

TOWN OF MILFORD

Office of Community Development
Planning • Zoning • Building Safety • Code Enforcement • Health
Economic Development • Active Projects



Administrative Review

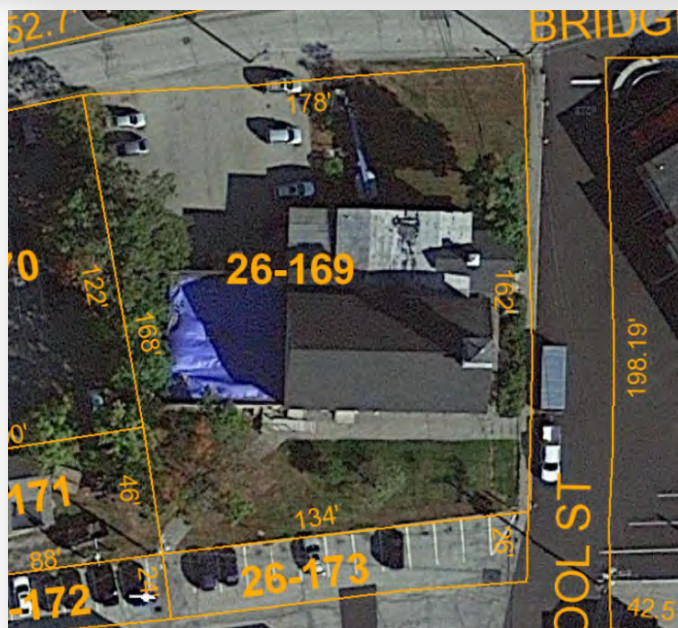
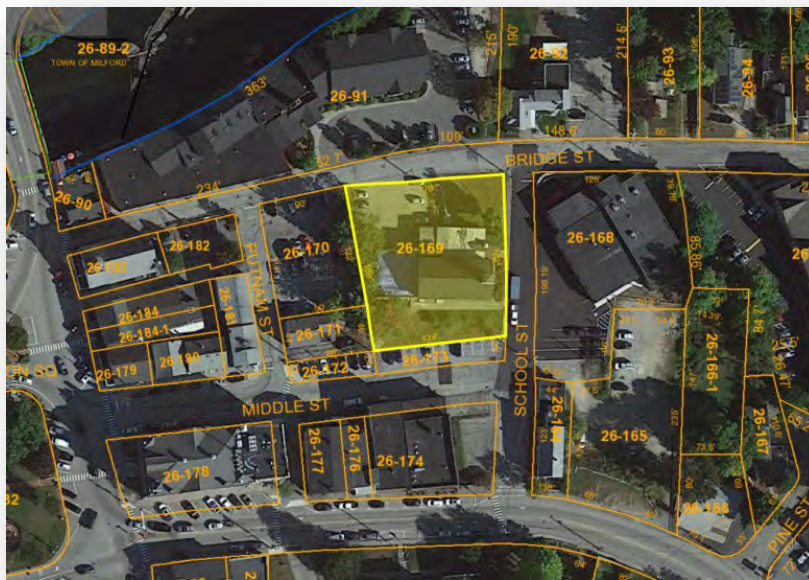
Date: March 23, 2021
To: Jason Plourde, Chair - Zoning Board of Adjustment
From: Lincoln Daley, Community Development Director
Subject: **Case #2021-08 Housing Initiatives of New England, 54 School Street, Tax Map 26 Lot 169 Special Exception Application.**

The applicant is before the Board of Adjustment seeking a SPECIAL EXCEPTION from the Milford Zoning Ordinance, Article V, Section 5.05.5 to allow the construction of a portion of the proposed addition to the existing vacant commercial building, dumpster pad, and generator within the 15 side dimensional setback in the Commercial 'C' District and Oval District Overlay. The purpose of the addition and improvements are associated with the proposed re-use and expansion of the building for an 18 unit multi-family housing development. In reviewing the files for this property, I offer the following comments:

1. Existing Conditions:
 - a. The subject property is approximately .52 acres (22,650 sq. ft.) and is primarily developed. The property contains the 19,192 square foot (gross area) former Milford Telegraph and School building. The building is mainly vacant with a small portion building leased for equipment storage.
 - b. Property contains approximately 134 feet of linear frontage on Middle Street, 162 linear feet of frontage on School Street, and 178 linear feet on Bridge Street. The property is serviced by municipal water and sewer.
 - c. The subject property falls within Commercial 'C' and Oval Districts and is situated among an established mixed-use, residential, and commercial neighborhood. To the north, the subject property abuts the Milford Mills multi-family senior development and a two-family residence. Located to the east is the Milford Fire Station. To the west, the property abuts a municipal parking lot and mixed use building. Lastly, to the south, the property abuts commercial and mixed used buildings.
 - d. The existing building is located 15.64 linear feet from the western side dimensional setback and 0 feet on the eastern front setback on School Street.
2. The applicant is seeking to convert and expand the existing vacant commercial building to an 18 unit, multi-family, senior housing development. A portion of the proposed expansion lies 11.8 feet from the side dimensional setback where 15 feet is required. In addition, the site plan displays the locations of a 10' x 10' square foot dumpster enclosure/pad and 8' x 10 square foot transformer pad within 5 feet of the same side dimensional setback. Both are defined as structures under the Milford Zoning Ordinance and will require setback relief under the same provision.
3. Pursuant to Section 5.05.5, locating a structure within the 15 foot side dimensional setback is permitted by way of Special Exception. Upon further examination of the surrounding properties within the downtown Oval district, we find that is it common for the existing structures to encroach within front, side, and rear setbacks comparatively. This is due to the pre-existing, non-conforming nature of the lots and age/location of the buildings.

4. This application submitted applications for Variance relief pursuant to Sections 5.03.4 and 5.05.1.P and a Site Plan Application before the Planning Board.
5. As part of the Board deliberation, the applicant should be prepared to discuss the following:
 - a. Explanation /summary of the options/alternatives considered.
 - b. The applicant will need to consider alternatives to relocate the dumpster enclosure/pad site and transformer pad to a minimum distance of 11.8 feet (same as the proposed building) or submit a Special Exception for the required relief.
 - c. Impact to abutting properties and comparative analysis of building/structure locations on abutting/neighbors lots.

Aerial Photograph(s) of Subject Property:



Street View(s) of Subject Property:



View From Middle Street



View From Middle Street



View From School Street



View From Middle & School & Street



View From School & Bridge Street



View From Bridge Street



View From Bridge Street



ZBA Application
MILFORD ZONING BOARD OF ADJUSTMENT
GENERAL PROPERTY INFORMATION FOR ALL APPLICATIONS

Date Received: _____
 Case Number: _____
 Application Number: _____
 Hearing Date: _____
 Decision Date: _____
 Decision: _____

PROPERTY INFORMATION

Street Address: **54 BRIDGE ST.**

Tax Map / Parcel #: **26-169** Lot Size: **24,671 s.f.**

PROPERTY CURRENTLY USED AS

**VACANT
 FORMER MILFORD CABINET BLDG.**

If the application involves multiple lots with different owners, attach additional copies of this page.

PROPERTY OWNER

Name: **HOUSING INITIATIVES OF NEW ENGLAND**

Address: **264 U.S. ROUTE 1 BLDG 300 SUITE 2A**

City/State/Zip: **SCARBOROUGH ME 04074**

Phone: (207) **774-8812**

Email: **ctaylor@hinec.org**

The applicant is the person who is making this proposal on behalf of themselves, the owner or a third party. This is usually the same as the property owner, but might be a tenant, someone who plans to purchase the property, an engineer or lawyer, etc. If the applicant is the same as the owner, just check "Same as owner" and leave the rest of this section blank.

APPLICANT/REPRESENTATIVE

SAME AS OWNER

Name: _____

Address: _____

City/State/Zip: _____

Email: _____

Phone: () _____ Cell: () _____

The undersigned property owner(s) hereby authorize(s) the filing of this application and agree to comply with all code requirements applicable to this application.

[Signature] _____ **3/15/2021**
 Property Owner's signature Date:

Zoning District (check one):

Residence A
 Residence B Residence R
 Commercial
 Limited Commercial
 Industrial
 Integrated Commercial-Industrial
 Integrated Commercial-Industrial-2

Overlay District (check any that apply):

West Elm Street Overlay
 Nashua/Elm Street Overlay
 Commerce & Community Overlay
 Open Space & Conservation
 Wetlands Conservation
 Groundwater Protection
 Floodplain Management

APPLICATION FEES

Application Fee:	\$75.00
Abutters Fee: \$4 x _____	
Amount received:	
Date Received:	
Check _____ Cash _____	

THE FEES ASSOCIATED WITH THIS APPLICATION DO NOT APPLY TO ANY OTHER FEES REQUIRED FOR APPROVAL OF THIS PROJECT. PLANNING, IMPACT, BUILDING AND OTHER FEES MAY APPLY.



ZBA Application – Special Exception
MILFORD ZONING BOARD OF ADJUSTMENT

Date Received: _____
 Case Number: _____
 Application #: _____
 Date Complete: _____
 Hearing Date: _____
 Decision Date: _____
 Decision: _____

PROPERTY INFORMATION	
Street Address:	54 BRIDGE ST.
Tax Map / Parcel #:	26 - 169
<p>A Special Exception is a use which is permitted by the Zoning Ordinance, but requires approval from the Zoning Board of Adjustment. Most special exceptions have a list of additional criteria that must be met in order for the ZBA to approve the application.</p> <p>*Note that in addition to the specific criteria that may be listed for a particular special exception, all special exceptions are subject to the general criteria in Section 10.02.1 of the Zoning Ordinance.</p>	
<p>What section of the Zoning Ordinance are you applying under?</p> <p>Article <u> V </u> Section <u> 5.05.7 </u></p> <p>Describe the <u>use</u> you are proposing under the above section of the Ordinance.</p> <p><u> MULTI-FAMILY RESIDENTIAL </u></p> <p><u> AFFORDABLE SENIOR HOUSING </u></p>	

**Application for
(check all that apply):**

- Change/Expansion of Non-conforming Use/Structure (2.03.1.C)
- Wetland Buffer Impact (6.02.6)
- Accessory Dwelling Unit (10.2.6)
- Office in Res-A & B (10.2.7)
- Home Business (7.12.6)
- Side/Rear Yard Setback Reduction (Zoning District Specific)
- Other

General Criteria Section 10.02.1

Describe the project you are requesting a Special Exception for:
 IT IS PROPOSED TO RENOVATE AND EXPAND ON THE EXISTING MILFORD CABINET BUILDING TO CREATE AN 18 UNIT AFFORDABLE SENIOR HOUSING APARTMENT BUILDING.
 - THERE IS PROPOSED A MINOR ENCRoACHMENT IN THE SIDE YARD SETBACK.

Explain how the proposal meets the general criteria as specified in Article X, Section 10.02.1 of the Zoning Ordinance:

- A. The proposed use is similar to those permitted in the district because:
 THE USE IS PERMITTED IN THE COMMERCIAL DISTRICT, AND THERE ARE AFFORDABLE SENIOR LIVING APARTMENTS IN THE MILL BUILDING ACROSS THE STREET. OTHER USES AND EXISTING BLDGS DO NOT HAVE SETBACK REQUIREMENTS.
- B. The specific site is an appropriate location for the proposed use because:
 THE DEVELOPMENT WILL PRESERVE AND RENOVATE THE FORMER MILFORD CABINET (HISTORIC) BUILDING. THE CHARACTER AND MASS OF THE BUILDING WILL FIT WITH THE SURROUNDING BUILDINGS IN THE OVAL OVERLAY AREA.
- C. The use as developed will not adversely affect the adjacent area because:
 THE PROPOSED ADDITION HAS A SMALL CORNER OF BUILDING WITHIN THE SETBACK BEING 11.8 FEET FROM THE PROPERTY LINE ; THAT SIDE OF THE BUILDING HAS AN AVERAGE SETBACK DISTANCE OF 18.4 FEET WHERE 15 IS REQUIRED.
- D. There will be no nuisance or serious hazard to vehicles or pedestrians because:
 THE DEVELOPMENT PROVIDES ADEQUATE PARKING AND PEDESTRIAN ACCESS ON AND AROUND THE SITE.
 > SENIOR LIVING FACILITIES GENERATE VERY LOW TRAFFIC.
- E. Adequate appropriate facilities will be provided for the proper operation of the proposed use because:
 THE SITE WILL BE SERVICED BY MUNICIPAL SEWER AND WATER.
 PARKING IS PROVIDED ON SITE AND WITH AN EASEMENT ACROSS THE STREET.
 APPROPRIATE FACILITIES WILL BE PROVIDED FOR PROPER OPERATION OF THE APARTMENTS.



Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists

Independent Senior Living Facility, 54 School Street

Application for Multifamily Setback:

General Criteria

1.

The adjacent lot is a municipal parking lot and the building on lot 26-171 is well separated from the proposed senior housing building. The design has one corner 11.8 feet setback, where 15 feet is required. Other uses within the Oval Subdistrict area exempt from yard setbacks. And the other buildings from the Oval along Middle Street and Bridge Street area not setback. So, the proposed addition will be in keeping with the neighborhood while preservice the historic Milford Cabinet Building.

The project will provide affordable senior housing near the Oval Business area which helps meet a housing need in town and bring business to the shops and restaurants in the area.

2.

The proposed addition is setback a minimum of 11.8 feet and an average setback of 18.4 feet on that side. The addition is well away from any adjacent building with municipal parking on two sides and a small building on lot 26-171 approximately 36 feet from the property line.

3.

It will allow us to preserve the historic Milford Cabinet Building while construction the addition for affordable senior housing in a configuration that best works architecturally and with the site. This layout will allow a building consistent with the surrounding structures.

4.

The addition provides an average of 18.4-foot setback (which exceeds the required 15 feet), but in order to preserve the Milford Cabinet building and layout the addition is an efficient functional manner there is one corner 11.8 feet from the property line. This layout is very much in keeping with the neighborhood.

Preserving the Milford Cabinet building while adding the addition will increase property value and increase the value of the surrounding properties.



Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists

This site is unique in that the project is preserving the historic Milford Cabinet building. To have the means to preserve and incorporate the historic building into the project, 18 units are needed to make the development (building size and architecture) fit into the surrounding area (with the mill building across the street and surrounding municipal buildings). Other uses within the Oval Subdistrict area are exempt from any setback requirements.

The proposed 18-unit Independent Living senior housing building will fit in to the neighborhood with the setbacks, mass, and proportions of the building, it preserves the historic Milford Cabinet building and will provided a type of housing that helps meet a need within the community. The addition is an average of 18.4 feet from the property line.

We appreciate your consideration in this matter. If you have any questions or comments, please do not hesitate to contact me at (603) 472-4488 or email jkevan@tfmoran.com.

Sincerely,
TFMoran Inc.

