAS WHERE SHOWN. BARRIERS ARE TO BE MAINTAINED UNTIL DISTURBED AREAS ARE PAVED OR GRADED.
9. THE FILL MATERIAL SHALL BE OF APPROVED SOIL TYPE FREE FROM STUMPS, ROOTS, WOOD, ETC. TO BE PLACED IN 14" LIFTS OR AS SPECIFIED. BALDOZERS, TRUCKS, TRACTORS, OR ROLLERS MAY BE USED FOR COMPACTION BY ROUTING THE EQUIPMENT TO ALL AREAS FOR EACH LAYER.
10. AVOID THE USE OF FUTURE OPEN SPACES (LOAM & SEED) WHEREVER POSSIBLE DURING CONSTRUCTION. CONSTRUCTION TRAFFIC SHALL USE THE ROADWAYS OF FUTURE ROADS.



CONSTRUCTION SPECIFICATIONS:
A. THE WATTLE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY WEARABLES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED PROMPTLY.

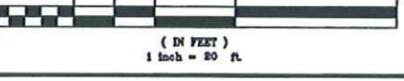
HAY WATTLE DETAIL
NOT TO SCALE



LEGEND

○	UTILITY POLE	---	OVERHEAD SERVICE WIRES	⊕	WATER VALVE	—○—	PROP. CONTOUR ELEVATION
○	DRAIN MANHOLE	---	DOUBLE SOLID YELLOW LINE	⊕	FIRE HYDRANT	—○—	PROP. SILT FENCE
○	SEWER MANHOLE	---	SINGLE SOLID WHITE LINE	⊕	GAS VALVE	—○—	PROP. CLEANOUT
○	TELEPHONE MANHOLE	---	SIGN	---	CONTOUR ELEVATION	CB-1	PROP. CATCH BASIN
□	CATCH BASIN	---	TREELINE	---	GAS LINE	DM-1	PROP. DRAIN MANHOLE
---	WATER LINE	---		---	PROP. STRAW WATTLE	SM-1	PROP. SEWER MANHOLE

GRAPHIC SCALE



NO.	DATE	REV. BY	DESCRIPTION
1	3/7/14	CMT	REV'S PER TOWN REVIEW

SQUARE FEET	
ACRES	
V# NEW	
Store# NEW	
Gas Station# NEW	

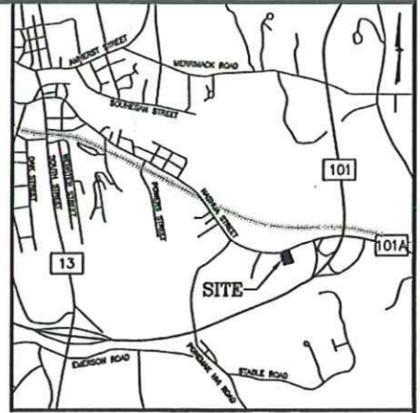
583 NASHUA STREET MILFORD, NH

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

EROSION CONTROL PLAN CFG6.0

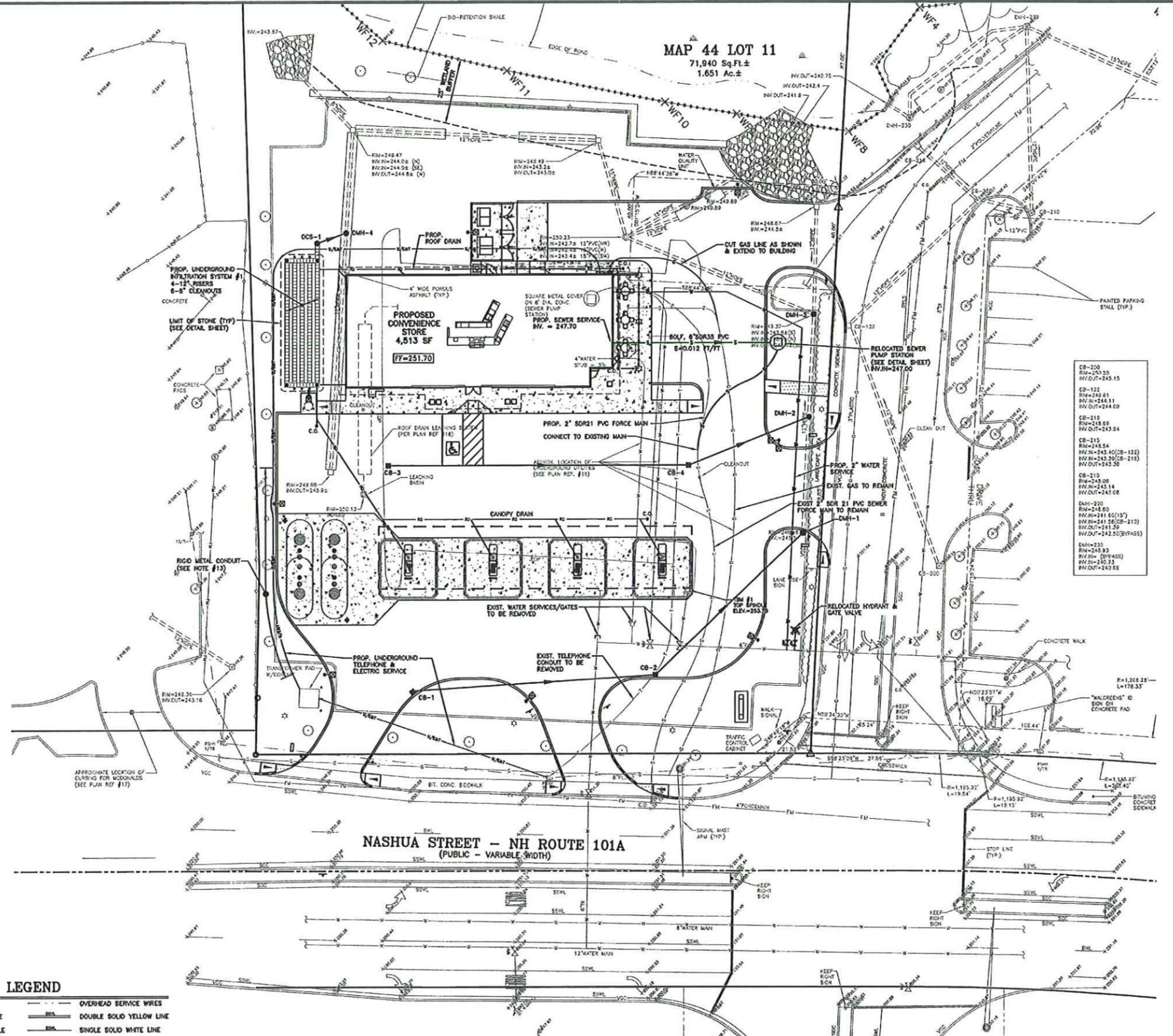
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DATE: JANUARY 20, 2014
FILE: 3422SP.dwg
DRAWN BY: NFM
CHECKED BY: CMT

MAP 44 LOT 11
71,940 Sq. Ft. ±
1.651 Ac. ±



LOCATION MAP
(NOT TO SCALE)

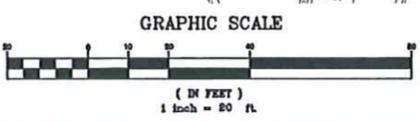
- NOTES:**
- 1) ALL SANITARY SEWER PIPE SHALL BE PVC (50R-35), UNLESS OTHERWISE NOTED.
 - 2) ALL WATER PIPE SHALL BE COPPER (TYPE K), UNLESS OTHERWISE NOTED.
 - 3) ELEVATIONS ARE BASED ON MVDG 1929 DATUM.
 - 4) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
 - 5) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER OF ANY DISCREPANCIES.
 - 6) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
 - 7) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
 - 8) ALL WATER, SEWER AND DRAINAGE CONSTRUCTION SHALL CONFORM TO THE TOWN OF MILFORD WATER UTILITIES DEPARTMENT AND PUBLIC WORKS DEPARTMENT STANDARDS AND SPECIFICATIONS.
 - 9) ALL ELECTRIC, TELEPHONE AND CABLE TV LINES ARE TO BE UNDERGROUND AND INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY CO. SPECIFICATIONS.
 - 10) ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY UTILITY COMPANY AND LOCAL DPW.
 - 11) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO NHDOT AND MILFORD DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
 - 12) SEE GRADING & DRAINAGE PLAN FOR DETAILED DRAINAGE INFORMATION.
 - 13) ELECTRICAL CONDUIT WITHIN 20' OF TANKS OR DISPENSERS MAY NEED TO BE RIGID METAL CONDUIT WITH CONCRETE ENCASUREMENT. CONTRACTOR TO COORDINATE WITH UTILITY COMPANY AND/OR TOWN ELECTRICAL INSPECTOR AS REQUIRED.