Jeffrey Kevan
Senior Project Manager

48 Constitution Drive
Bedford, NH 03110
Phone (603) 472-4488
Fax (603) 472-9747
www.tfmoran.com

GENERAL INFORMATION

OWNER/PREPARED FOR

MAP 26, PARCEL 169
HOUSING INITIATIVES OF NEW
ENGLAND CORP.
264 US RTE 1, BLDG 300 STE 2A
SCARBOROUGH, ME 04074

RESOURCE LIST

PLANNING DEPARTMENT
1 UNION SQUARE
MILFORD, NH 03055
(603)249-0620
LINCOLN DALEY
COMMUNITY DEV. DIRECTOR

PUBLIC WORKS

289 SOUTH STREET
MILFORD, NH 03055
RICK RIENDEAU, DIRECTOR
(603)249-0685

POLICE DEPARTMENT

19 GARDEN STREET
MILFORD, NH 03055
(603)249-0630
MIKE VIOLA, CHIEF

FIRE DEPARTMENT

39 SCHOOL STREET
MILFORD, NH 03055
(603)249-0680
KEN FLAHERTY, CHIEF

WATER UTILITIES

564 NASHUA STREET
MILFORD, NH 03055
(603)249-0660
KEVIN STEATSON, DIRECTOR

ELECTRIC:

EVERSOURCE
P.O. BOX 330
MANCHESTER, NH 03105-0330
PHONE: (603) 634-3514
CONTACT: MARIO BOUCHER

GAS:

LIBERTY UTILITIES
130 ELM STREET
MANCHESTER, NH 03101
PHONE: (603) 782-2321
CONTACT: ANDY MORGAN

ABUTTERS

MAP 26, LOT 91
MILFORD MILL LTD. PARTNERSHIP
264 US RTE 1, BLDG 300 STE 2A
SCARBOROUGH, ME 04074

MAP 26, LOT 92
JUDITH E WHITE, TRUSTEE,
JUDITH E WHITE REVOCABLE TRUST
100 BRIDGE STREET
MILFORD, NH 03055-4571

MAP 26, LOT 164
PAUL C & LORI A WORRALL
1 BORDER STREET
MILFORD, NH 03055

MAP 26, LOT 165
RISK REALTY LLC
480 NASHUA STREET
MILFORD, NH 03055

MAP 26, LOT 168
TOWN OF MILFORD, NH
ONE UNION SQUARE
MILFORD, NH 03055-4230

MAP 26, LOT 170
TOWN OF MILFORD, NH
ONE UNION SQUARE
MILFORD, NH 03055-4230

MAP 26, LOT 171
ARCHELON PROPERTIES, LLC
62 PRENTISS ST.
CAMBRIDGE, MA 02140

MAP 26, LOT 172
TOWN OF MILFORD, NH
ONE UNION SQUARE
MILFORD, NH 03055-4230

MAP 26, LOT 173
TOWN OF MILFORD, NH
ONE UNION SQUARE
MILFORD, NH 03055-4230

MAP 26, LOT 174
KSH REALTY, LLC
320 MILE SLIP ROAD
MILFORD, NH 03055

MAP 26, LOT 176
RONALD & LOREEN RACICOT,
CO-TRUSTEES, RACICOT FAMILY
21 OLD WILTON ROAD
MILFORD, NH 03055

MAP 26, LOT 177
FRANKO ESTATE HOLDINGS OF NH LLC
116 BURNS HILL ROAD
WILTON, NH 03086-5106

MILFORD INDEPENDENT ELDERLY HOUSING

**54 SCHOOL STREET
MILFORD, NEW HAMPSHIRE**

INDEX OF SHEETS

SHEET SHEET TITLE

1	COVER SHEET
2	EXISTING CONDITIONS
3	OVERVIEW - EXISTING CONDITIONS PLAN
4	SITE PREPARATION PLAN
5	SITE LAYOUT PLAN
6	GRADING & DRAINAGE PLAN
7	UTILITY PLAN
8	LANDSCAPE PLAN
9	STORM WATER MANAGEMENT PLAN
10-15	DETAIL SHEETS
L1	LIGHTING PLAN (BY ...)
A-16	ARCHITECTURAL LAYOUT

PERMITS / APPROVALS

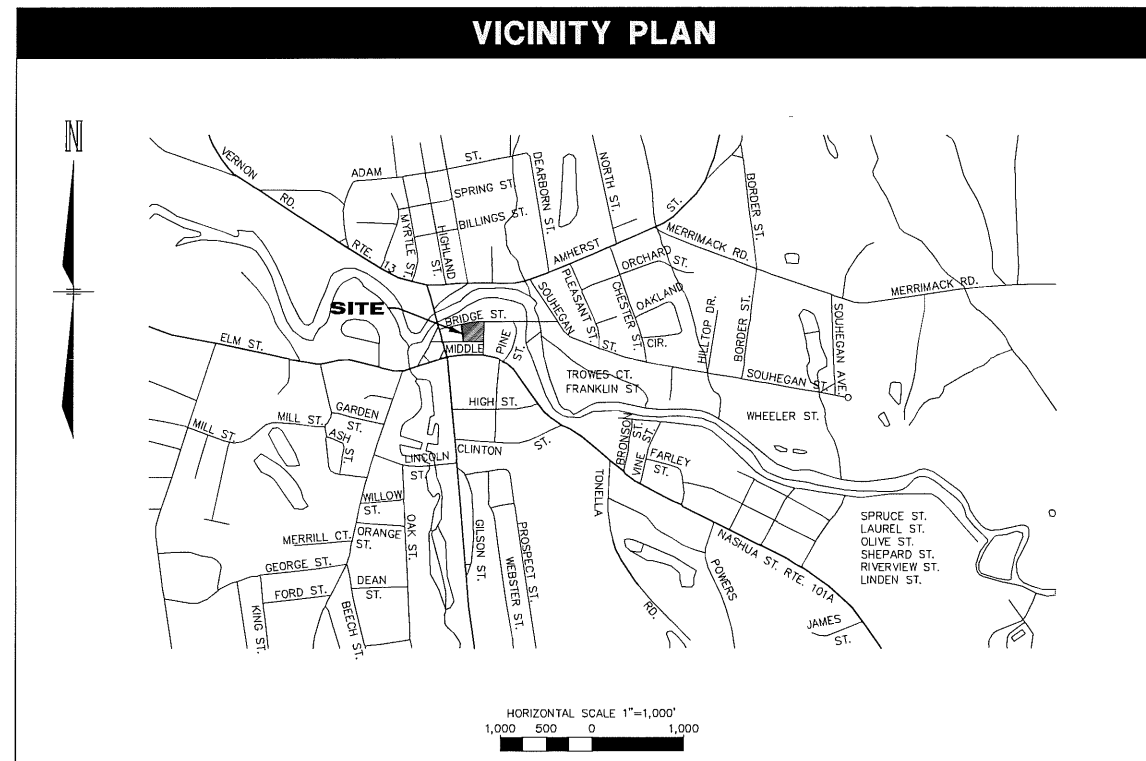
	NUMBER	APPROVED	EXPIRES
TOWN SITE PLAN			

WAIVERS

THE FOLLOWING WAIVERS FROM THE TOWN OF MILFORD SITE REVIEW REGULATIONS ARE BEING REVIEWED BY THE PLANNING BOARD:

1. TOWN OF MILFORD SITE REVIEW REGULATIONS SECTION 5.03.4 -
VARIANCE FOR ALLOWABLE DENSITY
2. TOWN OF MILFORD SITE REVIEW REGULATIONS SECTION 5.05.5.B -
SPECIAL EXCEPTION FOR SIDE SETBACK

VICINITY PLAN



OWNER'S SIGNATURE _____ DATE _____
OWNER OR REPRESENTATIVE _____

APPROVED BY THE TOWN OF MILFORD PLANNING BOARD

ON _____ AND _____
BOARD MEMBER _____
BOARD MEMBER _____

TAX MAP 26 LOT 169
COVER SHEET
**MILFORD INDEPENDENT
SENIOR HOUSING**
54 SCHOOL STREET, MILFORD, NH
OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.

SCALE: NTS MARCH 22, 2021

TFM	Civil Engineers	48 Constitution Drive
	Structural Engineers	Bedford, NH 03110
	Traffic Engineers	Phone (603) 472-4488
	Land Surveyors	Fax (603) 472-9747
	Landscape Architects	www.tfmoran.com
	Scientists	

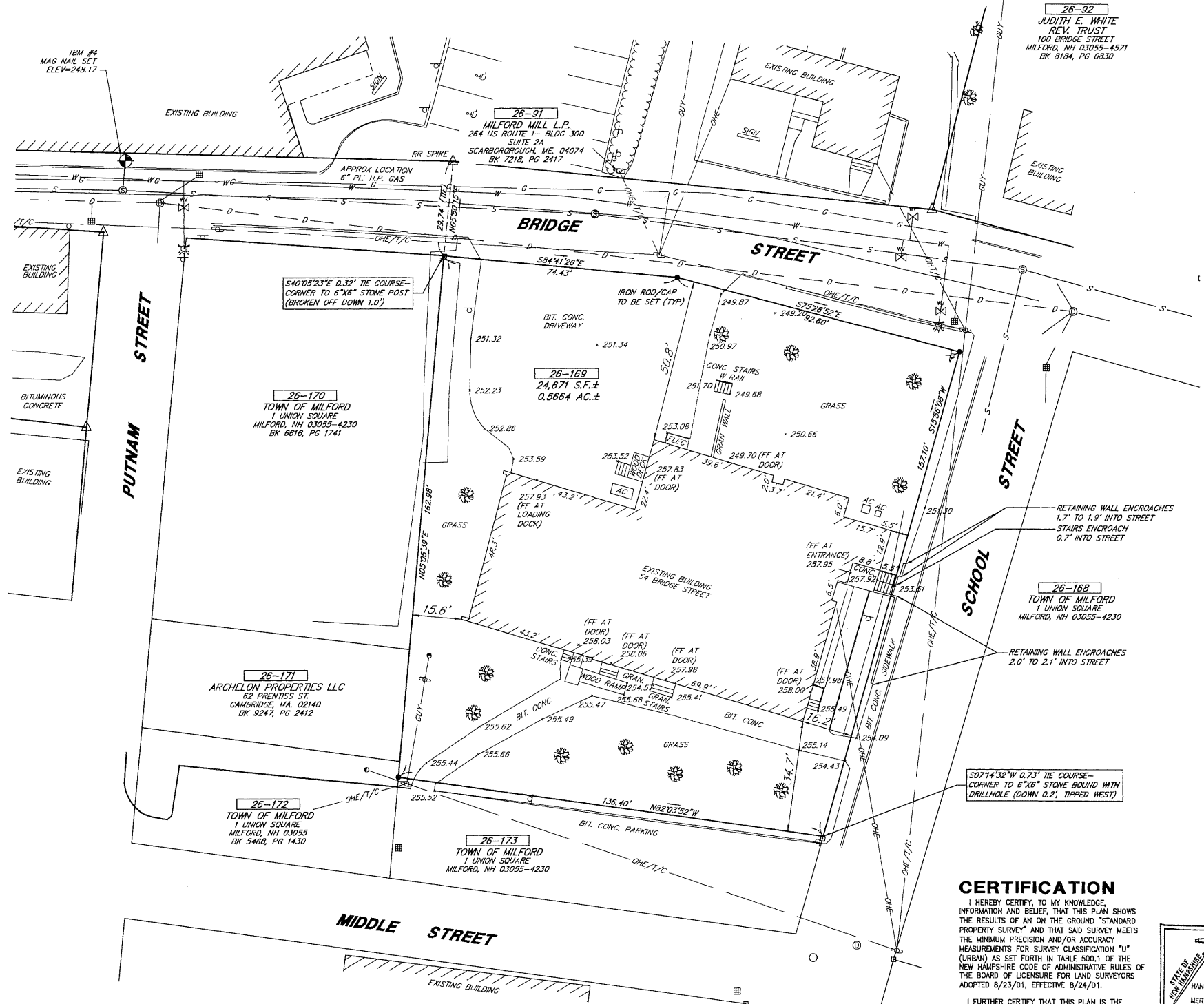
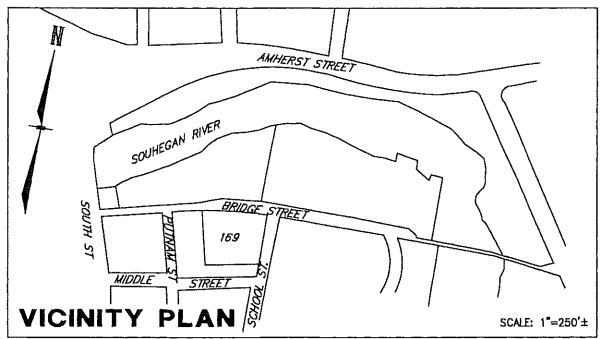
76451.21 DR SRP FB
CK JK CADFILE 76451-21 COVER-DETAILS SHEET 1 OF 15

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This plan is not effective unless signed by a duly authorized officer of Thomas F. Moran, Inc.





REFERENCE PLANS

- 1. SUBDIVISION PLAN OF LAND, MILFORD TEXTILE CORP. SAMUEL GOLDMAN - HELEN N. GOODWIN (GUARDIAN), BRIDGE STREET, MILFORD, NEW HAMPSHIRE, SCALE 1"=20', DATED JANUARY 1974 BY ALLAN H. SWANSON, INC AND RECORDED AS PLAN #7432 AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS.
- 2. MILL PROPERTY IN MILFORD-AS SOLD MAURICE A. GOLDMAN, 1916 BY S.H. ABBOT, SURVEYOR.
- 3. PART OF BRICK SCHOOL HOUSE PROPERTY, MILFORD, NH, CONVEYED TO JOHN T. SMITH, SURVEYED OCT. 1, 1929 BY LL JUNKINS, SURVEYOR, SCALE 30 FT=1 INCH, RECORDED AS PLAN #581 AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS.
- 4. BK PROPERTIES SUBDIVISION PLAN OF LAND OF TOWN OF MILFORD ON BRIDGE ST., PINE ST. AND SCHOOL ST., MILFORD, NEW HAMPSHIRE, SCALE: 1"=30', DATED AUGUST 25, 1989 (LAST REVISED 10/11/89) BY REISLAND ASSOCIATED, INC., RECORDED AS PLAN #23865 AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS.
- 5. MIDDLE ST., MILFORD, DATED 1922, BY S.H. ABBOT, SURVEYOR, SCALE 40' = 1 INCH.
- 6. SITE PLAN SOUHEGAN NATIONAL BANK, MILFORD, N.H., SCALE: 1"=20', DATED MARCH 7, 1980 (LAST REVISED 4/18/80), BY THOMAS F. MORAN INC., RECORDED AS PLAN #13100 AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS.

NOTES

- 1. OWNER OF RECORD OF MAP 26 LOT 169: HOUSING INITIATIVES OF NEW ENGLAND CORP., 264 US ROUTE 1-BLDG 300 SUITE 2A, SCARBOROUGH, ME. 04074. DEED REFERENCE TO PARCEL IS BOOK B430, PAGE 911. AREA OF MAP 26, LOT 169 = 24,671 S.F.± OR 0.5664 ACRES±.
- 2. [26-169] INDICATES TAX MAP AND LOT NUMBER.
- 3. EXAMINATION OF THE FLOOD INSURANCE RATE MAP FOR HILLSBOROUGH COUNTY, NEW HAMPSHIRE (ALL JURISDICTIONS), MAP NUMBER 33011C0450D, EFFECTIVE DATE SEPTEMBER 25, 2009, INDICATES THAT THE SUBJECT PARCEL IS NOT LOCATED WITHIN A SPECIAL FLOOD HAZARD AREA.
- 4. BENCHMARK USED: USGS DISK G-1 #1934 MOUNTED VERTICALLY IN THE NORTHWEST CORNER OF THE TOWN HALL (5'± ABOVE SIDEWALK). ELEV=282.53 (NOV 1929). BENCHMARKS SET: AS NOTED.
- 5. THE LOCATION OF ANY UNDERGROUND UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE. TFMORAN INC. MAKES NO CLAIM TO THE ACCURACY OR COMPLETENESS OF UNDERGROUND UTILITIES SHOWN. PRIOR TO ANY EXCAVATION ON SITE THE CONTRACTOR SHALL CONTACT DIG SAFE AT 811.
- 6. CURRENT ZONING DISTRICT: COMMERCIAL.
- 7. TOWN OF MILFORD STREET RECORDS DEFINE BRIDGE STREET AS A CLASS V HIGHWAY, "33 FEET WIDE AT SWANGING BRIDGE TAPERING TO 26 FEET WIDE AT JUNCTION WITH UNION SQUARE". TOWN OF MILFORD STREET RECORDS DEFINE SCHOOL STREET AS A CLASS V HIGHWAY, APPROXIMATELY 30' WIDE FROM BRIDGE STREET SOUTHERLY TO THE NORTHEAST CORNER OF MIDDLE STREET. STREET LINES AS ESTABLISHED BY THIS SURVEY ARE BASED UPON REFERENCE PLANS 1, 2, 3 AND 6.
- 8. EASEMENTS, RIGHTS, AND RESTRICTIONS SHOWN OR IDENTIFIED ARE THOSE WHICH WERE FOUND DURING RESEARCH PERFORMED AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS. OTHER RIGHTS, EASEMENTS, OR RESTRICTIONS MAY EXIST WHICH A TITLE EXAMINATION OF SUBJECT PARCEL(S) WOULD DETERMINE.