CB-200	R/W=250.55	INV./OUT.=245.15
CB-122	R/W=248.61	INV./OUT.=244.11
CB-116	R/W=248.69	INV./OUT.=243.64
CB-215	R/W=248.54	INV./OUT.=243.40(28-122)
CB-210	R/W=248.08	INV./OUT.=243.39(28-210)
CB-211	R/W=248.14	INV./OUT.=243.08
DMH-220	R/W=248.00	INV./OUT.=241.60(15')
DMH-218	R/W=248.00	INV./OUT.=241.56(28-210)
DMH-219	R/W=248.00	INV./OUT.=241.39
DMH-220	R/W=248.00	INV./OUT.=241.50(BYPASS)
DMH-230	R/W=248.93	INV./OUT.=248.40(30)
DMH-231	R/W=248.93	INV./OUT.=248.73
DMH-232	R/W=248.93	INV./OUT.=249.06

LEGEND

○	UTILITY POLE	—	OVERHEAD SERVICE WIRES
⊙	DRAIN MANHOLE	—	DOUBLE SOLID YELLOW LINE
⊙	SEWER MANHOLE	—	SINGLE SOLID WHITE LINE
⊙	TELEPHONE MANHOLE	—	SIGN
□	CATCH BASIN	⊕	OBSERVATION WELL
—	WATER LINE	—	TREELINE
—	WATER VALVE	—	CONTOUR ELEVATION
—	FIRE HYDRANT	—	GAS LINE
—	GAS VALVE	—	PROP. GATE VALVE
—	PROP. CLEANOUT		
CB-1	PROP. CATCH BASIN		
DMH-1	PROP. DRAIN MANHOLE		
SMH-1	PROP. SEWER MANHOLE		



REVISIONS		SQUARE FEET
NO.	DATE	DESCRIPTION
1	3/7/14	CMT
		REV'S PER TOWN REVIEW

V# NEW	SQUARE FEET
Store# NEW	ACRES
Gas Station# NEW	

CROWLEY & ASSOCIATES

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583 NASHUA STREET
MILFORD, NH

Cumberland FARMS

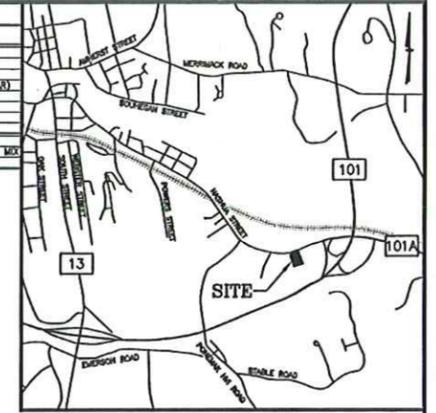
CUMBERLAND FARMS INC.
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FRAMINGHAM, MA 01702

SCALE: 1"=20'
DATE: JANUARY 20, 2014
FILE: 3422SP.dwg
DRAWN BY: NFM
CHECKED BY: CMT

UTILITIES PLAN
CFG7.0

MAP 44 LOT 11
71,940 Sq.Ft.±
1.651 Ac.±

PLANT QNTY	BOTANICAL NAME	COMMON NAME	MIN. INSTAL. SIZE	MATURE SIZE/REMARKS
SHRUBS				
8	LAMPROLIS CHINENSIS "SEAGREEN"	SEAGREEN JAMPER	18"-24" HT.	4'-8" HT. MAX./FOURTH LIKE HAWK
16	BURUS MICROPHYLLA CULTIVAR	BURWOOD	24" HT. MIN.	3" HT. MAX./GREEN VELVET OR SIMILAR
7	CHAMAECYPARIS PRESERA CULTIVAR	ORNY THROATLEAF FALSEDYPRESS	18"-24" SPREAD	2" HT. MAX./GOLDEN-WIDE SPREADING (TILIFERA ARUBA-NANA OR SIMILAR)
18	ERIOCHLOA GRASS COMPACTA	PAN ERIODORISER COMPACT	24" HT. MIN.	1/2 COMPACT BUSHY BURNING FRAGR
34	THELA OCCIDENTALIS TERRA	DARK AMERICAN ASSOCIATE	6'-8" HT.	3/4 HT. MAX./LARGE, UPRIGHT/SCREEN/HEDGE
PERENNIALS & GRASSES				
70	HIMERICALLIS "STELLA D'ORO"	DIARY YELLOW GAYLARD	1 GAL.	18" O.C./GROUNDCOVER (STELLA D'ORO, RUBY STELLA, STELLA SUPREME MO)

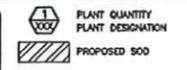
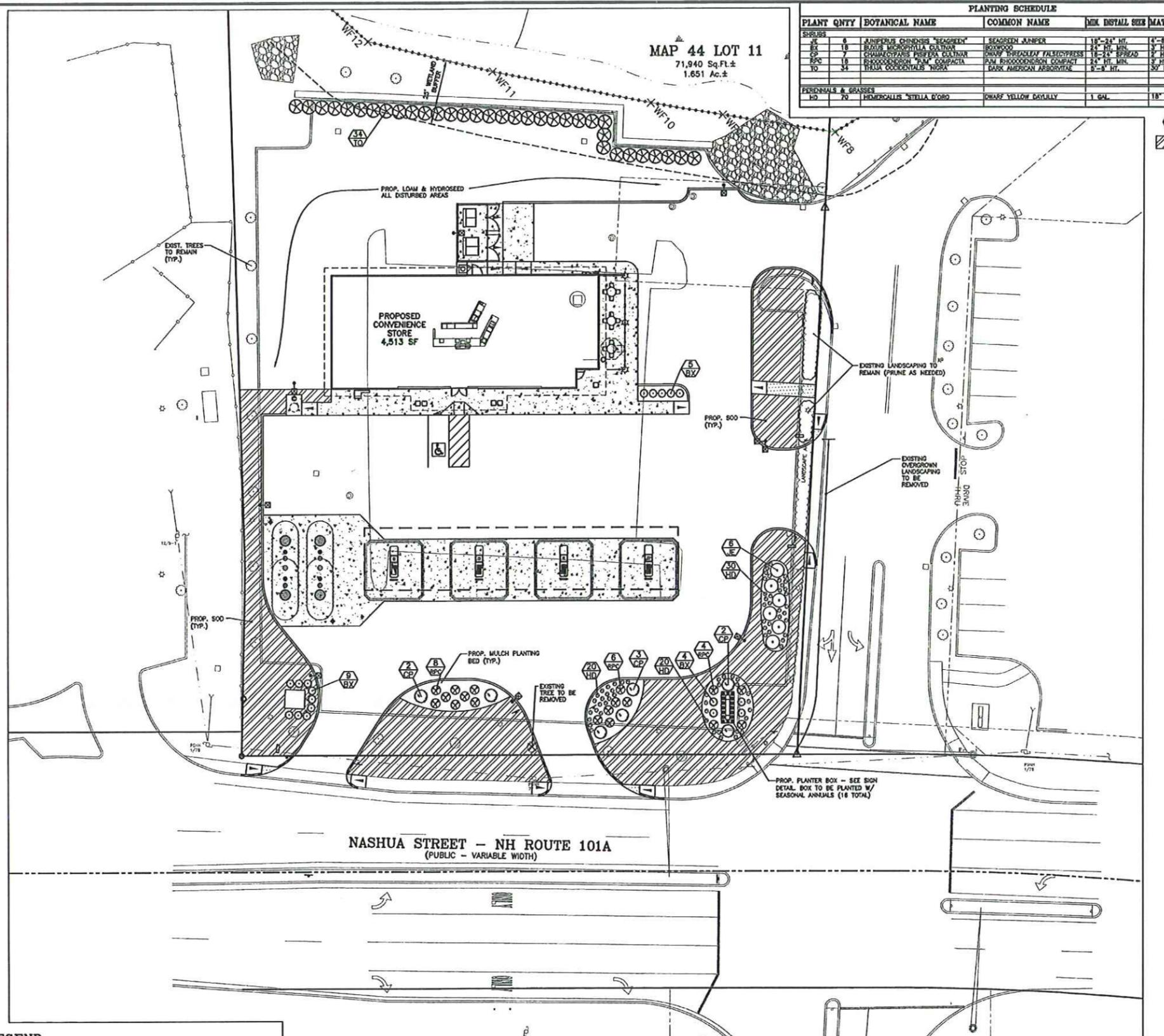


LOCATION MAP
(NOT TO SCALE)

- NOTES:**
- ALL PLANT STOCK SHALL CONFORM TO ANSI Z260.1 - NURSERY STOCK, LATEST EDITION (AMERICAN ASSOCIATION OF NURSERMEN, INC.)
 - 4" AGED PINEBARK MULCH AND A WEED BARRIER (TY-PAR FABRIC OR APPROVED EQUAL) SHALL BE APPLIED TO ALL SHRUB AND GROUNDCOVER BEDS. INSTALL WEED BARRIER AS PER MANUFACTURERS RECOMMENDATIONS.
 - PLANT PIT BACKFILL SHALL BE MIXED AT A RATE OF 7 PARTS OF TOPSOIL TO 2 PARTS OF DEHYDRATED COW MANURE. SLOW RELEASE FERTILIZER SHALL BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS. USE EXISTING ON-SITE TOPSOIL AS PART OF BACKFILL WHEN AVAILABLE.
 - ALL LANDSCAPED AREAS NOT PLANTED WITH TREES, SHRUBS OR GROUNDCOVER SHALL BE RESTORED WITH SEED OR SOO AS INDICATED ON PLANS.
 - ALL SOO, SEED, SHRUB AND TREE AREAS SHALL RECEIVE 6" PH CORRECTED TOPSOIL. AFTER TOPSOIL IS SPREAD EVENLY OVER ENTIRE AREA, ALL CLODS, LUMPS, STONES AND OTHER DELETERIOUS MATERIAL SHALL BE RAKED UP AND REMOVED.
 - APPLICATION OF GRASS SEED, FERTILIZERS AND MULCH SHALL BE ACCOMPLISHED BY BROADCAST SEEDING OR HYDROSEEDING AT THE RATES OUTLINED BELOW:
 LIMESTONE: 100 LBS./1,000 SQUARE FEET.
 FERTILIZER: 500 LBS./ACRE OF 10-20-20 OR 1000 LBS./ACRE OF 8-10-10.
 MULCH: HAY MULCH APPROXIMATELY 3 TONS/ACRE

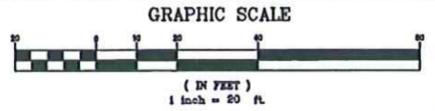
SEED MIX (SLOPES LESS THAN 4:1)	LBS./ACRE
CREeping RED FESCUE	20
TALL FESCUE	15
PERENNIAL RYEGRASS	5
	2
	42

SLOPE MIX (SLOPES GREATER THAN 4:1)	LBS./ACRE
CREeping RED FESCUE	20
TALL FESCUE	20
BIRDFOOT TREEFOIL	48
 - FOR TEMPORARY EROSION CONTROL NOTES, SEE EROSION & SEDIMENT CONTROL PLAN.
 - NEWLY GRADED AREAS REQUIRING SLOPE PROTECTION OUTSIDE OF NORMAL SEEDING SEASON SHALL RECEIVE STRAW MULCH AT THE APPROXIMATE RATE OF NO MORE THAN 3 TONS PER ACRE.
 - ANY CHANGES IN PLANT LOCATIONS OR TYPES SHALL BE APPROVED BY THE DEVELOPER AND TOWN PRIOR TO INSTALLATION.
 - PLANTINGS SHALL BE GUARANTEED BY THE CONTRACTOR FOR ONE YEAR AFTER WRITTEN ACCEPTANCE OF THE DEVELOPER.
 - EXPOSED SOILS SHALL BE SEEDED OR HAY MULCHED WITHIN 72 HOURS OF FINAL GRADING.
 - ALL WORK SHALL BE COORDINATED WITH APPLICABLE EPA NPDES/SWPPP PERMIT WORK AS REQUIRED.
 - THE CONTRACTOR SHALL INSTALL AN IRRIGATION SYSTEM TO PROVIDE COMPLETE COVERAGE OF ALL SEED, SOO AREAS AND SHRUB BEDS. THE SYSTEM SHALL INCLUDE A TIMER WITH RAIN SENSOR AND SHALL BE INSTALLED IN ACCORDANCE WITH LOCAL CODES.



LEGEND

- | | | | |
|---|-------------------|---------|--------------------------|
| ○ | UTILITY POLE | — — — | OVERHEAD SERVICE WIRES |
| ⊙ | DRAIN MANHOLE | == | DOUBLE SOLID YELLOW LINE |
| ⊙ | SEWER MANHOLE | --- | SINGLE SOLID WHITE LINE |
| ⊙ | TELEPHONE MANHOLE | + | SIGN |
| □ | CATCH BASIN | ⊕ | OBSERVATION WELL |
| — | WATER LINE | ~ | TREELINE |
| ⊕ | WATER VALVE | — 150 — | CONTOUR ELEVATION |
| ⊕ | FIRE HYDRANT | — | GAS LINE |
| ⊕ | GAS VALVE | ⊙ | NUMBER OF PARKING SPACES |