LEGEND

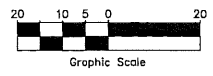
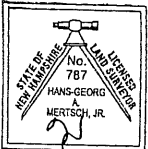
- IRON PIN/CAP TO BE SET
- RAILROAD SPIKE
- GRANITE POST / BOUND
- CURB
- EDGE OF PAVEMENT
- EDGE OF GRAVEL
- RETAINING WALL
- RAILING
- SIGN
- GASGATE
- LIGHT POLE
- GROUND LIGHT
- HYDRANT
- SEWER MANHOLE
- DRAIN MANHOLE
- CATCH BASIN
- WATER GATE
- UTILITY POLE
- SPOT GRADE
- BROOK, STREAM OR RIVER
- LIMITS OF CLEARING / TREELINE
- TREES
- SHRUBLINE
- RIP-RAP
- SEWER LINE
- WATER LINE (SEE NOTE 5)
- DRAIN LINE
- GAS LINE
- OVERHEAD UTILITIES
- UNDERGROUND UTILITIES



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This plan is not effective unless signed by a duly authorized officer of Thomas F. Moran, Inc.

CERTIFICATION

I HEREBY CERTIFY, TO MY KNOWLEDGE, INFORMATION AND BELIEF, THAT THIS PLAN SHOWS THE RESULTS OF AN ON THE GROUND "STANDARD PROPERTY SURVEY" AND THAT SAID SURVEY MEETS THE MINIMUM PRECISION AND/OR ACCURACY MEASUREMENTS FOR SURVEY CLASSIFICATION "U" (URBAN) AS SET FORTH IN TABLE 500.1 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES OF THE BOARD OF LICENSURE FOR LAND SURVEYORS ADOPTED 8/23/01, EFFECTIVE 8/24/01.
I FURTHER CERTIFY THAT THIS PLAN IS THE RESULT OF AN ACTUAL FIELD SURVEY MADE ON THE GROUND AND HAS A MAXIMUM ERROR OF CLOSURE OF ONE PART IN TEN THOUSAND ON ALL PROPERTY LINES WITHIN AND BORDERING THE SUBJECT PROPERTY.

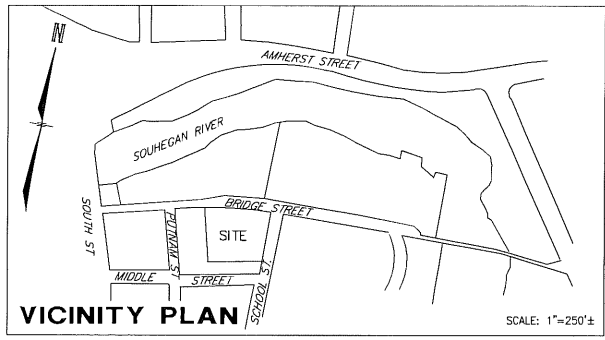
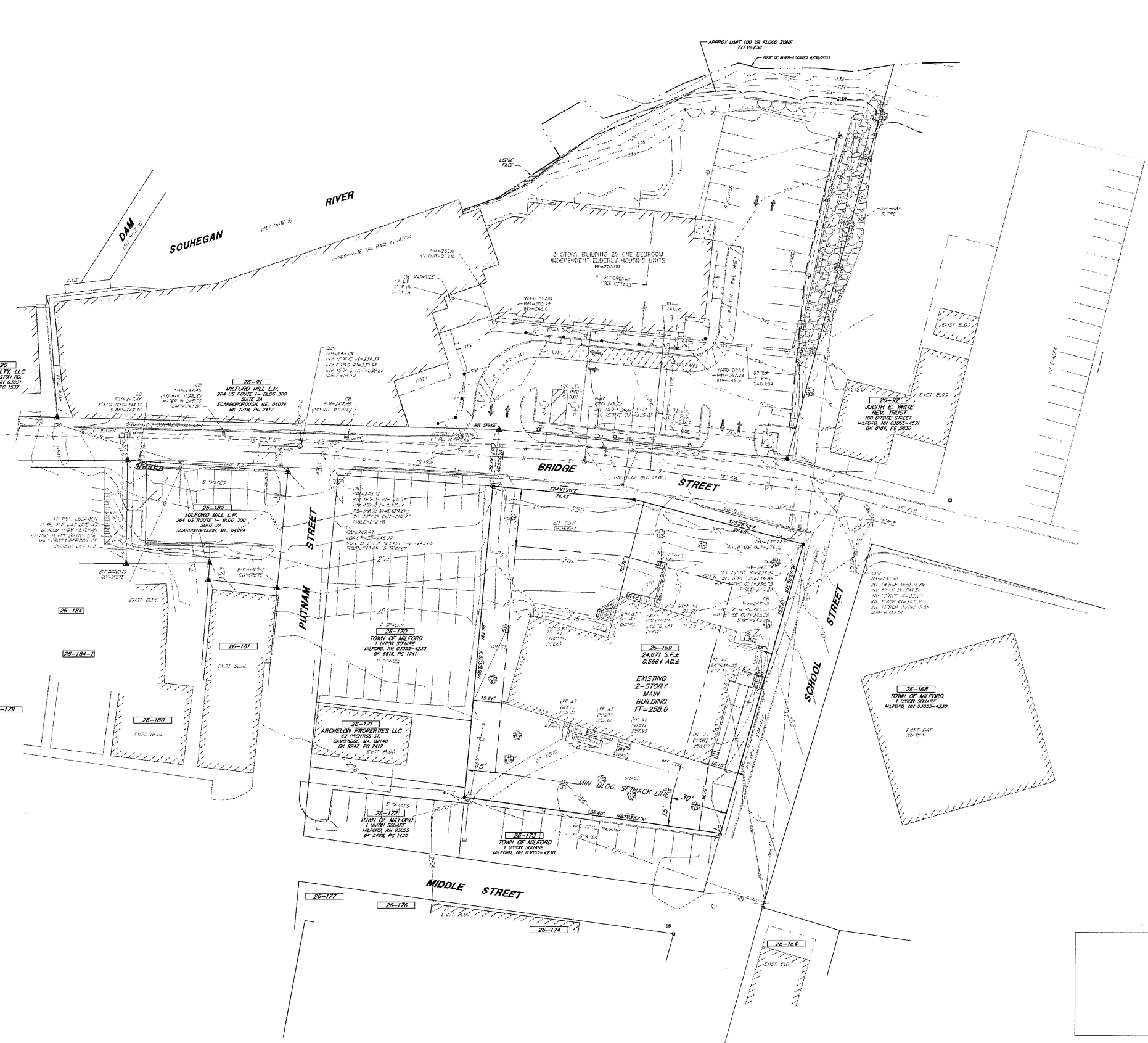


REV.	DATE	DESCRIPTION	DR	CK

TAX MAP 26 LOT 169
BOUNDARY AND EXISTING CONDITIONS PLAN
BRIDGE STREET AND SCHOOL STREET
54 BRIDGE STREET
MILFORD, NEW HAMPSHIRE
OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND
SCALE: 1"=20' JANUARY 18, 2021

TFM Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists
48 Constitution Drive
Bedford, NH 03110
Phone (603) 472-4488
Fax (603) 472-9747
www.tfmoran.com

76451.20 DR STB FB
CK HGM CADFILE 76451-21 SURVEY.DWG SHEET 2 OF 15



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NOTES

- OWNER OF RECORD OF MAP 26 LOT 169: HOUSING INITIATIVES OF NEW ENGLAND CORP., 264 US ROUTE 1-BLDG 300 SUITE 2A, SCARBOROUGH, ME 04074. DEED REFERENCE TO PARCEL IS BOOK 8430, PAGE 911.
AREA OF MAP 26, LOT 169 = 24,671 S.F.± OR 0.5664 ACRES±
- [26-169] INDICATES TAX MAP AND LOT NUMBER.
- CURRENT ZONING: COMMERCIAL AND OVAL SUBDISTRICT
THE SITE IS LOCATED WITHIN THE FOLLOWING OVERLAY DISTRICTS:
-ACQUIFER PROTECTION OVERLAY DISTRICT
-GROUNDWATER PROTECTION DISTRICT LEVEL 1
-OVAL SUBDISTRICT

	REQUIRED	PROVIDED
MIN. LOT SIZE:	20,000 S.F.	24,671 S.F.
MIN. LOT FRONTAGE:	150'	324'
MIN. BUILDING SETBACKS:		
FRONT	30'	5.5'
SIDE/REAR	15'	15.6'
OPEN SPACE:	30%	35.6%

- EXAMINATION OF THE FLOOD INSURANCE RATE MAP FOR HILLSBOROUGH COUNTY, NEW HAMPSHIRE (ALL JURISDICTIONS), MAP NUMBER 330110459D, EFFECTIVE DATE SEPTEMBER 25, 2009, INDICATES THAT THE SUBJECT PARCEL IS NOT LOCATED WITHIN A SPECIAL FLOOD HAZARD AREA.
- THE LOCATION OF ANY UNDERGROUND UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE. TFMORAN INC. MAKES NO CLAIM TO THE ACCURACY OR COMPLETENESS OF UNDERGROUND UTILITIES SHOWN. PRIOR TO ANY EXCAVATION ON SITE THE CONTRACTOR SHALL CONTACT DIG SAFE AT 811.
- TOWN OF MILFORD STREET RECORDS DEFINE BRIDGE STREET AS A CLASS V HIGHWAY, "33 FEET WIDE AT SWINGING BRIDGE TAPERING TO 26 FEET WIDE AT JUNCTION WITH UNION SQUARE".
- TOWN OF MILFORD STREET RECORDS DEFINE SCHOOL STREET AS A CLASS V HIGHWAY, APPROXIMATELY 30' WIDE FROM BRIDGE STREET SOUTHERLY TO THE NORTHEAST CORNER OF MIDDLE STREET.
- STREET LINES AS ESTABLISHED BY THIS SURVEY ARE BASED UPON REFERENCE PLANS 1, 2, 3 AND 6.
- EASEMENTS, RIGHTS, AND RESTRICTIONS SHOWN OR IDENTIFIED ARE THOSE WHICH WERE FOUND DURING RESEARCH PERFORMED AT THE HILLSBOROUGH COUNTY REGISTRY OF DEEDS. OTHER RIGHTS, EASEMENTS, OR RESTRICTIONS MAY EXIST WHICH A TITLE EXAMINATION OF SUBJECT PARCEL(S) WOULD DETERMINE.
- THE SITE IS LOCATED WITHIN THE GROUNDWATER PROTECTION DISTRICT, LEVEL 1.

LEGEND

- IRON PIN/CAP TO BE SET
- RAILROAD SPIKE
- GRANITE POST / BOUND
- CURB
- EDGE OF PAVEMENT
- EDGE OF GRAVEL
- RETAINING WALL
- RAILING
- GASGATE
- LIGHT POLE
- GROUND LIGHT
- HYDRANT
- SEWER MANHOLE
- DRAIN MANHOLE
- CATCH BASIN
- WATER GATE
- UTILITY POLE
- SPOT GRADE
- BROOK, STREAM OR RIVER
- LIMITS OF CLEARING / TREELINE
- TREES
- SHRUBLINE
- RIP-RAP
- SEWER LINE
- WATER LINE (SEE NOTE 5)
- DRAIN LINE
- GAS LINE
- OVERHEAD UTILITIES
- UNDERGROUND UTILITIES

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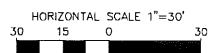
TAX MAP 26 LOT 169
OVERVIEW - EXISTING CONDITIONS PLAN
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
SCALE: 1" = 30'
MARCH 22, 2021

TFM Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists

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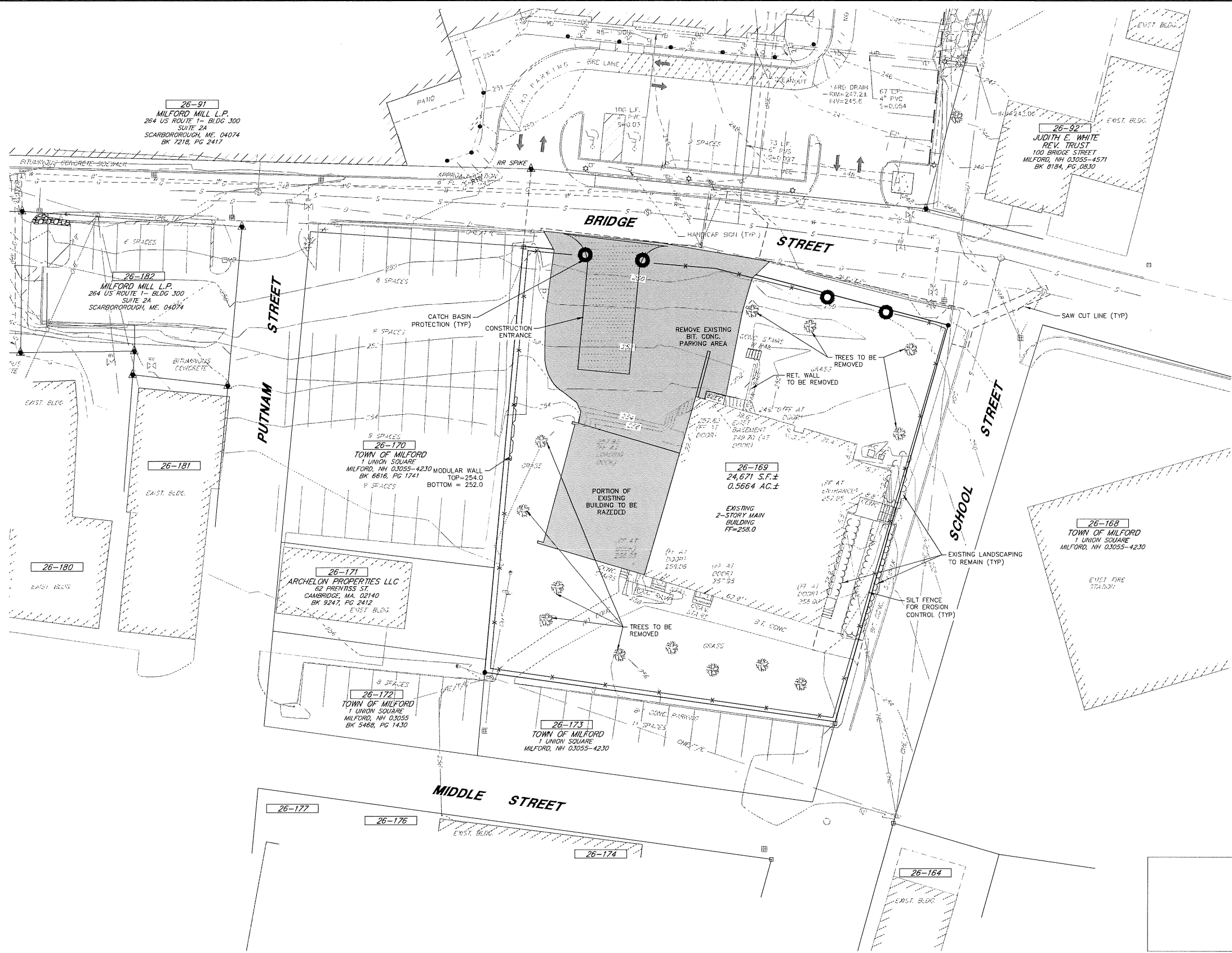
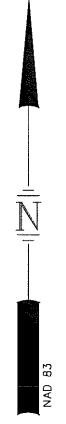
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SHEET 3 OF 15



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- NOTES**
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND DETERMINING THE LOCATIONS, SIZE, AND ELEVATIONS OF ALL EXISTING UTILITIES, SHOWN OR NOT SHOWN ON THESE PLANS PRIOR TO THE START OF ANY DEMOLITION. THE LOCATIONS SHOWN ON THESE PLANS ARE NOT GUARANTEED BY THE OWNER OR THE ENGINEER. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES INTERFERING WITH THE PROPOSED DEMOLITION TO DETERMINE APPROPRIATE ACTION TO BE TAKEN BEFORE PROCEEDING WITH THE WORK. IT IS ALSO THE CONTRACTOR'S RESPONSIBILITY TO ANTICIPATE CONFLICTS AND REPAIR EXISTING UTILITIES AS NECESSARY TO COMPLETE THE WORK AT NO ADDITIONAL COST TO THE OWNER.
 2. THE CONTRACTOR SHALL MAINTAIN EMERGENCY ACCESS TO ALL AREAS AFFECTED BY WORK AT ALL TIMES.
 3. THE CONTRACTOR SHALL VERIFY ALL SURVEY INFORMATION IN THE FIELD AND REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO THE START OF CONSTRUCTION.
 4. EXISTING UTILITY SERVICES TO BE DISCONTINUED ARE TO BE CAPPED AS REQUIRED BY THE RESPECTIVE UTILITY COMPANIES.
 5. CONSTRUCTION DEBRIS AND INVASIVE SPECIES SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A LEGAL MANNER.
 6. PRIOR TO THE START OF WORK, THE CONTRACTOR SHALL PLACE ORANGE CONSTRUCTION FENCING AROUND EACH TREE TO BE RETAINED THROUGHOUT CONSTRUCTION. NO STOCKPILES OF MATERIAL ARE PERMITTED WITHIN THE DRIP LINE OF THE TREES TO BE SAVED.
 7. CONTACT THE LANDSCAPE ARCHITECT IMMEDIATELY IF ANY TREES ARE DAMAGED DURING CONSTRUCTION.

- CONSTRUCTION SEQUENCE NOTES**
- TO MINIMIZE EROSION AND SEDIMENTATION DUE TO CONSTRUCTION, CONSTRUCTION SHALL FOLLOW THIS GENERAL CONSTRUCTION SEQUENCE.
- MODIFICATIONS TO THE SEQUENCE NECESSARY DUE TO THE CONTRACTOR'S SCHEDULE SHALL INCLUDE APPROPRIATE TEMPORARY AND PERMANENT EROSION AND SEDIMENTATION CONTROL MEASURES.
- THE CONTRACTOR SHALL SCHEDULE WORK SUCH THAT ANY CONSTRUCTION AREA IS STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE EXCEPT AS NOTED BELOW. NO MORE THAN 5 ACRES OF DISTURBED LAND SHALL BE UNSTABILIZED AT ANY ONE TIME.
- THE PROJECT SHALL BE MANAGED SO THAT IT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER ARC 3800 RELATIVE TO INVASIVE SPECIES.
1. INSTALL ALL PERIMETER EROSION PROTECTION MEASURES AS INDICATED ON THE PLANS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
 2. DURING CONSTRUCTION EVERY EFFORT SHALL BE MADE TO MANAGE SURFACE RUNOFF QUALITY.
 3. DAILY, OR AS REQUIRED, CONSTRUCT TEMPORARY BERMS, DRAINS, DITCHES, SILT BARRIERS, SEDIMENT TRAPS, ETC. MULCH AND SEED AS REQUIRED. (TEMPORARY SEED MIXTURE OF WINTER RYE APPLIED AT A RATE OF 2.5 LBS/1000 SF SHALL BE USED).
 4. CONDUCT MAJOR EARTHWORK, INCLUDING CLEARING AND GRUBBING, WITHIN THE LIMITS OF WORK. ALL CUT AND FILL SLOPES SHALL BE SEEDED WITHIN 72 HOURS AFTER GRADING.
 5. ALL STRIPPED TOPSOIL AND OTHER EARTH MATERIALS SHALL BE STOCKPILED OUTSIDE THE IMMEDIATE WORK AND WETLAND AREAS. A SILT BARRIER SHALL BE CONSTRUCTED AROUND THESE PILES IN A MANNER TO PROVIDE ACCESS AND AVOID SEDIMENT OUTSIDE OF THE WORK AREA.
 6. CONSTRUCT BUILDING PAD AND COMMENCE NEW BUILDING CONSTRUCTION.
 7. CONSTRUCT TEMPORARY CULVERTS AND DIVERSIONS AS REQUIRED.
 8. BEGIN PERMANENT AND TEMPORARY INSTALLATION OF SEED AND MULCH.
 9. PERFORM EARTHWORK NECESSARY TO ESTABLISH ROUGH GRADING AROUND PARKING FIELDS AND ACCESS DRIVES. MANAGE EXPOSED SOIL SURFACES TO AVOID TRANSPORTING SEDIMENTS INTO WETLANDS. PARKING LOTS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
 10. INSTALL SUBSURFACE UTILITIES (WATER, SEWER, GAS, ELECTRIC, COMMUNICATIONS, DRAINAGE, DRAINAGE FACILITIES, ETC.).
 11. COMPLETE BUILDING.
 12. COMPLETE SEEDING AND MULCHING. SEED TO BE APPLIED WITH BROADCAST SPREADER OR BY HYDRO-SEEDING, THEN ROLLED, RAKED, OR DRAGGED TO ASSURE SEED/SOIL CONTACT.
 13. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER SEEDED AREAS HAVE BECOME FIRMLY ESTABLISHED AND SITE IMPROVEMENTS ARE COMPLETE.
 14. DURING THE COURSE OF THE WORK AND UPON COMPLETION, THE CONTRACTOR SHALL REMOVE ALL SEDIMENT DEPOSITS, EITHER ON OR OFF SITE, INCLUDING CATCH BASINS, AND SUMPS, DRAIN PIPES AND DITCHES, CURB LINES, ALONG SILT BARRIERS, ETC. RESULTING FROM SOIL AND/OR CONSTRUCTION OPERATIONS.
 15. SEE WINTER CONSTRUCTION SEQUENCE FOR WRK CONDUCTED AFTER OCTOBER 15TH.

TAX MAP 26 LOT 169
SITE PREPARATION PLAN
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.

SCALE: 1" = 20' MARCH 22, 2021

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HORIZONTAL SCALE 1"=20'

REV	DATE	DESCRIPTION	DR	CK

TFM Civil Engineers
 Structural Engineers
 Traffic Engineers
 Land Surveyors
 Landscape Architects
 Scientists

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 L CK JK CADFILE
 E 76451-21 SITE SHEET 4 OF 15

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CONTACT THE STATE TO REMOVE THIS SIGN PRIOR TO CONSTRUCTION

HORIZONTAL SCALE 1"=20'

REV	DATE	DESCRIPTION	DR	CK

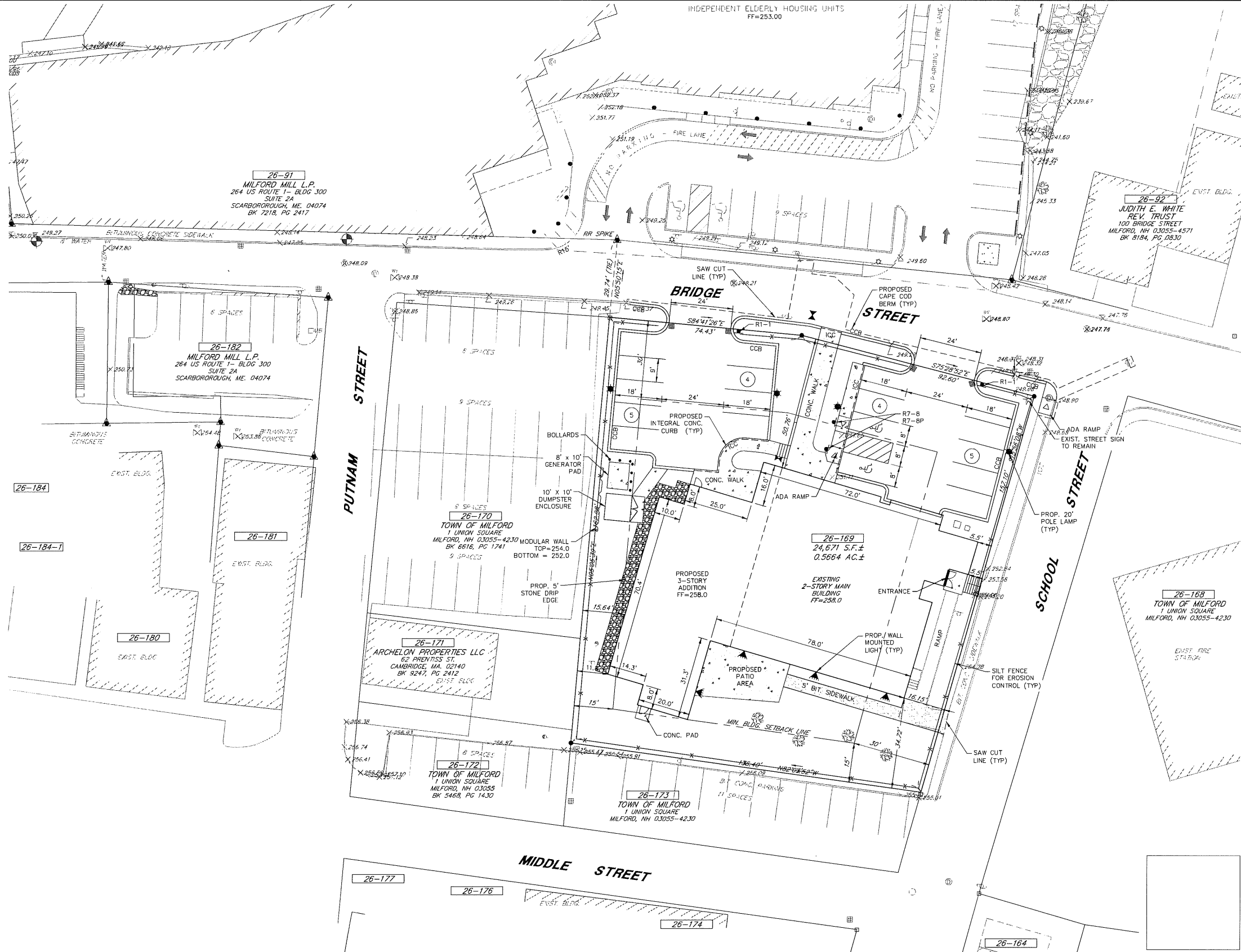
- ### NOTES
- OWNER OF RECORD OF MAP 26 LOT 169 - HOUSING INITIATIVES OF NEW ENGLAND CORP, 264 US RTE. 1 - BLDG 300, SUITE 2A, SCARBOROUGH, ME 04074. DEED REFERENCE TO PARCEL IS BOOK 8430, PAGE 0911.
 - AREA OF MAP 26, LOT 169 = 24,671 S.F. ± OR 0.566 ACRES ±
 - [26-169] INDICATES TAX MAP AND LOT NUMBER.
 - THE PURPOSE OF THIS PLAN IS TO SHOW AN ADDITION TO THE EXISTING BUILDING AND RENOVATE TO CHANGE THE USE TO INDEPENDENT SENIOR HOUSING. PROPOSING 3-STORY 3,330 SF FOOTPRINT ADDITION, RENOVATE EXISTING BUILDING TO CREATE AN 18 UNIT APARTMENT BUILDING.
 - CURRENT ZONING: COMMERCIAL AND OVAL SUBDISTRICT
- THE SITE IS LOCATED WITHIN THE FOLLOWING OVERLAY DISTRICTS:
- AQUIFER PROTECTION OVERLAY DISTRICT
 - GROUNDWATER PROTECTION DISTRICT LEVEL 1
 - OVAL SUBDISTRICT
- | MIN. LOT SIZE: | REQUIRED | PROVIDED |
|-------------------------|-------------|----------------------|
| MIN. LOT FRONTAGE: | 20,000 S.F. | 24,671 S.F. |
| MIN. BUILDING SETBACKS: | 150' | 324' |
| FRONT: | 30' | 5.5' (GRANDFATHERED) |
| SIDE/REAR: | 15' | 11.8' AND >15' |
| OPEN SPACE: | 30% | 31% |
- THE PROPERTY WILL BE SERVICED BY THE FOLLOWING SERVICES:
 STORM DRAINAGE: PRIVATE AND MUNICIPAL
 SANITARY SEWER: MUNICIPAL
 WATER: MUNICIPAL
 GAS: LIBERTY UTILITIES
 ELECTRIC: EVERSOURCE
 TELEPHONE: CONSOLIDATED COMMUNICATIONS
 - PARKING CALCULATIONS REQUIRED:
 SENIOR HOUSING DEVELOPMENT: 1.5 SP./UNIT X 18 UNITS = 27 SPACES REQ.
 PROPOSED: 18 SPACES (INCL. 2 ADA) - ON SITE
 10 SPACES (PARKING EASEMENT ON LOT 26-92)
 28 SPACES TOTAL
 - IN THE EVENT THAT THE SNOW STORAGE AREAS PROVIDED ON THE SITE ARE COMPLETELY UTILIZED, EXCESS SNOW SHALL BE TRANSPORTED OFF SITE FOR DISPOSAL IN ACCORDANCE WITH N.H.D.E.S. REGULATIONS.
 - THE CONTRACTOR SHALL BID AND PERFORM THE WORK IN ACCORDANCE WITH ALL LOCAL, STATE AND NATIONAL CODES, SPECIFICATIONS, REGULATIONS AND STANDARDS.
 - LIGHTING, SIGNAGE, LANDSCAPING AND SCREENING SHALL MEET THE REQUIREMENTS OF THE TOWN OF MILFORD ZONING ORDINANCE AND SITE PLAN REGULATIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS NOTED OTHERWISE.
 - WRITTEN DIMENSIONS HAVE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN CASE OF CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWING AND/OR SPECIFICATION, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY FOR CLARIFICATIONS.
 - THE CONTRACTOR IS RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THESE PLANS, PREPARED BY TFMORAN, INC., DO NOT EXTEND TO OR INCLUDE SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR THEIR EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF THE SURVEYOR OR ENGINEER HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND/OR LOCAL REGULATIONS.
 - WATER, SEWER, ROAD (INCLUDING PARKING LOT) AND DRAINAGE WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH TOWN OF MILFORD'S WATER UTILITIES DEPARTMENT AND PUBLIC WORKS DEPARTMENT STANDARDS.
 - AS-BUILT PLANS SHALL BE DELIVERED TO THE BUILDING DEPARTMENT PRIOR TO A CERTIFICATE OF OCCUPANCY BEING ISSUED.

TAX MAP 26 LOT 169
SITE PLAN
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
 SCALE: 1" = 20' MARCH 22, 2021

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	Traffic Engineers	Phone (603) 472-4488
	Land Surveyors	Fax (603) 472-9747
	Landscape Architects	www.tfmor.com
	Scientists	

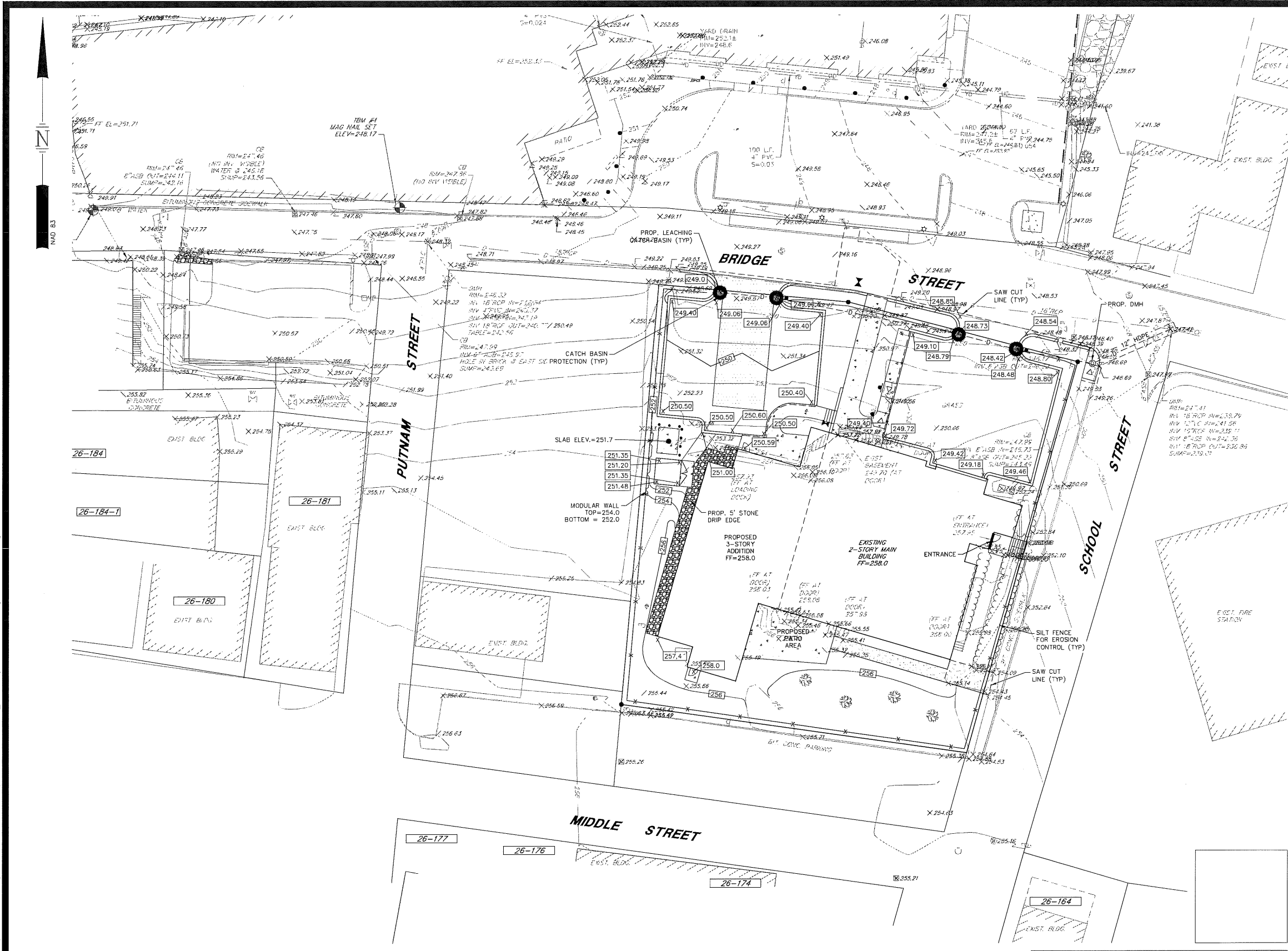
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SHEET 5 OF 15