REVISIONS		
NO.	DATE	REV. BY: DESCRIPTION
1	3/7/14	CMT REV'S PER TOWN REVIEW

SQUARE FEET
ACRES
V# NEW
Store# NEW
Gas Station# NEW

44 Siles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

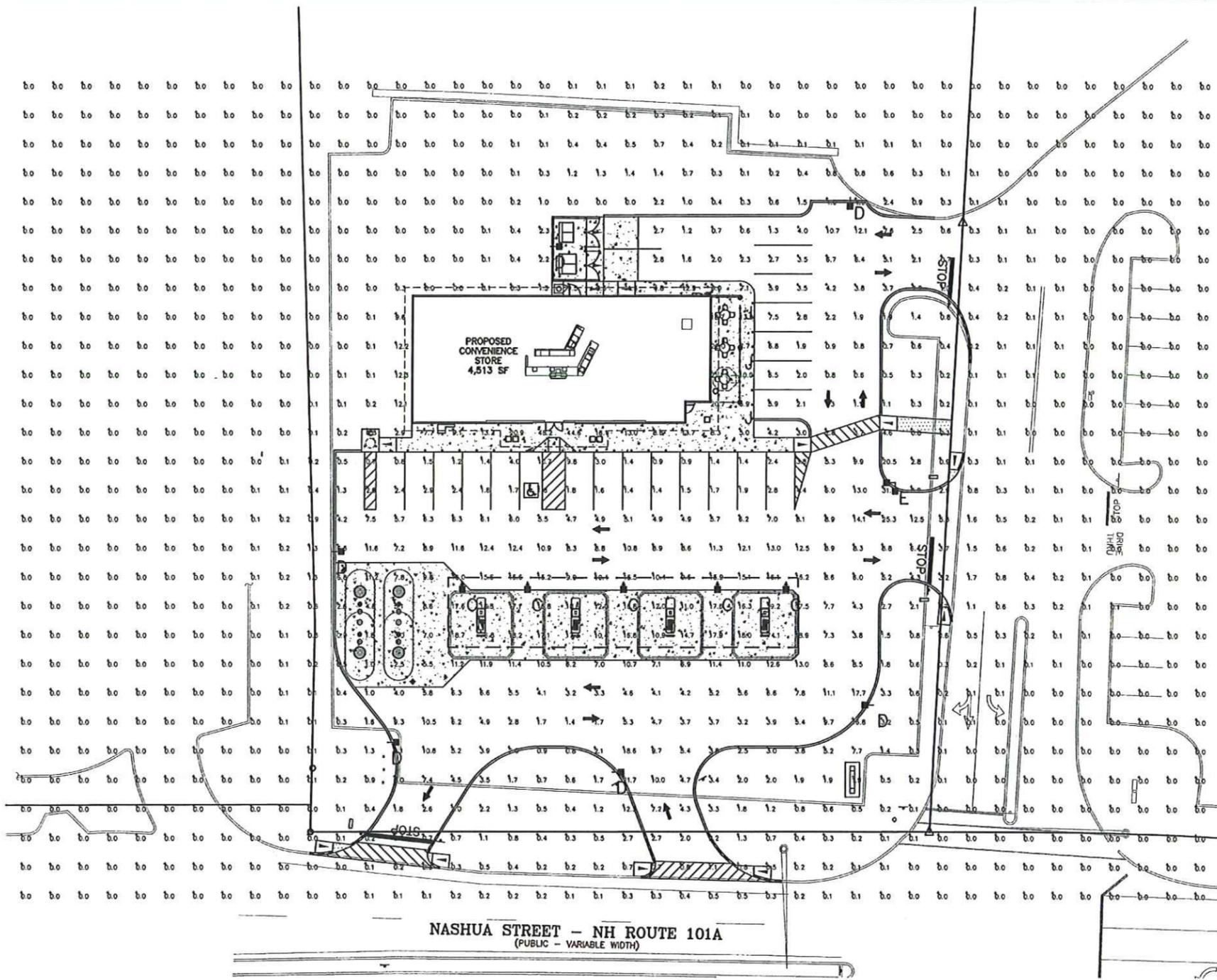
583 NASHUA STREET
MILFORD, NH

Cumberland Farms Inc.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

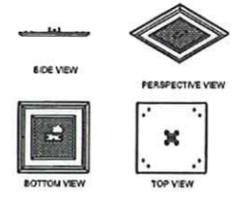
LANDSCAPE PLAN
CFG8.0

SCALE: 1"=20'
DATE: JANUARY 29, 2014
FILE: 3422SP.dwg
DRAWN BY: NFM
CHECKED BY: CMT

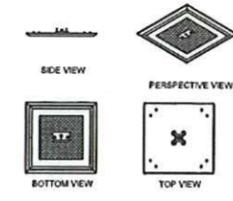
F:\Projects\34221\34221.dwg LA 2/07/14 10:58am nfm



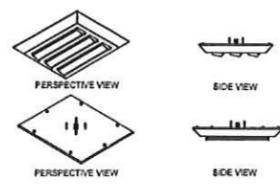
CRS-SC-LED-84
LED Crossover Gold Symmetric Canopy Light



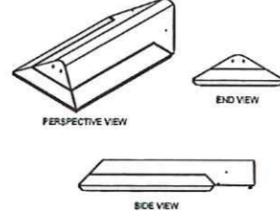
CRS-AC-LED-84
LED Crossover Gold Asymmetric Canopy Light



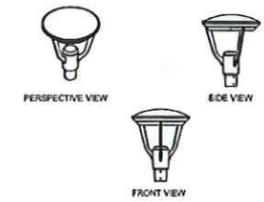
CRO3-FO
LED Crossover Focus Light



XAM
LED Crossover Area Light



XENM3 PT
LED Crossover Area Light



Initial Footcandle levels at grade.

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Calc/Pls	Illuminance	Fc	2.31	76.8	0.0	N.A.	N.A.
PUMP VERTICAL	Illuminance	Fc	48.93	89.9	18.8	2.60	4.78
CANOPY	Illuminance	Fc	20.50	76.8	10.4	1.97	7.38
INTERIOR STOREFRONT	Illuminance	Fc	6.43	31.5	0.5	12.66	63.00

NOTE: ALL 14' POLES TO BE MOUNTED ON CONCRETE PEDESTALS 2' ABOVE GRADE, FOR A TOTAL MOUNTING HEIGHT OF 16'.

Luminaire Schedule

Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Arr. Lum. Lumens	Arr. Watts
▶	8	A	SINGLE	CRO3-FO-LED-30-CW-UE	1.000	N.A.	2674	36.1
▶	5	B	SINGLE	CRS-SC-LED-84-SS-CW-UE	1.000	N.A.	10747	97
▶	5	C	SINGLE	CRS-AC-LED-84-SS-CW-UE	1.000	N.A.	10367	98
▶	6	D	SINGLE	XAMU-FT-LED-128-SS-CW-UE-S-14'POLE+4"BASE	1.000	N.A.	14088	136
▶	1	E	2 @ 90 DEGREES	XAMU-FT-LED-128-SS-CW-UE-D90-14'POLE+4"BASE	1.000	N.A.	28176	272
●	26	G	SINGLE	OM6LED27-R6LED40KWCS (FIXTURE BY OTHER)	1.000	N.A.	1738	27
●	3	J	SINGLE	XENM3-PT-5-LED-63-450-CW-UE-7'MH	1.000	N.A.	5027	96.6

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.
This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with the Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaire may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted.

luminaire
American Innovation Through Technology

LIGHTING PROPOSAL **LO-119127-1**
LUMBERLAND FARMS
583 NASHUA ST
MILFORD, NH

DATE: 3/7/14
SCALE: 1"=20'

March 12, 2014

MEMO

TO: Planning Board
Jodie Levandowski, Town Planner/GIS Coordinator

FROM: Bill Parker, Community Development Director *BP*

**RE: Nashua Street Corridor Improvements Fund/Proposed Cumberland Farms
(Map 44/Lot 11)**

The Nashua Street Corridor Improvements Fund was established in December 2000 as a result of a condition of approval for the Ledgewood Estates 55+ development. At that time it was my recommendation to the Planning Board that a mechanism be established to obtain contributions from developers with projects directly impacting Nashua Street traffic conditions and operations. The fund was (and still is) earmarked to include, but not be limited to, roadway design and engineering, road widening, property acquisition, signalization, design, and related corridor traffic mitigation needs. Sidewalk construction has been included for proposed improvements.

For the Ledgewood Estates project, the basis of the fee was based on 'peak hour traffic generation per unit'. The peak hour generation per unit rate for senior/assisted living units, according to the traffic study done for the project by Stephen Pernaw, was 0.45 trips per unit.

At the time of approval I suggested a contribution of \$100/peak hour trip, which translated into a \$45/unit contribution for each unit in Ledgewood Estates. With 189 units, the total contribution was \$8,505.00 (0.45 peak hour trips/unit x \$100/peak hour trip = \$45. \$45 x 189 units = \$8,505.00). The contribution was collected at the time the certificate of occupancy was granted.