NOTES

1. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF MILFORD, AND SHALL BE BUILT IN A WORKMANLIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH THE SITE AND ALL SURROUNDING CONDITIONS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND DETERMINING THE LOCATION, SIZE AND ELEVATION OF ALL EXISTING UTILITIES, SHOWN OR NOT SHOWN ON THESE PLANS, PRIOR TO THE START OF ANY CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES FOUND INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROPRIATE REMEDIAL ACTION BE AGREED TO BY THE ENGINEER BEFORE PROCEEDING WITH THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT "DIGSAFE" (811) AT LEAST 72 HOURS BEFORE DIGGING.
4. THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES OWNING UTILITIES, EITHER OVERHEAD OR UNDERGROUND, WITHIN THE CONSTRUCTION AREA AND SHALL COORDINATE AS NECESSARY WITH THE UTILITY COMPANIES OF SAID UTILITIES. THE PROTECTION OR RELOCATION OF UTILITIES IS ULTIMATELY THE RESPONSIBILITY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL COORDINATE MATERIALS AND INSTALLATION SPECIFICATIONS WITH THE INDIVIDUAL UTILITY AGENCIES/COMPANIES, AND ARRANGE FOR ALL INSPECTIONS.
6. ROAD AND DRAINAGE CONSTRUCTION SHALL CONFORM TO THE TYPICAL SECTIONS AND DETAILS SHOWN ON THE PLANS, AND SHALL MEET LOCAL STANDARDS AND THE REQUIREMENTS OF THE LATEST NHDOT STANDARD SPECIFICATIONS FOR ROADS AND BRIDGE CONSTRUCTION AND THE NHDOT STANDARD STRUCTURE DRAWINGS UNLESS OTHERWISE NOTED.
7. STORM DRAINAGE SYSTEM SHALL BE CONSTRUCTED TO LINE AND GRADE AS SHOWN ON THE PLANS. CONSTRUCTION METHODS SHALL CONFORM TO NHDOT STANDARD SPECIFICATIONS, SECTION 603. CATCH BASINS AND DRAIN MANHOLES SHALL CONFORM TO SECTION 604. ALL CATCH BASIN GRATES SHALL BE TYPE B AND CONFORM TO NHDOT STANDARDS AND SPECIFICATIONS UNLESS OTHERWISE NOTED.
8. ALL MANHOLES IN PAVEMENT SHALL HAVE RIMS SET TO FINISH GRADE REGARDLESS OF ANY ELEVATIONS OTHERWISE SHOWN.
9. ALL ELEVATIONS SHOWN AT CURB ARE TO THE BOTTOM OF CURB UNLESS OTHERWISE NOTED. CURBS HAVE A 6" REVEAL UNLESS OTHERWISE NOTED.
10. ALL EXCAVATIONS SHALL BE THOROUGHLY SECURED ON A DAILY BASIS BY THE CONTRACTOR AT THE COMPLETION OF CONSTRUCTION OPERATIONS IN THE IMMEDIATE AREA.
11. THE EROSION CONTROL PRACTICES SHOWN ON THESE PLANS ARE ILLUSTRATIVE ONLY AND SHALL BE SUPPLEMENTED BY THE SITE CONTRACTOR AS NEEDED.
12. COORDINATE WITH ARCHITECTURAL PLANS FOR DETAILED GRADING AT BUILDING, AND SIZE AND LOCATION OF ALL BUILDING SERVICES.
13. COORDINATE WITH GEOTECHNICAL/STRUCTURAL PLANS FOR SITE PREPARATION AND OTHER BUILDING INFORMATION.
14. COORDINATE WITH MECHANICAL AND PLUMBING PLANS FOR ROOF DRAIN INFORMATION.
15. LIMITS OF WORK ARE SHOWN AS APPROXIMATE. THE CONTRACTOR SHALL COORDINATE ALL WORK TO PROVIDE SMOOTH TRANSITIONS. THIS INCLUDES GRADING, PAVEMENT, CURBING, SIDEWALKS AND ALIGNMENTS.
16. THE CONTRACTOR SHALL REFER TO THE GEOTECHNICAL REPORT FOR INFORMATION ABOUT GROUNDWATER CONDITIONS. THE CONTRACTOR SHALL FOLLOW THE GEOTECHNICAL ENGINEERS RECOMMENDED METHODS TO ADDRESS ANY GROUNDWATER ISSUES THAT ARE FOUND ON SITE.
17. THE CONTRACTOR IS RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR THE CONDITIONS AT THE SITE. WRITTEN DIMENSIONS HAVE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND REPORT DISCREPANCIES TO THE ENGINEER.
18. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CHECK THE ACCURACY OF THE TOPOGRAPHY AND REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO ANY EARTHWORK BEING PERFORMED ON THE SITE. NO CLAIM FOR EXTRA WORK WILL BE CONSIDERED FOR PAYMENT AFTER EARTHWORK HAS COMMENCED.
19. VERIFY TBM ELEVATIONS PRIOR TO CONSTRUCTION.
20. IN THE EVENT OF A CONFLICT BETWEEN PLANS, SPECIFICATIONS, AND DETAILS, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY FOR CLARIFICATION.
21. IF CONDITIONS AT THE SITE ARE DIFFERENT THAN SHOWN THE ENGINEER SHALL BE NOTIFIED PRIOR TO PROCEEDING WITH THE AFFECTED WORK.
22. THESE PLANS WERE PREPARED UNDER THE SUPERVISION OF A LICENSED PROFESSIONAL ENGINEER. TFMORAN INC. ASSUMES NO LIABILITY AS A RESULT OF ANY CHANGES OR NON-COMFORMANCE WITH THESE PLANS EXCEPT UPON THE WRITTEN APPROVAL OF THE ENGINEER OF RECORD.
23. TFMORAN INC. ASSUMES NO LIABILITY FOR WORK PERFORMED WITHOUT AN ACCEPTABLE PROGRAM OF TESTING AND INSPECTION AS APPROVED BY THE ENGINEER OF RECORD.
24. THE SITE CONTRACTOR SHALL ENSURE THAT ALL WORK IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF NHDES ENV-WQ 1500 AS APPLICABLE.
25. AT COMPLETION OF CONSTRUCTION, THE SITE CONTRACTOR SHALL PROVIDE A LETTER CERTIFYING THAT THE PROJECT WAS COMPLETED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS, AND A LETTER STAMPED BY A QUALIFIED ENGINEER THAT THEY HAVE OBSERVED ALL UNDERGROUND DETENTION SYSTEMS, INFILTRATION SYSTEMS, OR FILTERING SYSTEMS PRIOR TO BACKFILL, AND THAT SUCH SYSTEMS CONFORM TO THE APPROVED PLANS AND SPECIFICATIONS.
26. IF ANY DEVIATIONS FROM THE APPROVED PLANS AND SPECIFICATIONS HAVE BEEN MADE, THE SITE CONTRACTOR SHALL PROVIDE AS-BUILT DRAWINGS STAMPED BY A LICENSED SURVEYOR OR QUALIFIED ENGINEER ALONG WITH A LETTER STAMPED BY A QUALIFIED ENGINEER DESCRIBING ALL SUCH DEVIATIONS, AND BEAR ALL COSTS FOR PREPARING AND FILING ANY NEW PERMITS OR PERMIT AMENDMENTS THAT MAY BE REQUIRED.




TAX MAP 26 LOT 169
GRADING AND DRAINAGE PLAN
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.

SCALE: 1" = 20' MARCH 22, 2021

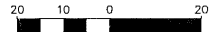
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HORIZONTAL SCALE 1"=20'



REV	DATE	DESCRIPTION	DR	CK



Civil Engineers
 Structural Engineers
 Traffic Engineers
 Land Surveyors
 Landscape Architects
 Scientists

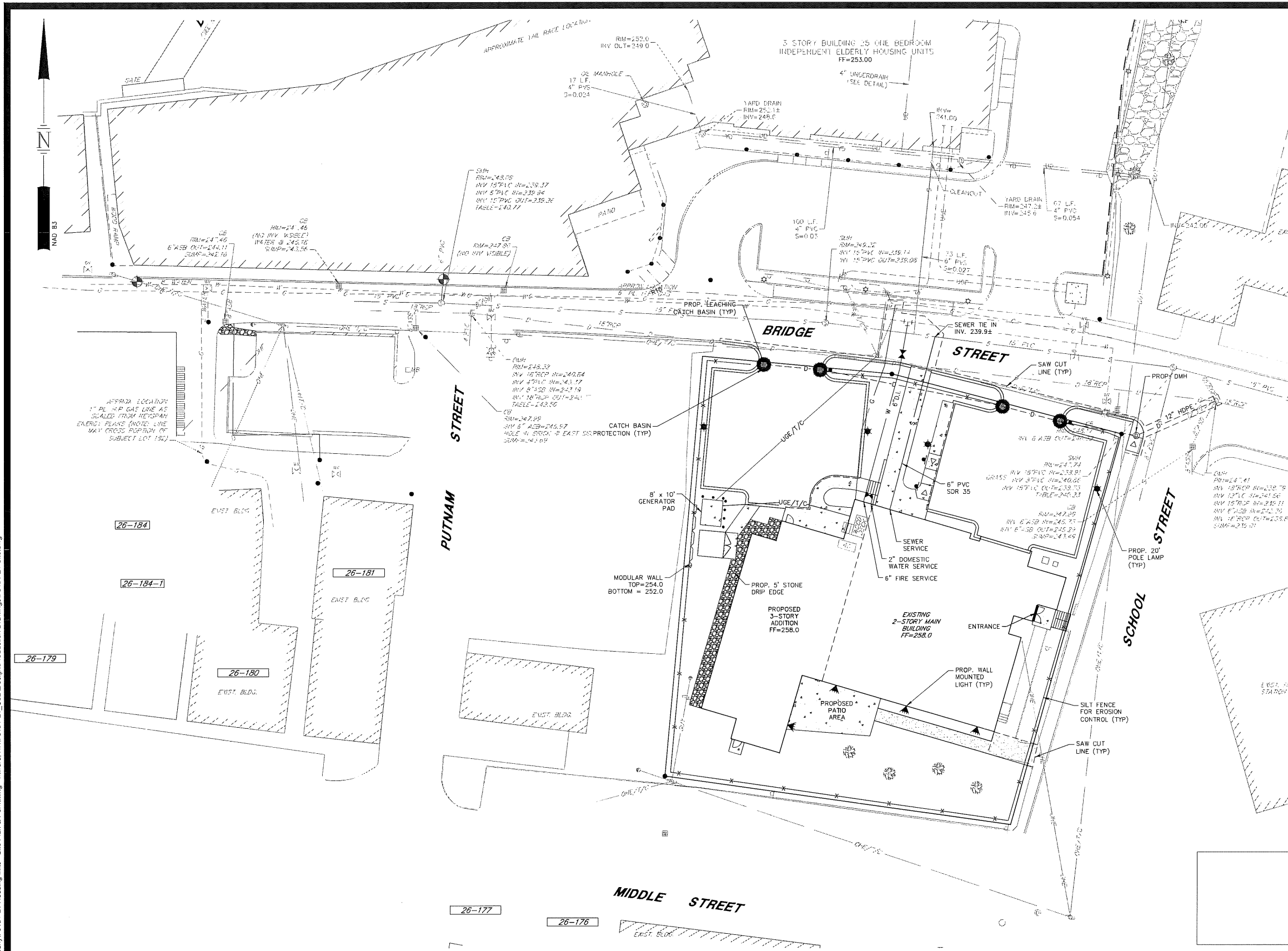
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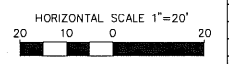
NOTES

1. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF MILFORD, AND SHALL BE BUILT IN A WORKMANLIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH THE SITE AND ALL SURROUNDING CONDITIONS.
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4. THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES OWNING UTILITIES, EITHER OVERHEAD OR UNDERGROUND, WITHIN THE CONSTRUCTION AREA AND SHALL COORDINATE AS NECESSARY WITH THE UTILITY COMPANIES OF SAID UTILITIES. THE PROTECTION OR RELOCATION OF UTILITIES IS ULTIMATELY THE RESPONSIBILITY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL COORDINATE MATERIALS AND INSTALLATION SPECIFICATIONS WITH THE INDIVIDUAL UTILITY AGENCIES/COMPANIES, AND ARRANGE FOR ALL INSPECTIONS.
6. ALL MANHOLES IN PAVEMENT SHALL HAVE RIMS SET TO FINISH GRADE REGARDLESS OF ANY ELEVATIONS OTHERWISE SHOWN.
7. SANITARY SEWER SHALL BE CONSTRUCTED TO THE STANDARDS AND SPECIFICATIONS AS SHOWN ON THESE PLANS. ALL SEWER MAINS AND FITTINGS SHALL BE PVC AND SHALL CONFORM TO ASTM F 679 (SDR 35 MINIMUM). SANITARY MANHOLES SHALL CONFORM TO NHDES WATER DIVISION WASTEWATER ENGINEERING BUREAU STANDARDS AND SPECIFICATIONS SHOWN HEREON.
8. ON-SITE WATER DISTRIBUTION SHALL BE TO MILFORD WATER DEPARTMENT STANDARDS AND SPECIFICATIONS. WATER MAINS SHALL HAVE A MINIMUM OF 5.5 FEET COVER, WHERE WATER PIPES CROSS SEWER LINES A MINIMUM OF 18" VERTICAL SEPARATION BETWEEN THE TWO OUTSIDE PIPE WALLS SHALL BE OBSERVED. HORIZONTAL SEPARATION BETWEEN WATER AND SEWER SHALL BE 10 FEET MINIMUM.
9. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR CONDUIT AND WIRING TO ALL SIGNS AND LIGHTS. CONDUIT TO BE A MINIMUM OF 24" BELOW FINISH GRADE.
10. VERIFY UTILITY CONNECTIONS TO BUILDING WITH ARCHITECTURAL DRAWINGS.
11. ALL EXCAVATIONS SHALL BE THOROUGHLY SECURED ON A DAILY BASIS BY THE CONTRACTOR AT THE COMPLETION OF CONSTRUCTION OPERATIONS IN THE IMMEDIATE AREA.
13. LIMITS OF WORK ARE SHOWN AS APPROXIMATE. THE CONTRACTOR SHALL COORDINATE ALL WORK TO PROVIDE SMOOTH TRANSITIONS. THIS INCLUDES GRADING, PAVEMENT, CURBING, SIDEWALKS AND ALIGNMENTS.
14. THE CONTRACTOR IS RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR THE CONDITIONS AT THE SITE. WRITTEN DIMENSIONS HAVE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND REPORT DISCREPANCIES TO THE ENGINEER.
15. UNLESS OTHERWISE SPECIFIED, ALL UNDERGROUND STRUCTURES, PIPES, CHAMBERS, ETC. SHALL BE COVERED WITH A MINIMUM OF 18" OF COMPACTED SOIL BEFORE EXPOSURE TO VEHICLE LOADS.