This formula was subsequently applied to these **commercial residential** projects impacting the Nashua Street corridor:

Stonehouse Condominiums (21 units)	\$ 945.00
Charles Street Commons (30 units)	\$ 2,979.00
Cahill Place (49 units)	\$ 2,205.00
Quarrywood Green (66 units)	\$ 2,970.00

Two **commercial** projects on Nashua Street also were subject to contributions to the Fund, specifically for sidewalk plans and construction. The calculation was based on a cost/linear foot for sidewalk construction (\$10.78/LF). Contributions were:

Giorgio's (Vizas Realty)	\$ 3,925.00
Nashua Eye Associates	\$ 2,545.00

Two additional contributions were made to this fund. Three firms from the Powers Street industrial area (Hy-Ten, Spear, and St. Gobain) requested that a signalization study be conducted to look at their concerns with the Powers Street/Nashua Street intersection. Also, the Stabile Companies were asked by the Planning Board to contribute to the anticipated Nashua Street/Ponemah Hill Road connection.

Powers Street Industries	\$ 2,320.00
Stabile Companies	\$10,000.00

Actual sidewalk and roadway modifications along Nashua Street frontages were included in the site plan approvals for Richmond Plaza/McDonald's parcel; the so-called '99 Restaurant' parcel, the Walgreen's parcel, and Quarrywood Green on Ponemah Hill Road.

The above collected contributions have been spent to offset the cost of a Nashua Street corridor improvement plan and signalization study (Stephen Pernaw) and fully engineered plans for the Nashua Street/Ponemah Hill Road signalization project and the Nashua Street Sidewalk Project. The last collection of contributions was in 2006 from Nashua Eye Associates. Since that time there has been no development proposed in the corridor that would necessitate a contribution.

Proposed Contribution from Cumberland Farms Development (Map 44/Lot 11)

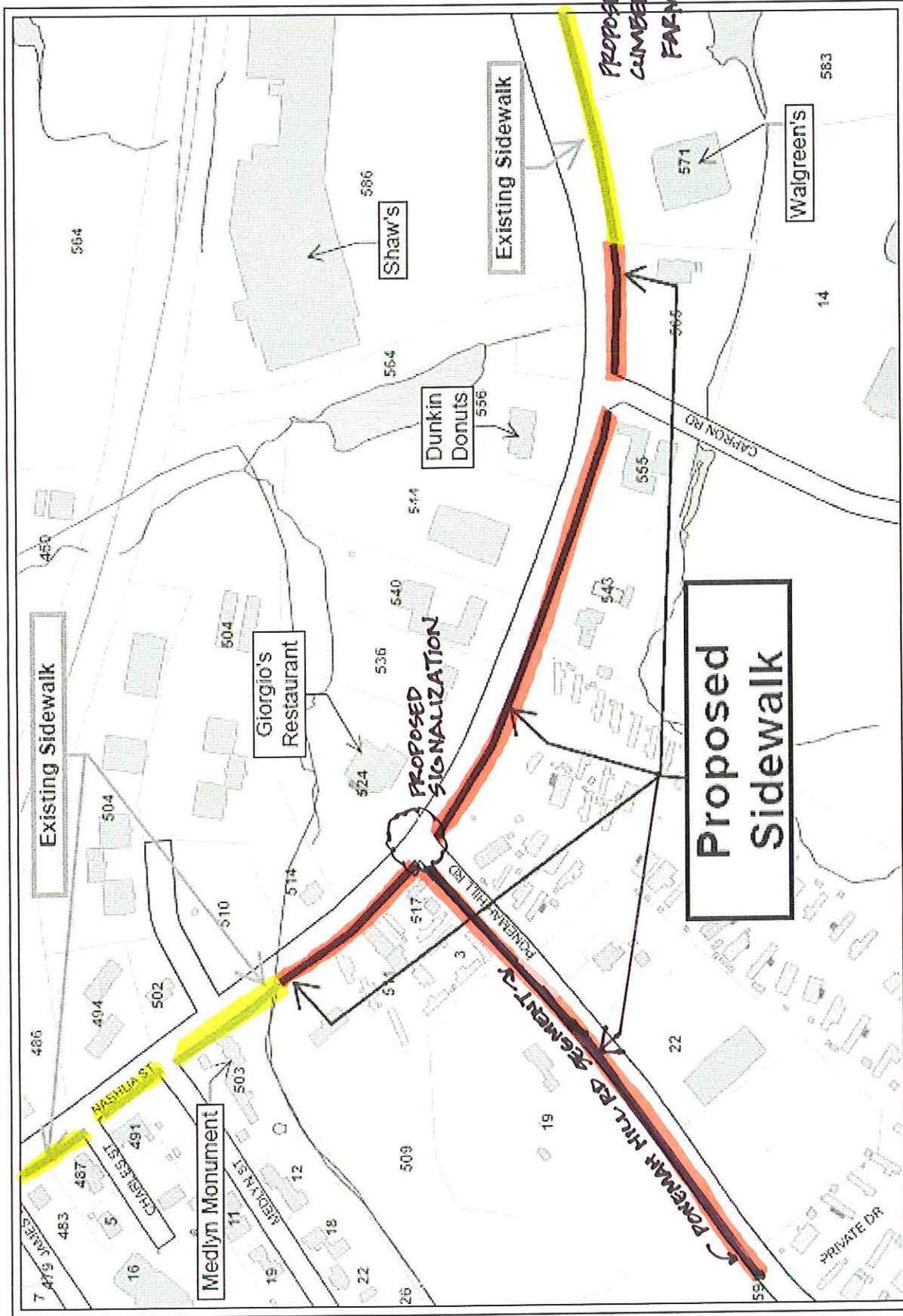
Sidewalks on Nashua Street were required as part of the site development of the Walgreen's site (44/13-1) and the proposed '99' Restaurant (44/11). Cumberland Farms is now proposing the construction of a gas station/convenience store on 44/11. If sidewalk did not exist at the location it is reasonable to expect that sidewalk construction would be a requirement of site plan approval as a planned segment of the Nashua Street sidewalk project. Using a current estimate provided by Public Works Director Rick Riendeau of \$14/LF for sidewalk construction the cost of a 216.13 LF of sidewalk (the frontage on Nashua Street) would be \$3,025.82.

A staff level meeting with the developer's engineers was held on Monday, March 3rd to review and reach consensus on recommendations to the Planning Board on access to the site. At the close of this meeting, the representatives stated that Cumberland Farms is agreeable to contributing \$15,000.00 to be placed in the Nashua Street Corridor Improvement Fund. It is the recommendation from this Office that this contribution be graciously accepted by the Planning Board and so noted in the Board's approval along with recommendations the Board may have as to how the contribution should be utilized.

cc: Guy Scaife, Town Administrator

Attach: Map of Sidewalk Project Area

NASHUA STREET SIDEWALK CONSTRUCTION PROJECT



Milford, NH
This map was produced from a GIS file developed by the Milford Planning Department. The Town of Milford is not responsible for the accuracy of the data or the map.

QUARRY-
WOOD
GREEN

3/12/14 OGD



TOWN OF MILFORD, NH

COMMUNITY DEVELOPMENT DEPARTMENT
1 UNION SQUARE, MILFORD, NH 03055

TEL: (603)249-0620
www.milford.nh.gov

STAFF MEMO

Date: March 18, 2014
To: Town of Milford Planning Board
From: Jodie Levandowski, Town Planner
Subject: **Carol Colburn – Osgood Rd & Woodhawk Dr – Map 51, Lot 1;**
Major open space subdivision creating twenty-seven (27) new residential lots.
(Tabled from 2/25/14 meeting)

BACKGROUND:

The applicant is back before the Planning Board to continue with an application to subdivide map 51 lot 1 into 27 open space residential lots, with a through road connecting to Woodhawk Dr and one cul-de-sac. The subdivision is located within the Residence “R” District and in accordance with Section 5.04.1 of the Milford Zoning Ordinance, single-family dwellings and its accessory structures are acceptable uses within this District.

At the February 25, 2014 Planning Board Meeting the Board tabled the application until the March 18, 2014 meeting, to allow time for the applicant to complete a response to the peer review comments from CEI and any plan revisions. Since that time the applicant and their engineers have received copies of the peer review comments from CEI, dated February 11, 2014 and are working to revise and compile necessary information for submittal.

(See attached staff memo from November 19, 2013 and peer review comments from CEI)

STAFF RECCOMENDATIONS:

As of March 13th Staff has not received copies of the revised plans based on CEI and 11/19/2013 staff comments. The applicant should bring revised plans to the meeting which the Board can review and discuss. However, no actions can be made as staff and interdepartmental reviews have not been completed. The Board should hear a presentation from the applicant and present any questions or concerns at the meeting.

Staff recommends the Board table the application to April to allow time for Staff and Department Heads to review a complete plan set and comment. At this time, Staff does not have clear information on the extent of revisions made since the original November 2013 submission.



STAFF MEMO

Planning Board Meeting

November 19, 2013

Agenda Item #3:– Carole M Colburn Revocable Trust- Osgood Rd/Woodhawk Dr- Map 51 Lot 1;

Public Hearing for a Major Open Space Subdivision Creating Twenty-Seven (27) New Residential Lots.

HISTORY:

The Planning Board will likely recognize this plan from the Design Review phase for an application to subdivide the parcel into 32 open space residential lots, with a through road connecting to Woodhawk Dr and one cul-de-sac. That application made it through Design Review phase in February of 2007, but never returned for Final Application as the economy stalled and the money for outside engineering review was not available. Provided below, is a timeline prepared for the project beginning in December of 2006 through present time.

TIMELINE:

December 2006 – Discussion- Conceptual discussion of the proposed subdivision. The Board reviewed the proposal and asked the applicant to come back with a formal application for design review. At the meeting the Board discussed the idea of having a conventional subdivision on this property.

February 2007 – Design Review- Design review for a potential subdivision of the original 94.9 acre parcel into 32 lots meeting all area, frontage and slope requirements. The lots are to be serviced by individual wells and septic. The Planning Board approved the density for no more than thirty-two (32) lots and for the applicant to go forward with an open space subdivision plan.

July 2007 - ZBA Hearing - ZBA Hearing was tabled until the August 16, 2007 meeting for a special exception from Article VI, Section 6.026.A.6 to impact 10,800 SF of wetlands and a special exception from Article V, Section 6.026.B to impact 19,762 SF of wetlands buffer for the construction of a roadway.

August 2007 – ZBA Hearing & State Application- Applicant received special exception approval from the ZBA on August 16, 2007 from Article VI, Section 6.026.A.6 to impact 10,800 SF of wetlands and a special exception from Article V, Section 6.026.B to impact 19,762 SF of wetlands buffer for the construction of a roadway. The applicant also submitted a dredge and fill application to the state.

September 2007 – Final Application - First public hearing for the final application of a subdivision off of Osgood road. At this meeting the Board tabled the application until the November 2007 meeting pending a site walk scheduled for October 2nd and outside engineering review and comments.

November 2007 – March 2008 – Extension Request – Per the applicant’s request, a sixty-five (65) day extension was granted in accordance with RSA 676:4 and application was tabled to the May 20, 2008 meeting.

May 2008 – Extension Request – Per the applicant’s request, a six (6) month extension was granted in accordance with RSA 676:4 and application was tabled to the December 16, 2008 meeting with the condition that abutters be re-notified at the applicant’s cost.

December 2008 – Application Withdrawal – Applicant decided to withdraw their application and hoped to return when the economy turns around.

September 2011 – Scenic Road Hearing & Public Hearing for Minor Subdivision – Applicant returned to the Board in 2011 with a separate application proposing to subdivide lot 51/1 into 3 new building lots and one large remainder lot on Osgood Rd. The Planning Board conditionally approved the subdivision of the 3 frontage lots. A scenic Road hearing was also held for the partial removal of stonewall and potential tree cutting/trimming for one new driveway and one new shared driveway off of Osgood Road. Planning Board granted approved subject to the disturbed portion of the stone wall is rebuilt along the new driveway or incorporated into the existing wall.

October 2012 – Public Hearing for a Lot Line Adjustment & Minor Subdivision- Applicant was back before the board last October for a lot line adjustment to revise the common lot line between lots 51-1 and 51/1-2 by exchanging parcels to create a more even lot and to create one new buildable lot. The Planning Board conditionally approved the lot line adjustment and subdivision. The subdivision created a lot of 2.514 acres (109,493 sq. ft.), leaving the original parcel with 85.366 acres (3,718,606 sq. ft.). The large (85.366 acre) remainder lot was left with less than 200 feet of frontage on a Class V or better road. The Planning Board expressed at this time they would not like to see any further subdivision of lot 51-1 without an open space plan being presented.

PROPOSAL:

The applicant is back before the Planning Board for the first public hearing of the final application for a subdivision off of Osgood road. The 85.366 acre parcel would be subdivided into 27 residential lots meeting all area, frontage and slope requirements and two open space non-building lots totaling 44+ acres abutting the Hitchiner Town Forest. The proposed lots will be serviced by on-site wells and septic systems (DES application pending) and underground power and communication services.

The lots as proposed range from 53,019 square feet to 80,239 square feet in size with frontage off of an extension of Woodhawk Drive. The applicant is proposing a 4,100 ft expansion of Woodhawk Drive with 24’ of pavement and a 50’ ROW along with a 524 ft dead-end hammerhead roadway with 24’ of pavement and a 50’ ROW. The proposed road will cross the a wetland in two

places, 6,935 SF of impact will be associated with the first crossing and 3,865 SF of impact will be associated with the second crossing.

The applicant has minimized cuts and fills for the development of the roadway, and details a maximum slope of 8% as the roadway winds throughout the subdivision. The hammerhead has a maximum slope of 4.25% and then levels out to a 1.50% grade for the end of the roadway.

SITE INFORMATION:

Zoning for the entire site is Residential 'R' (Rural) with minimum lot frontage requirements of 200 feet and lot area of 2.00 acres (87,120 SF) with building setbacks of 30 feet front and 15 feet rear and sides. Additionally, the site is located within the Level 1 Groundwater Protection Overlay District and lies outside of the 100 year flood hazard area as shown on FIRM Panel 33011C0470D.

Lot 51-1 is presently under a current use tax lien and listed as a non-buildable lot as there is less than fifteen (15) feet of frontage on a Class V or better road.

ZBA APPROVAL:

The applicant received approval from Article VI, Sections 6.02.6:A.6 on 10/7/2013 to impact 10,800 SF of wetlands and 6.02.6:B to impact 19,762 SF of wetland buffer for the construction of the proposed roadway (a dredge and fill application has been submitted to the state).

OPEN SPACE:

The parcel encompasses a total of approximately ± 85 acres on the southern side of Burns Hill, with frontage on Osgood Road. The applicant is proposing two tracts of open space. The first tract is located on the southern portion of the property (51-1) and is approximately 20.778 acres (905,107 SF) with approximately 339,280 SF being wet. Open space lot 51-1 will abut to an existing Conservation Easement (8415/1291) already monitored by the Conservation Commission. The second tract of open space (51-1-32) will extend from the most southern tip of the lot to the most northern, for a total of 23.378 acres (1,018,356 SF) abutting the Hitchner Town Forest.

While in past discussion the Conservation Commission has expressed interest in the two open space lots, per section 5.08 of the Development Regulations the applicant needs to specify how the open space lots (Map 51, Lot 1 & 1-32) will be owned. Their options are: a government agency or nonprofit; in common by 6 residential lots; a homeowners association of the 6 residential lots; or the land may remain with the developer. If the applicant would like to propose some other form of ownership of the open space they will need to seek Planning Board approval.

DRAINAGE/STORMWATER:

The site is entirely wooded with a predominate drainage pattern of draining into the site's central wetland area and then flowing to the north and south off site. The slopes throughout the site provide for a majority of the existing site to drain by sheet flow to adjacent wetlands.