TAX MAP 26 LOT 169
UTILITY PLAN
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
 SCALE: 1" = 20'
 MARCH 22, 2021

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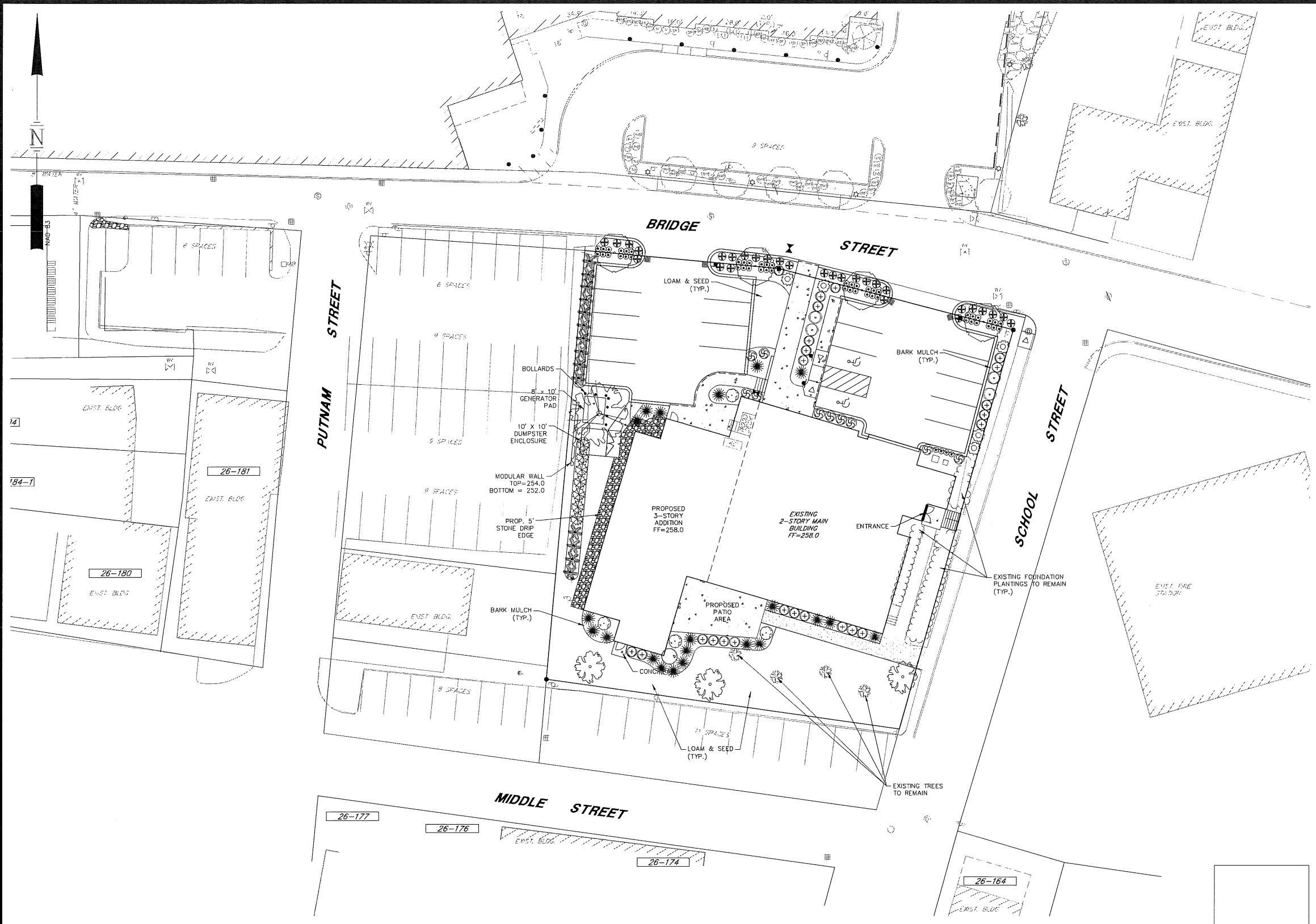


REV	DATE	DESCRIPTION	DR	CK

	Civil Engineers Structural Engineers Traffic Engineers Land Surveyors Landscape Architects Scientists	48 Constitution Drive Bedford, NH 03110 Phone (603) 472-4488 Fax (603) 472-9747 www.tfmoran.com
	76451.21	DR SRP FB CK JK CADFILE 76451-21 SITE

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LANDSCAPE LEGEND				
SYMBOL	QTY	BOTANICAL NAME COMMON NAME	SIZE	REMARKS
	3	ACER R. 'BOWHALL' BOWHALL RED MAPLE	2 1/2" TO 3" CAL.	B&B
	1	PLATANUS X ACREIFOLIA 'BLOODGOOD' BLOODGOOD LONDON PLANETREE	2 1/2" TO 3" CAL.	B&B
	4	PYRUS C. 'CHANTICLEER' CHANTICLEER FLOWERING PEAR	2 1/2" TO 3" CAL.	B&B
	10	EUONYMUS F. 'MOONSHADOW' MOONSHADOW WINTERCREEPER	3 GAL.	CONT.
	24	HYDRANGEA MACROPHYLLA 'PENNY MAC' PENNY MAC HYDRANGEA	3 GAL.	CONT.
	4	HYDRANGEA PANICULATA 'FIRE LIGHT' FIRE LIGHT PANICLE HYDRANGEA	2 1/2" TO 3"	B&B
	5	JUNIPERUS C. 'PFITZERIANA COMPACTA' COMPACT PFITZER JUNIPER	3 GAL.	CONT.
	4	RHODODENDRON 'LEE'S DARK PURPLE' LEE'S DARK PURPLE RHODODENDRON	2' TO 2 1/2'	B&B
	21	TAXUS M. 'GREENWAVE' GREENWAVE YEW	18" TO 24"	B&B
	7	THUJA O. 'TECHNY' MISSION ARBORVITAE	5' TO 6'	B&B
	2	VIBURNUM PRUNIFOLIUM 'RED ARROW' RED ARROW BLACKHAW VIBURNUM	4' TO 5'	B&B
	52	BOUPELLOJA 'BLONDE AMBITIONS' BLONDE AMBITION BLUE GRAMA GRASS	2 GAL.	CONT.
	14	MISCANTHUS S. 'VAREGATA' VAREGATED SILVER GRASS	3 GAL.	CONT.
	29	ARCTOSTAPHYLOS UVA-URSI BEARBERRY	1 GAL.	CONT.

LANDSCAPE NOTES
 (SEE DETAILS FOR ADDITIONAL NOTES)
 MILFORD DEVELOPMENT LANDSCAPE STANDARDS

MILFORD DEVELOPMENT LANDSCAPE STANDARDS ARE INTENDED AS SUGGESTED GUIDELINES FOR THE OVAL SUB-DISTRICT, BUT NOT REQUIRED.

GENERAL

- THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE RULES, REGULATIONS, LAWS, AND ORDINANCES HAVING JURISDICTION OVER THIS PROJECT SITE.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES AND NOTIFY OWNER'S REPRESENTATIVE OF CONFLICTS.
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL QUANTITIES SHOWN ON PLANS BEFORE PRICING THE WORK. ANY DIFFERENCE IN QUANTITIES SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR CLARIFICATION. LANDSCAPE QUANTITIES SHOWN ON THE PLAN SHALL SUPERCEDE QUANTITIES LISTED IN LANDSCAPE LEGEND.
- THE CONTRACTOR SHALL CONTACT THE LANDSCAPE ARCHITECT PRIOR TO STARTING WORK AND VERIFY THAT THE PLANS IN THE CONTRACTOR'S POSSESSION ARE THE MOST CURRENT PLANS AVAILABLE AND ARE THE APPROVED PLAN SET FOR USE IN CONSTRUCTION.
- ALL PLANT MATERIALS INSTALLED SHALL MEET OR EXCEED THE SPECIFICATIONS OF THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION (ANLA) [FORMERLY THE AMERICAN ASSOCIATION OF NURSERMEN] IN THE AMERICAN STANDARD FOR NURSERY STOCK (AS AMENDED) [ANSI Z60.1-1996].
- ALL PLANTS SHALL BE FIRST CLASS AND SHALL BE REPRESENTATIVE OF THEIR NORMAL SPECIES AND/OR VARIETIES. ALL PLANTS MUST HAVE GOOD, HEALTHY, WELL-FORMED UPPER GROWTH AND A LARGE, FIBROUS, COMPACT ROOT SYSTEM.
- ALL PLANTS SHALL BE FREE FROM DISEASE AND INSECT PESTS AND SHALL COMPLY WITH ALL APPLICABLE STATE AND FEDERAL LAWS PERTAINING TO PLANT DISEASES AND INFESTATIONS.
- ALL TREES SHALL BE BALLED AND BURLAPPED (B & B) UNLESS OTHERWISE NOTED OR APPROVED BY LANDSCAPE ARCHITECT.
- IF APPLICABLE, THE CONTRACTOR SHALL HAVE ALL FALL TRANSPLANTING HAZARD PLANTS DUG IN THE SPRING AND STORED FOR FALL PLANTING.
- ALL INVASIVE PLANT SPECIES FROM THE "NEW HAMPSHIRE PROHIBITED INVASIVE PLANT SPECIES LIST", TO BE REMOVED SHALL BE DONE SO IN ACCORDANCE WITH THE "INVASIVE SPECIES ACT, HB 1258-FN."

GUARANTEE
 THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL LANDSCAPE WORK FOR A PERIOD OF ONE YEAR, BEGINNING AT THE START OF THE MAINTENANCE PERIOD.

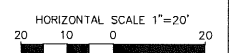
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LANDSCAPE PLAN
MILFORD INDEPENDENT SENIOR HOUSING
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SCALE: 1" = 20' MARCH 22, 2021

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CONSTRUCTION GENERAL PERMIT

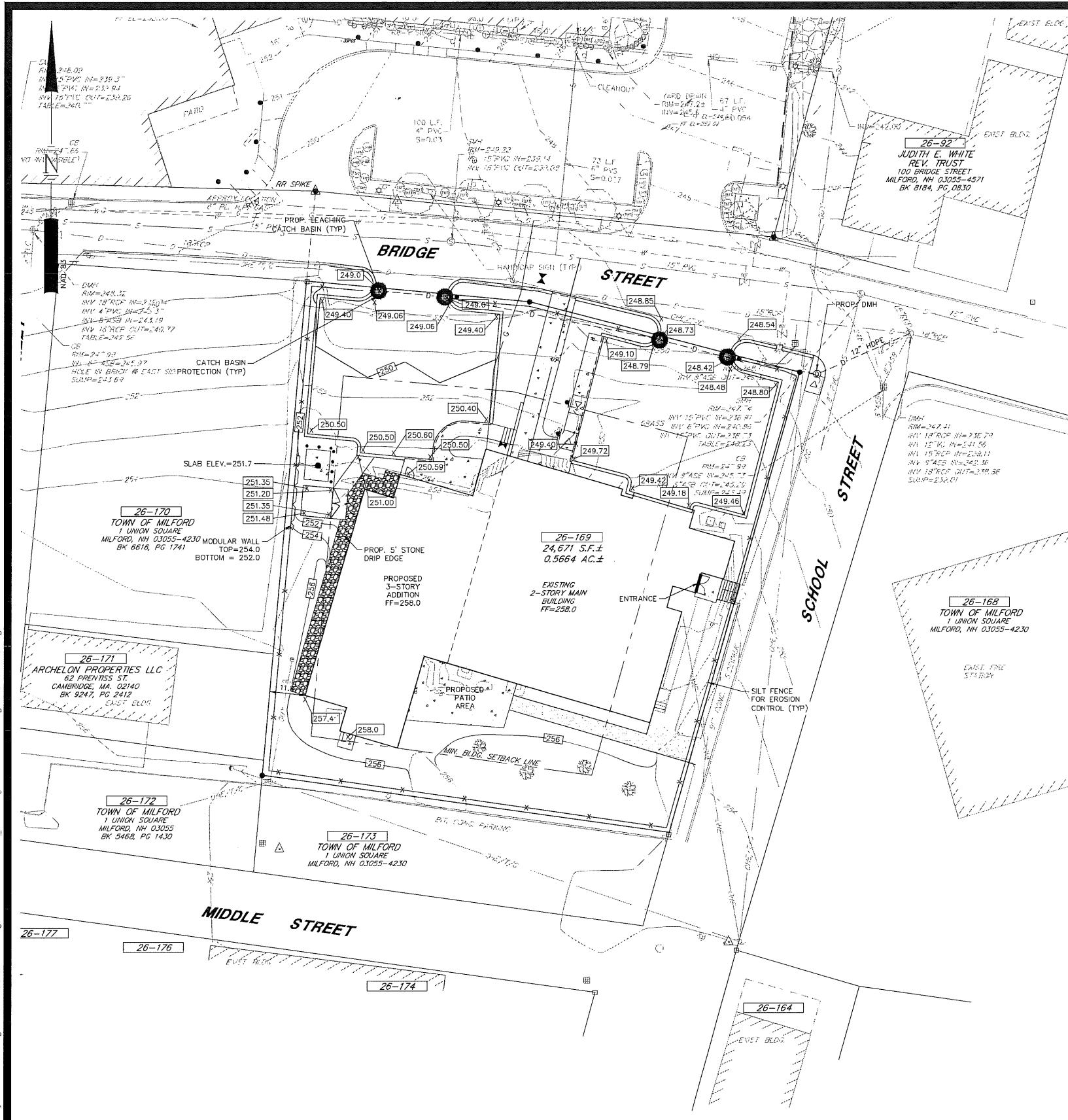
1. THE OWNER, IN CONJUNCTION WITH THE CONTRACTOR (OPERATORS), MUST OBTAIN A CONSTRUCTION GENERAL PERMIT (CGP) FOR LARGE CONSTRUCTION ACTIVITIES (FIVE OR MORE ACRES) OR SMALL CONSTRUCTION ACTIVITIES (GREATER THAN ONE ACRE BUT LESS THAN FIVE ACRES) FROM THE ENVIRONMENTAL PROTECTION AGENCY (EPA). AS PART OF THE CGP, A STORMWATER NOTICE OF INTENT (NOI) MUST BE SUBMITTED TO THE EPA AT LEAST 7 DAYS PRIOR TO COMMENCING CONSTRUCTION. THE NOI MUST BE SUBMITTED TO STORM WATER NOTICE OF INTENT (4203M), USEPA, 1200 PENNSYLVANIA AVE. NW, WASHINGTON, DC 20460.
2. THE CGP OUTLINES A SET OF PROVISIONS MANDATING THE OWNER AND CONTRACTOR TO COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER REGULATIONS, INCLUDING, BUT NOT LIMITED TO, STORM WATER POLLUTION PREVENTION PLANS (SWPPPs), IMPLEMENTATION OF EROSION AND SEDIMENTATION CONTROLS, EQUIPMENT MAINTENANCE GUIDELINES, ETC. PLEASE CONTACT USEPA OFFICE OF WASTE WATER MANAGEMENT AT 202-564-9545 OR AT WWW.EPA.GOV/NPDES/STORMWATER FOR ADDITIONAL INFORMATION. FOR FURTHER ASSISTANCE, CONTACT ABBY SWANE OF NEW ENGLAND'S EPA REGION 1 AT 617-918-1841.

NOTES

1. IT IS BEING PROPOSED TO SHOW AN ADDITION TO THE EXISTING BUILDING AND RENOVATE TO CHANGE THE USE TO INDEPENDENT SENIOR HOUSING.
2. TOTAL SITE AREA: 0.56 AC
TOTAL AREA OF DISTURBANCE: 0.31 AC
3. SOILS SHOWN ARE FROM THE SOIL SURVEY OF HILLSBOROUGH COUNTY, NH, EASTERN PART, PREPARED BY USDA-NATIONAL RESOURCE CONSERVATION SERVICES.
H8B - HINCKLY LOAMY SAND
4. STORM WATER DRAINAGE SYSTEM IS SHOWN ON THE PLAN. SEE GRADING & DRAINAGE PLAN FOR RIM, INVERT, PIPE LENGTH, AND SLOPE INFORMATION.
POST-CONSTRUCTION RUNOFF COEFFICIENT: C=0.79
IMPERVIOUS SURFACE AREA: 0.39± AC
5. STABILIZATION PRACTICES FOR EROSION AND SEDIMENTATION CONTROLS:
TEMPORARY STABILIZATION - TOPSOIL STOCKPILES AND DISTURBED AREAS OF THE CONSTRUCTION SITE THAT WILL NOT BE REDISTURBED FOR 14 DAYS OR MORE MUST BE STABILIZED BY THE 14TH DAY AFTER THE LAST DISTURBANCE. THE TEMPORARY SEED SHALL BE ANNUAL RYE APPLIED AT THE RATE OF 1.1 LBS PER 1,000 SF. PRIOR TO SEEDING, A MINIMUM OF 2 TONS PER ACRE OF AGRICULTURAL LIMESTONE AND 500 LBS PER ACRE OF 10-20-20 FERTILIZER SHALL BE APPLIED. AFTER SEEDING, EACH AREA SHALL BE MULCHED WITH 1.5 TONS PER ACRE OF HAY MULCH. MULCH TO BE ANCHORED IN PLACE WHERE NECESSARY. AREAS OF THE SITE THAT WILL BE PAVED WILL BE TEMPORARILY STABILIZED BY APPLYING GEOTEXTILES AND A STONE SUB-BASE UNTIL BITUMINOUS PAVEMENT CAN BE APPLIED. CALCIUM CHLORIDE SHALL BE USED FOR DUST CONTROL IF NEEDED.
PERMANENT STABILIZATION - DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES PERMANENTLY CEASES SHALL BE STABILIZED WITH PERMANENT SEED NO LATER THAN 3 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY. THE PERMANENT SEED MIX SHALL BE AS SPECIFIED BY THE LANDSCAPE PLAN NOTES OR MAY OTHERWISE CONSIST OF 0.45 LBS/1,000 SF TALL FESCUE, 0.20 LBS/1,000 SF CREEPING RED FESCUE, AND 0.20 LBS/1,000 SF BIRDSFOOT TREFLOIL. PRIOR TO SEEDING, A MINIMUM OF 2 TONS PER ACRE OF AGRICULTURAL LIMESTONE AND 500 LBS PER ACRE OF 10-20-20 FERTILIZER SHALL BE APPLIED. AFTER SEEDING, EACH AREA SHALL BE MULCHED WITH 1.5 TONS PER ACRE OF HAY MULCH. MULCH TO BE ANCHORED IN PLACE WHERE NECESSARY.
6. STRUCTURAL PRACTICES FOR EROSION AND SEDIMENTATION CONTROL
SILT FENCE - WILL BE CONSTRUCTED AROUND THE PERIMETER OF THE DISTURBED AREAS AND WILL DELINEATE THE LIMITS OF WORK FOR THE PROPOSED CONSTRUCTION. THE SILT FENCE WILL BE INSTALLED BY STRETCHING REINFORCED FILTER FABRIC BETWEEN POSTS WITH AT LEAST 8" OF THE FABRIC BURIED BELOW THE GROUND SURFACE TO PREVENT GAPS FROM FORMING NEAR THE GROUND SURFACE. RUNOFF WILL FLOW THROUGH THE OPENINGS IN THE FILTER FABRIC WHILE RETAINING THE SEDIMENT WITHIN THE CONSTRUCTION AREA.
STABILIZED CONSTRUCTION ENTRANCE - WILL BE INSTALLED IN ACCORDANCE WITH THE DETAIL AT THE ENTRANCE TO THE CONSTRUCTION SITE TO HELP REDUCE VEHICLE TRACKING OF SEDIMENTS OFF THE SITE. THE STABILIZED ENTRANCE WILL BE 20'-WIDE AND FLARE AT THE ENTRANCE TO THE PAVED ROAD AND HAVE A DEPTH OF 12" OF STONE. THE STABILIZED ENTRANCE SHALL BE MAINTAINED UNTIL THE REMAINDER OF THE CONSTRUCTION SITE HAS BEEN FULLY STABILIZED. THE PAVED STREET ADJACENT TO THE SITE SHALL BE SWEEPED ON A WEEKLY BASIS TO REMOVE EXCESS MUD AND DIRT FROM BEING TRACKED FROM THE SITE. TRUCKS HAULING MATERIAL TO AND/OR FROM THE SITE SHALL BE COVERED WITH A TARPULAIN.
CATCH BASINS - WILL BE CLEANED ON AN ANNUAL BASIS TO REMOVE ALL SEDIMENTS FROM THE CATCH BASIN SUMPS.
CATCH BASIN PROTECTION - WILL BE INSTALLED AT ALL CATCH BASINS WITHIN THE CONSTRUCTION AREA. FILTER FABRIC WILL BE INSTALLED AROUND THE GRATES OF CATCH BASINS THAT ARE LOCATED IN THE TRAVEL WAY AND STONE/FILTER FABRIC PROTECTION WILL BE INSTALLED AT THE CATCH BASINS FOUND WITHIN THE PARKING AREA AND GRASS.
BLANKET SLOPE PROTECTION - SHALL BE INSTALLED ON ALL 2:1 SLOPES OR STEEPER ON SITE. ANCHOR THE TOP OF THE BLANKET BY ANCHORING THE BLANKET IN A 6" DEEP TRENCH. BACKFILL AND COMPACT TRENCH AFTER STAPLING. ROLL THE BLANKET IN THE DIRECTION OF STORM WATER FLOW. WHERE 2 OR MORE STRIPS OF BLANKET ARE REQUIRED, A MINIMUM OF 4" OF OVERLAP SHALL BE PROVIDED.
7. STORM WATER MANAGEMENT
STORM WATER DRAINAGE FOR DEVELOPED AREAS WILL BE COLLECTED BY A PIPE AND CATCH BASIN CLOSED DRAINAGE SYSTEM. RUNOFF FROM THE ENTIRE SITE DISCHARGES TO THE MUNICIPAL DRAINAGE SYSTEM THAT OUTLETS TO THE MERRIMACK RIVER FROM BRIDGE STREET.
8. ALL CONSTRUCTION DEBRIS AND WASTE MATERIALS SHALL BE COLLECTED AND STORED IN SECURE DUMPSTERS OR APPROVED ENCLOSURE AND REMOVED FROM THE SITE ON A WEEKLY BASIS. NO CONSTRUCTION WASTE SHALL BE BURIED ON SITE. PORTABLE TOILET SANITARY WASTE FACILITIES WILL BE PROVIDED DURING CONSTRUCTION AND MAINTAINED/DISPOSED OF ON A REGULAR BASIS IN ACCORDANCE WITH TOWN AND STATE REGULATIONS.
9. THRUST BLOCK SHALL BE PROVIDED WHERE WATER LINE CHANGES DIRECTION OR TAPS INTO EXISTING WATER LINE.
10. A LIST OF CONSTRUCTION ITEMS AND OTHER PRODUCTS USED ON THIS PROJECT SHALL BE KEPT ON RECORD WITH THIS PLAN ON-SITE. ALL CHEMICALS, PETROLEUM PRODUCTS AND OTHER MATERIALS USED DURING CONSTRUCTION SHALL BE STORED IN A SECURE AREA, AND PRECAUTIONS USED TO PREVENT POTENTIAL SOURCES OF CONTAMINATION OR POLLUTION. ANY SPILL OF THESE TYPES OF SUBSTANCES SHALL BE CLEANED UP AND DISPOSED OF IN A LEGAL MANNER AS SPECIFIED BY STATE REGULATIONS AND THE MANUFACTURER. ANY SPILL IN AMOUNTS EQUAL TO OR EXCEEDING REPORTABLE QUANTITY AS DEFINED BY THE EPA SHALL TAKE THE FOLLOWING STEPS:
- NOTIFY THE NATIONAL RESPONSE CENTER IMMEDIATELY AT (888) 424-8802; IN WASHINGTON, D.C., CALL (202) 426-2675.
- WITHIN 14 DAYS, SUBMIT A WRITTEN DESCRIPTION OF THE RELEASE TO THE EPA REGIONAL OFFICE PROVIDING THE DATE AND CIRCUMSTANCES OF THE RELEASE AND THE STEPS TO BE TAKEN TO PREVENT ANOTHER RELEASE.
- MODIFY THE POLLUTION PREVENTION PLAN TO INCLUDE THE INFORMATION LISTED ABOVE.

- GOOD HOUSEKEEPING:**
THE FOLLOWING GOOD HOUSEKEEPING PRACTICES WILL BE FOLLOWED ONSITE DURING THE CONSTRUCTION PROJECT.
- AN EFFORT WILL BE MADE TO STORE ONLY ENOUGH PRODUCT REQUIRED TO DO THE JOB.
- ALL MATERIALS STORED ONSITE WILL BE STORED IN A NEAT, ORDERLY MANNER IN THEIR APPROPRIATE CONTAINERS AND, IF POSSIBLE, UNDER A ROOF OR OTHER ENCLOSURE;
- PRODUCTS WILL BE KEPT IN THEIR ORIGINAL CONTAINERS WITH THE ORIGINAL MANUFACTURER'S LABELS;
- SUBSTANCES WILL NOT BE MIXED WITH ONE ANOTHER UNLESS RECOMMENDED BY THE MANUFACTURER;
- WHENEVER POSSIBLE, ALL OF A PRODUCT WILL BE USED UP BEFORE DISPOSING OF THE CONTAINER;
- MANUFACTURER'S RECOMMENDATIONS FOR PROPER USE AND DISPOSAL WILL BE FOLLOWED;
- TRASH DUMPSTERS SHALL BE GASKETED OR HAVE A SECURE WATERTIGHT LID AND BE PLACED AWAY FROM STORMWATER CONVEYANCES AND DRAINS.
- THE SITE SUPERINTENDENT WILL INSPECT DAILY TO ENSURE PROPER USE AND DISPOSAL OF MATERIALS ONSITE.
- HAZARDOUS PRODUCTS:**
THESE PRACTICES ARE USED TO REDUCE THE RISKS ASSOCIATED WITH HAZARDOUS MATERIALS:
- PRODUCTS WILL BE KEPT IN ORIGINAL CONTAINERS UNLESS THEY ARE NOT RESEALABLE;
- ORIGINAL LABELS AND MATERIAL SAFETY DATA WILL BE RETAINED; THEY CONTAIN IMPORTANT PRODUCT INFORMATION;
- IF SURPLUS PRODUCT MUST BE DISPOSED OF, MANUFACTURER'S OR LOCAL AND STATE RECOMMENDED METHODS FOR PROPER DISPOSAL WILL BE FOLLOWED.
- PRODUCT SPECIFIC PRACTICES:**
THE FOLLOWING PRODUCT SPECIFIC PRACTICES WILL BE FOLLOWED ON SITE:
- PETROLEUM PRODUCTS:**
ALL ONSITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTATIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE. PETROLEUM PRODUCTS WILL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED. ANY ASPHALT SUBSTANCES USED ONSITE WILL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
- FERTILIZERS:**
FERTILIZERS USED WILL BE APPLIED ONLY IN THE MINIMUM AMOUNTS RECOMMENDED BY THE MANUFACTURER. ONCE APPLIED, FERTILIZER WILL BE WORKED INTO THE SOIL TO LIMIT EXPOSURE TO STORM WATER. STORAGE WILL BE IN A COVERED SHED. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER WILL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.
- PAINTS:**
ALL CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL NOT BE DISCHARGED TO THE STORM SEWER BUT WILL BE PROPERLY DISPOSED OF ACCORDING TO MANUFACTURER'S INSTRUCTIONS OR STATE AND LOCAL REGULATIONS.
- CONCRETE TRUCKS:**
EXCESS CONCRETE SHALL BE USED IN AREAS DESIGNATED BY THE SITE CONTRACTOR. WASH WATER SHALL BE DISPOSED OF USING BEST MANAGEMENT PRACTICES. BUILDING CONTRACTOR IS RESPONSIBLE FOR REMOVAL OF ALL DRUM WASH WATER ASSOCIATED WITH CONCRETE FOR THE BUILDING PAD. SITE CONTRACTOR TO COORDINATE AND PROVIDE BUILDING CONTRACTOR WITH AN AREA FOR DRUM WASH WATER.
- SPILL CONTROL PRACTICES:**
IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING PRACTICES WILL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:
- MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEANUP WILL BE CLEARLY POSTED AND SITE PERSONNEL WILL BE MADE AWARE OF THE PROCEDURES AND THE LOCATION OF THE INFORMATION AND CLEANUP SUPPLIES.
- MATERIALS AND EQUIPMENT NECESSARY FOR SPILL CLEANUP WILL BE KEPT IN THE MATERIAL STORAGE AREA ONSITE. EQUIPMENT AND MATERIALS WILL INCLUDE BUT NOT BE LIMITED TO BROOMS, DUST PANS, MOPS, RAGS, GLOVES, GOGGLES, KITTY LITTER, SAND, SAWDUST, AND PLASTIC AND METAL TRASH CONTAINERS SPECIFICALLY FOR THIS PURPOSE.
- ALL SPILLS WILL BE CLEANED UP IMMEDIATELY AFTER DISCOVERY.
- THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- SPILLS OF TOXIC OR HAZARDOUS MATERIAL WILL BE REPORTED TO THE APPROPRIATE STATE OR LOCAL GOVERNMENT AGENCY, REGARDLESS OF SIZE.
- THE SPILL PREVENTION PLAN WILL BE ADJUSTED TO INCLUDE MEASURES TO PREVENT THIS TYPE OF SPILL FROM REOCCURRING AND HOW TO CLEAN UP THE SPILL IF THERE IS ANOTHER ONE. A DESCRIPTION OF THE SPILL, WHAT CAUSED IT, AND THE CLEANUP MEASURES WILL ALSO BE INCLUDED.
- THE SITE SUPERINTENDENT RESPONSIBLE FOR THE DAY-TO-DAY SITE OPERATIONS, WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THEY WILL DESIGNATE AT LEAST THREE OTHER SITE PERSONNEL WHO WILL RECEIVE SPILL PREVENTION AND CLEANUP TRAINING. THESE INDIVIDUALS WILL EACH BECOME RESPONSIBLE FOR A PARTICULAR PHASE OF PREVENTION AND CLEANUP. THE NAMES OF RESPONSIBLE SPILL PERSONNEL WILL BE POSTED IN THE MATERIAL STORAGE AREA AND IN THE OFFICE TRAILER ONSITE.

11. THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN RECORDS OF CONSTRUCTION ACTIVITIES, INCLUDING DATES OF MAJOR GRADING ACTIVITIES, DATES WHEN CONSTRUCTION ACTIVITIES HAVE TEMPORARILY CEASED ON A PORTION OF THE SITE, DATES WHEN WORK IS COMPLETED ON A PORTION OF THE SITE, AND DATES WHEN STABILIZATION MEASURES ARE INITIATED ONSITE.
12. THE CONTRACTOR SHALL PERFORM INSPECTIONS OR HAVE A CONSULTING ENGINEER PERFORM INSPECTIONS EVERY SEVEN (7) DAYS AND WITHIN 24 HOURS AFTER A STORM OF 0.5" OR GREATER. INSPECTION REPORTS ARE TO BE KEPT ON FILE AT THE SITE WITH THIS PLAN. MAINTENANCE OR MODIFICATION SHALL BE IMPLEMENTED AND ADDED TO THE PLAN AS RECOMMENDED BY THE QUALIFIED INSPECTOR.



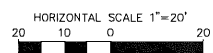
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SCALE: 1" = 20' MARCH 22, 2021

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CONSTRUCTION SEQUENCE NOTES

- INSTALL STABILIZED CONSTRUCTION ENTRANCE.
- CUT AND CLEAR TREES WITHIN AREA OF DISTURBANCE UNLESS OTHERWISE NOTED.
- CONSTRUCT TEMPORARY AND PERMANENT EROSION CONTROL FACILITIES PRIOR TO ANY EARTH MOVING OPERATION.
- ROUGH GRADE SITE. ALL SLOPES SHALL BE STABILIZED IMMEDIATELY AFTER GRADING. ALL DISTURBED AREAS SHALL BE STABILIZED NO LATER THAN 72 HOURS AFTER CONSTRUCTION ACTIVITY CEASES. IF EARTHWORK TEMPORARILY CEASES ON A PORTION OF OR THE ENTIRE SITE, AND WILL NOT RESUME WITHIN 21 DAYS, THE AREA SHALL BE STABILIZED.
AN AREA SHALL BE CONSIDERED STABILIZED IF:
A) BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
B) A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
C) A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH STONE OR RIPRAP HAS BEEN INSTALLED, OR
D) EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.
- CONSTRUCT CULVERTS, DETENTION BASINS AND TREATMENT SWALES. PLACE HEADWALLS, RIP-RAP AND OTHER DRAINAGE FACILITIES ACCORDING TO PLAN. THE CONTRACTOR SHALL STABILIZE ALL DITCHES, SWALES, AND PONDS/BASINS PRIOR TO DIRECTING FLOW TO THEM.
- INSTALL ALL UNDERGROUND UTILITIES.
- CONSTRUCT BUILDINGS.
- CONSTRUCT PARKING AND FINISH GRADE SITE ACCORDING TO PLAN. ALL SLOPES SHALL BE STABILIZED IMMEDIATELY AFTER GRADING.
- INSPECT AND MAINTAIN ALL EROSION AND SEDIMENTATION CONTROL MEASURES PERIODICALLY AND IMMEDIATELY AFTER STORM EVENTS.
- COMPLETE PERMANENT SEEDING AND LANDSCAPING.
- REMOVE TEMPORARY EROSION CONTROL MEASURES ONCE ALL AREAS ARE STABILIZED WITH A SUITABLE STAND OF GRASS, PAVEMENT OR COMPACTED GRAVELS.
* REFER TO THE STORM WATER MANAGEMENT PLAN FOR EROSION CONTROL MEASURES AND SPECIFIC INFORMATION.