There are two proposed oversized 4' box culverts located in the wetlands crossing for the proposed road. The oversized box culverts are intended for wildlife passage.

The proposed site improvements do not significantly alter the peak rate of storm water runoff to the existing Osgood Road drainage system. The small increase in runoff to Osgood Road will have no adverse effect downstream or on existing drainage.

PHASING:

The Planning Board requires developments which qualify as Major Subdivisions to take place over a period of years, in stages, in order to promote orderly development with minimal impact on the provisions of Town services. The allocation of building permits shall be by the following method:

<u># of New Building Permits</u>	<u>Phasing (years)</u>
1-10	None
11-20	2
21-30	3
35 31-40	4
40+	Minimum of 5

It is the intention of the phasing schedule to evenly distribute the number of building permits over the required number of years. However, if the Planning Board determines it is in the public’s best interest (i.e. through-road connection, etc.) to allow an applicant to have a greater number of permits in the beginning or end of the allotted phasing period, the Planning Board may grant an allowance for more permits in a single year, as long as the project remains phased over the entire phasing period. The approved phasing schedule shall be identified in a note on the plan or laid out as a phasing plan included in the final plan set.

The Board should discuss any possible phasing plan proposed for this project. Phasing of the roadway will not be possible as Woodhawk Drive has reached the Town’s maximum length for dead-end roads of 1,000 feet and Nye Drive is near the 1,000 foot maximum at its current state. All infrastructure including roadways (base coat), drainage and cisterns if applicable should be installed prior to commencement of any phasing plan and a note stating this should be included on the final plan.

INTERDEPARTMENTAL REVIEWS:

Fire Department: Comments regarding the Nye Drive subdivision proposal at this time.

1. The entire road should be named Woodhawk Dr. instead of Nye Dr. to avoid confusion with emergency response. The proposed road (dead end) could be named Nye Dr.
2. I know the plan was previously presented with 2 or 3 30K cisterns. I would like to have the developer look at the costs associated with putting the new combination sprinkler system in each home instead. The cost in our area right now is about 5K per home. Should the cost not benefit the developer than the cistern will be appropriate.
3. On the proposed road we would like to see the hammer head turned into a cul-de-sac and road flattened out.

4. All infrastructure including roadways (base coat), drainage and cisterns if applicable shall be installed prior to building commencing.

Department of Public Works:

1. I would like this to be reviewed by an outside consulting engineer.
2. Will need to have driveway profiles (could be typical) at culvert/ditch locations shown on the plan and should be wider for emergency vehicles.
3. Flatten hammer head area for winter maintenance and snow storage.

Environmental Coordinator:

I have the following comments at this point.

1. The EPA NPDES Construction General Permit requires the applicant to file an NOI covering the entire project, including the lots. The supporting documentation for the NOI submittal serves as a portion of the required documentation for the Milford Stormwater permit. Therefore, the Milford permit will include the disturbances on the individual lots and is not limited to common site disturbances such as roadways and utilities. Drainage on and from the lots should be considered. If on-lot improvements, such as rain gardens, are required to achieve infiltration of the 1" storm, these should be included. Notes should be changed to reflect the above.
2. Stormwater runoff should be modeled at the 1" event to demonstrate complete infiltration of that storm within the project's boundaries.
3. All stormwater conveyance structures as well as treatment and infiltration facilities (except those serving individual lots) must have developed access along with maintenance easements.

Water Utilities: Water and sewer service is not available for this application.

Ambulance: A second access road to the Badger Hill Development is a positive for this plan. The second entrance will facilitate public ingress/egress plus emergency services in emergency situations or adverse conditions.

Zoning Code Enforcement: Properties are zoned Residence 'R' and is proposed to be developed as an open space subdivision. No issues relative to zoning as long as the project meets the criteria specified in Article VI, Section 6.04.0 Open Space and Conservation District.

No comments were received as of November 14, 2013 from Police or Assessing. The Heritage Commission and Conservation Commission's regular meeting were held after staff memos were distributed, if any comments come in, Staff will let the Board know at the meeting.

WAIVERS:

No waivers requested.

NOTICES SENT:

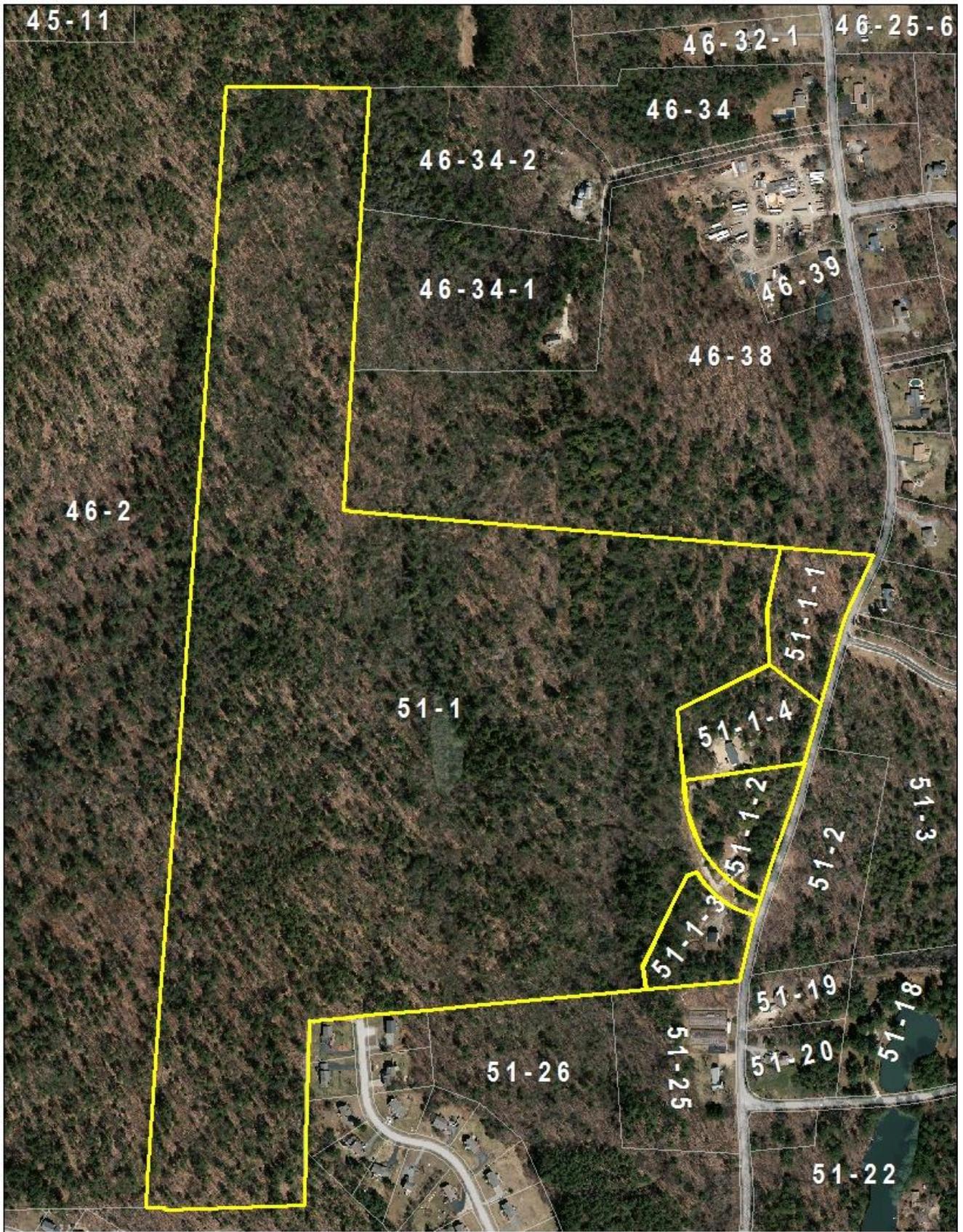
Notices were sent to all abutters on November 8, 2013

APPLICATION STATUS:

The application is complete and ready to be accepted at this time. The Board will need to make a determination of regional impact. Please find the attached plan set.

STAFF RECCOMENDATIONS:

At this time, bearing in mind past Board discussions about this site, the Board should discuss with the applicant any questions or concerns with the project and make a motion to send the plan out for review of the drainage study, stormwater plan and roadway. It will take some time for the Town's consulting engineer to complete the review, the Board should discuss with the applicant a reasonable date to return to the Board in either January or February once the consultant has completed their review.





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February 11, 2014

Ms. Jodie Levandowski, Town Planner
Office of Community Development
Town of Milford
1 Union Square
Milford, NH 03055-4240

RE: REVIEW OF DRAINAGE & ROADWAY DESIGN
CAROLE M. COLBURN REVOCABLE TRUST
OSGOOD ROAD & WOODHAWK DRIVE

Dear Ms. Levandowski:

The Milford Planning Department has requested Comprehensive Environmental Inc (CEI) to review the roadway design, drainage design and supporting drainage calculations as depicted in the design plans and associated documentation for the proposed subdivision of tax map lot 51-1 in Milford, NH. This letter summarizes CEI's comments on the management of stormwater in compliance with the Milford Stormwater Management and Erosion Control Regulations and roadway and subdivision design in accordance with the Milford Development Regulations, and DPW Infrastructure Design, Construction and Administration Standards.

CEI has based its review on the following information furnished to the Planning Department:

- A. Drawings entitled "Carole M. Colburn Revocable Trust, Osgood Road & Woodhawk Drive, Milford, New Hampshire" dated October 15, 2013, prepared by Meridian Land Services, Inc. The drawings include 12 sheets of drawings.
- B. Drainage calculations and related documentation for the project included in a report entitled "Storm Water Management Report, Carole M. Colburn Revocable Trust, Milford, NH, Tax Map Lots 51-1" dated May 23, 2007, Revised January 2, 2008, prepared by Meridian Land Services, Inc.

CEI offers the following comments regarding the documentation of the proposed site plan.

- Engineering
- Design
- Construction
- Inspection

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- Drainage & Flooding
- Energy & Sustainability
- Hazardous Waste
- Permitting & NEPA
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- Transportation
- Water & Wastewater
- Watershed Restoration



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General

1. Stockpile areas, stump composting areas, and snow management areas have not been depicted on the plans.
2. The invert information for HW-5B is not listed on the profile.
3. Design information for treatment swale – 6 should be added to the detail chart on sheet D-2.
4. The riprap pad sizes shown on the plans do not depict the sizes listed in the detail chart on sheet D-2.
5. The riprap sizing calculations in Section 7.0 of the Stormwater Management Report do not match the sizes listed in the detail chart on plan sheet D-2.
6. The construction sequence should consider the installation of the wetland culverts prior to the construction of the roadway.
7. The 4'x4' wetland box culvert detail should be updated to depict the proposed ground line 12-inches above the bottom inside face of the structure (embedment).

Roadway Design

1. The Town's typical roadway section for open drainage shows a paved shoulder when guardrail is required. We would agree with the design engineer, reduction of impervious pavement wherever possible is encouraged.
2. A detail of the piping beneath the proposed driveways which cross the riprap ditches should be provided. Calculations demonstrating the piping is adequately sized should be included.

Stormwater Management Calculations and Analysis

1. The plans state an Alteration of Terrain Permit (#WPS-8016) has been approved for this project. Due to the elapsed time since the permit was issued for this project the AOT permit may no longer be valid. The design engineer should provide documentation that the AOT permit is still valid or documentation that an extension was granted if it has been more than 5 years from the date of issuance.



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If a new AOT Permit is required for the proposed site, the site will need to be redesigned to meet the current State requirements for stormwater management.

2. The post-development peak rate of runoff exceeds the pre-development peak rate of runoff in several instances across the site, for several design storm events. This conflicts with the generally accepted practice that developments should mitigate increases in stormwater runoff due to increased impervious areas prior to discharge to reduce impacts to off-site properties.
3. Peak runoff rates comparing the pre- and post-development discharge rates for the 50-year storm should be included with the summary tables within the Stormwater Management Report.
4. The land cover-type “brush” has been used to calculate the runoff from the site in the proposed conditions. No “brush” has been accounted for in the existing conditions. This cover-type may underestimate the proposed peak runoff rate if brush is not specifically constructed as part of the project. In addition the proposed plans do not clearly indicate where the “brush” is proposed.
5. The design engineer should specify the assumption made for calculating new impervious area associated with individual lots, home and driveway construction.
6. The detention basin calculations credit an exfiltration rate of 10 inches per hour for each basin. Test pit information including seasonal high water table elevations shall be provided at the location and approximate elevation of the proposed exfiltration device. The exfiltration rate should be confirmed by field testing.
 - a. If exfiltration is practical at these locations, the detention basin detail should be revised. The detail currently calls for a compacted subgrade, which would inhibit exfiltration.

Notes should be added to the drawings to protect the exfiltration surfaces from degradation by construction activity, including:

- i. Prevention of contamination of the exposed subgrade by construction sediment.
- ii. Prevention of excessive compaction by construction vehicles.



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- iii. Prevention of the discharge of water from construction dewatering activities into these facilities.
 - iv. Prevention of discharge of stormwater into these facilities until the contributing areas are stabilized, unless specific measures are provided for protecting and restoring the exfiltration surface.
- b. If exfiltration is found to be impractical at these locations test pits may still be warranted. The majority of test pits across the site show seasonal high water approximately 30-inches below the ground surface. The proposed detention basins will be excavated below the existing ground surface, it should be verified that the bottom elevation of the proposed basins will not be exposed to standing groundwater.
7. Have calculations been performed to ensure the size of the crushed stone used for the check dams is sufficient to withstand design storm runoff velocities?
 8. Have the elevations of the stone check dams within the riprap ditches been designed to ensure the flow of stormwater will not overtop onto the roadway during the design storm?
 9. Have riprap pad sizing calculations been performed for the outlet control structure pipes discharging from the detention basins?
 10. Culvert 5, HW-5A to HW-5B, has not been sized for the total receiving watershed. Only flows from the proposed subdivision have been modeled through the culvert. No flow from the offsite upland watershed/ wetlands have been modeled to ensure the culvert is sized to handle all flows that will require passage through the structure.

Stormwater Management System Design

1. Sediment forebays are required for all basin and swale designs.
2. Emergency spillways should be utilized at all detention ponds, preferably in undisturbed ground.
3. Typical design standards for culverts can be found in the New Hampshire Stream Crossing Guidelines.
 - a. Concrete box culverts are typically embedded 2-feet below the adjacent streambed elevation.



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- b. The width of the proposed structure is typically 1.2 times the bankfull width of the stream plus 2-feet.
- 4. The streambed material used to bury the structure 12-inches should be specified. The substrate within the structure should match that of the substrate in the natural stream channel. Riprap pads should not be required at the inlets and outlets of the proposed wetland box culverts.
- 5. Details of the stream bank stabilization along the newly filled banks of the wetlands should be provided. Wetland restoration techniques and specific wetland seed mixtures and plantings should be used to restore the area.

Erosion/Sediment Controls and Maintenance

- 1. Sediment shall be removed once the volume reached ¼ to ½ the height of the haybale.
- 2. Stockpiles should be surrounded by sediment barriers until seeding can become established and provide proper stabilization.
- 3. A post-construction Operation and Maintenance Plan should be provided for all stormwater management devices. Including but not limited to: responsible parties, tasks to be completed, frequency of inspection and maintenance, inspection log form.

If you have any questions or comments regarding this report please contact me at (603) 424-8444 ext. 314.

Sincerely,

COMPREHENSIVE ENVIRONMENTAL INC

Scott Salvucci
Scott C. Salvucci, P.E.
Project Engineer

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- Inspection

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