GENERAL NOTES

- ALL IN PAVEMENT MANHOLES SHALL HAVE RIMS SET TO FINISH GRADE REGARDLESS OF ANY ELEVATIONS OTHERWISE SHOWN.
- WHERE DEPTH OF COVER IS LESS THAN 3 FEET CLASS V REINFORCED CONCRETE PIPE SHALL BE USED.
- THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES OWNING UTILITIES, EITHER OVERHEAD OR UNDERGROUND, WITHIN THE CONSTRUCTION AREA AND SHALL COORDINATE AS NECESSARY WITH THE UTILITY COMPANIES OF SAID UTILITIES. THE PROTECTION OR RELOCATION OF UTILITIES IS ULTIMATELY THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL MAINTAIN EMERGENCY ACCESS TO ALL AREAS AFFECTED BY HIS WORK AT ALL TIMES.
- ALL EXCAVATIONS SHALL BE THOROUGHLY SECURED ON A DAILY BASIS BY THE CONTRACTOR AT THE COMPLETION OF CONSTRUCTION OPERATIONS IN THE IMMEDIATE AREA.
- EROSION CONTROL SYSTEMS SHALL BE INSTALLED AND MAINTAINED FOR THE DURATION OF THE PROJECT IN ACCORDANCE WITH APPLICABLE NHDES STANDARDS. THESE DETAILS SERVE AS A GUIDE ONLY.
- REFER TO THE CITY'S STANDARD DETAILS, LATEST REVISION, FOR ADDITIONAL INFORMATION AND CRITERIA.
- THE CONTRACTOR SHALL STABILIZE ALL DITCHES, SWALES, AND PONDS PRIOR TO DIRECTING FLOW TO THEM.
- THE SMALLEST PRACTICAL AREA SHALL BE DISTURBED DURING CONSTRUCTION, BUT IN NO CASE SHALL EXCEED 5 ACRES AT ANY ONE TIME BEFORE DISTURBED AREAS ARE STABILIZED.

WINTER CONSTRUCTION

IN ADDITION TO THE OTHER NOTES CONTAINED ON THIS PLAN, THE FOLLOWING MUST BE IMPLEMENTED:

- WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED AS SUCH THAT NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME.
- AN AREA WITHIN 100 FEET OF A PROTECTED NATURAL RESOURCE MUST BE PROTECTED WITH A DOUBLE ROW OF SEDIMENT BARRIERS.
- TEMPORARY MULCH MUST BE APPLIED WITHIN 7 DAYS OF SOIL EXPOSURE OR PRIOR TO ANY STORM EVENT, BUT AFTER EVERY WORKDAY IN AREAS WITHIN 100 FEET FROM A PROTECTED NATURAL RESOURCE.
- AREAS THAT HAVE BEEN BROUGHT TO FINAL GRADE MUST BE PERMANENTLY MULCHED THE SAME DAY.
- IN THE EVENT OF A SNOWFALL GREATER THAN 1 INCH (FRESH OR CUMULATIVE), THE SNOW SHALL BE REMOVED FROM THE AREAS DUE TO BE SEEDED AND MULCHED.
- LOAM SHALL BE FREE OF FROZEN CLUMPS BEFORE IT IS APPLIED.
- A DITCH THAT WILL BE CONSTRUCTED DURING THE WINTER MUST BE STABILIZED WITH RIPRAP.

OVERWINTER STABILIZATION

- PERMANENT STABILIZATION CONSISTS OF AT LEAST 85% VEGETATION, PAVEMENT/GRAVEL BASE OR RIPRAP.
- ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE, THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.
- ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
- AFTER OCTOBER 15, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDT ITEM 304.3.
- DO NOT EXPOSE SLOPES OR LEAVE SLOPES EXPOSED OVER THE WINTER OR FOR ANY OTHER EXTENDED TIME OF WORK SUSPENSION UNLESS FULLY PROTECTED WITH MULCH.
- APPLY HAY MULCH AT TWICE THE STANDARD RATE (150 LBS. PER 1,000 SF). THE MULCH MUST BE THICK ENOUGH SUCH THAT THE GROUND SURFACE WILL NOT BE VISIBLE AND MUST BE ANCHORED.
- USE MULCH AND MULCH NETTING OR AN EROSION CONTROL MULCH BLANKET OR MIX FOR ALL SLOPES GREATER THAN 8% OR OTHER AREAS EXPOSED TO DIRECT WIND.
- INSTALL AN EROSION CONTROL BLANKET IN ALL DRAINAGE WAYS (BOTTOM AND SIDES) WITH A SLOPE GREATER THAN 3%.
- SEE THE VEGETATION MEASURES FOR MORE INFORMATION ON SEEDING DATES AND TYPES.

EROSION CONTROL NOTES

DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED:

- INSTALLATION OF SILTATION FENCES AND OTHER EROSION CONTROL MEASURES SHALL BE COMPLETED PRIOR TO THE START OF SITE WORK IN ANY GIVEN AREA. PREFABRICATED SILTATION FENCES SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
- SILTATION FENCES AND OTHER EROSION CONTROL MEASURES SHALL BE KEPT CLEAN DURING CONSTRUCTION AND REMOVED WHEN ALL SLOPES HAVE A VEGETATIVE COVER OF GREATER THAN 85%. EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EVERY RAINFALL.
- EXISTING VEGETATION IS TO REMAIN UNDISTURBED WHEREVER POSSIBLE.
- THE AREA OF LAND EXPOSED AND THE TIME OF EXPOSURE SHALL BE MINIMIZED. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 72 HOURS AFTER FINAL GRADING.
- ALL DISTURBED AREAS SHALL HAVE A MINIMUM OF 4" OF LOAM. ACCEPTABLE SEED MIXES ARE AS FOLLOWS:

TYPICAL LAWN MIX (MIN. 200 LBS/ACRE):
 33% CREEPING RED FESCUE (MIN. 66 LBS/ACRE)
 42% PERENNIAL RYEGRASS (MIN. 84 LBS/ACRE)
 21% KENTUCKY BLUEGRASS (MIN. 42 LBS/ACRE)
 4% REDTOP (MIN. 8 LBS/ACRE)

TEMPORARY LAWN MIX (MIN. 47 LBS/ACRE):
 100% ANNUAL RYE

WILDFLOWER SLOPE (NHDT TYPE 44) MIX 3:1 OR GREATER SLOPES (MIN. 160 LBS/ACRE):

38% CREEPING RED FESCUE (MIN. 76 LBS/ACRE)
 32% PERENNIAL RYEGRASS (MIN. 64 LBS/ACRE)
 5% REDTOP (MIN. 10 LBS/ACRE)
 5% ALSIKE CLOVER (MIN. 10 LBS/ACRE)
 5% BIRDSFOOT TREFOIL (MIN. 10 LBS/ACRE)
 3% LANCE-LEAF COREOPSIS (MIN. 3 LBS/ACRE)
 3% OXEYE DAISY (MIN. 3 LBS/ACRE)
 3% BUTTERFLY WEED (MIN. 3 LBS/ACRE)
 3% BLACKEYED SUSAN (MIN. 3 LBS/ACRE)
 3% WILD LUPINE (MIN. 3 LBS/ACRE)

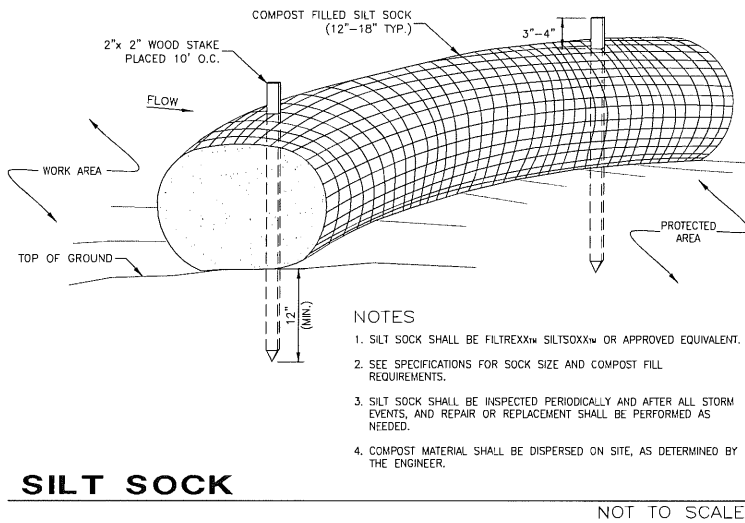
GENERAL SLOPE (NHDT TYPE 44) MIX 3:1 OR GREATER SLOPES (MIN. 160 LBS/ACRE):

44% CREEPING RED FESCUE (MIN. 88 LBS/ACRE)
 38% PERENNIAL RYEGRASS (MIN. 76 LBS/ACRE)
 6% REDTOP (MIN. 12 LBS/ACRE)
 6% ALSIKE CLOVER (MIN. 12 LBS/ACRE)
 6% BIRDSFOOT TREFOIL (MIN. 12 LBS/ACRE)

- PLACING LOAM ON SITE
 a. ALL SUBGRADE ELEVATIONS SHOULD BE UNIFORMLY GRADED TO RECEIVE LOAM AND SHALL BE INSPECTED AND APPROVED BY THE GENERAL CONTRACTOR PRIOR TO PLACEMENT OF LOAM.
 b. PLACE LOAM TO FORM A MINIMUM DEPTH OF 4" WHEN FILLED, UNLESS OTHERWISE INDICATED. ALL DEPRESSIONS EXPOSED DURING THE ROLLING SHALL BE ROLLED WITH ADDITIONAL LOAM.
- SEED BED PREPARATION
 AFTER FINISH GRADING AND JUST BEFORE SEEDING, THE AREAS TO BE SEEDDED SHALL BE LOOSENEED TO PROVIDE A ROUGH, FIRM BUT FINELY PULVERIZED SEEDBED. THE INTENT IS A TEXTURE CAPABLE OF RETAINING WATER, SEED AND FERTILIZER WHILE REMAINING STABLE AND ALLOWING SEED TIME TO GERMINATE. SEED SHALL BE APPLIED TO THE CONDITIONED SEEDBED NOT MORE THAN 48 HOURS AFTER THE SEEDBED HAS BEEN PREPARED.
- LIME AND FERTILIZER SHALL BE INCORPORATED INTO THE SOIL PRIOR TO OR AT THE TIME OF AT THE TIME OF SEEDING. A MINIMUM OF 2 TONS PER ACRE OF AGRICULTURAL LIMESTONE AND 500 LBS. PER ACRE OF 10-20-20 FERTILIZER SHALL BE APPLIED. SEEDING PRACTICES SHALL COMPLY WITH LOCAL USDA SOIL CONSERVATION SERVICES RECOMMENDATIONS.
- HAY MULCH OR JUTE MATTING SHALL BE USED WHERE INDICATED ON THE PLANS. A MINIMUM OF 1.5 TONS OF MULCH PER ACRE SHALL BE APPLIED. MULCH SHALL BE ANCHORED IN PLACE WHERE NECESSARY. JUTE MATTING SHALL BE LAID IN THE DIRECTION OF RUNOFF FLOW AND APPLIED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.
- PERMANENT OR TEMPORARY COVER MUST BE IN PLACE BEFORE THE GROWING SEASON ENDS. WHEN SEEDDED AREAS ARE MULCHED, PLANTINGS MAY BE MADE FROM EARLY SPRING TO EARLY OCTOBER. WHEN SEEDDED AREAS ARE NOT MULCHED, PLANTINGS SHOULD BE MADE FROM EARLY SPRING TO MAY 20 OR FROM AUGUST 15 TO SEPTEMBER 15. NO DISTURBED AREA SHALL BE LEFT EXPOSED DURING WINTER MONTHS.
- WATER SHALL BE USED FOR DUST CONTROL IN APPROPRIATE AREAS.

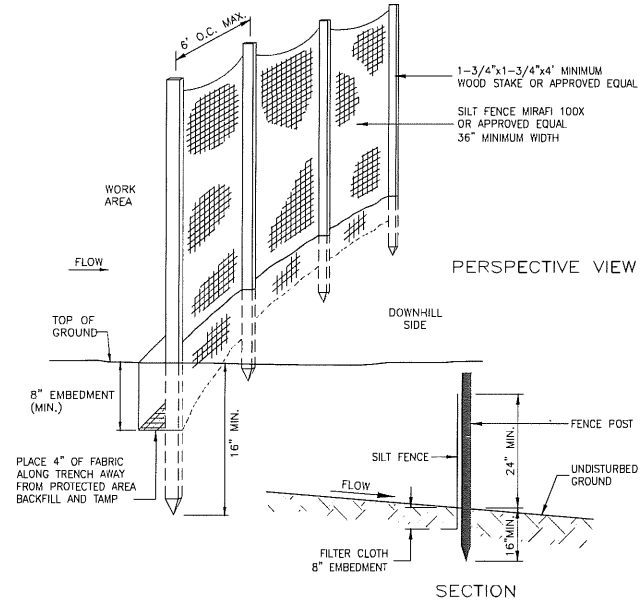
STOCKPILE NOTES

- LOCATE STOCKPILES A MINIMUM OF 50 FEET AWAY FROM CONCENTRATED FLOWS OF STORMWATER, DRAINAGE COURSES AND INLETS.
- PROTECT ALL STOCKPILES FROM STORMWATER RUN-ON USING TEMPORARY PERIMETER MEASURES SUCH AS DIVERSIONS, BERMS, SANDBAGS OR OTHER APPROVED PRACTICES.
- STOCKPILES SHOULD BE SURROUNDED BY SEDIMENT BARRIERS, SUCH AS SILT FENCE OR SILT SOCK, TO PREVENT MIGRATION OF MATERIAL BEYOND THE IMMEDIATE CONFINES OF THE STOCKPILES.
- IMPLEMENT WIND EROSION CONTROL PRACTICES AS APPROPRIATE ON ALL STOCKPILED MATERIAL.
- PLACE BAGGED MATERIALS ON PALLETS AND UNDER COVER.
- INACTIVE STOCKPILES
 a. INACTIVE SOIL STOCKPILES SHOULD BE COVERED WITH ANCHORED TARPS OR PROTECTED WITH SOIL STABILIZATION MEASURES (TEMPORARY SEED AND MULCH OR OTHER TEMPORARY PRACTICE) AND TEMPORARY PERIMETER SEDIMENT BARRIERS AT ALL TIMES.
 b. INACTIVE STOCKPILES OF CONCRETE RUBBLE, ASPHALT CONCRETE RUBBLE, AGGREGATE MATERIALS AND OTHER SIMILAR MATERIALS SHOULD BE PROTECTED WITH TEMPORARY SEDIMENT PERIMETER BARRIERS AT ALL TIMES. IF THE MATERIALS ARE A SOURCE OF DUST, THEY SHOULD ALSO BE COVERED.
- ACTIVE STOCKPILES
 a. ALL STOCKPILES SHOULD BE SURROUNDED WITH TEMPORARY LINEAR SEDIMENT BARRIERS PRIOR TO THE ONSET OF PRECIPITATION. PERIMETER BARRIERS SHOULD BE MAINTAINED AT ALL TIMES, AND ADJUSTED AS NEEDED TO ACCOMMODATE THE DELIVERY AND REMOVAL OF MATERIALS FROM THE STOCKPILE. THE INTEGRITY OF THE BARRIER SHOULD BE INSPECTED AT THE END OF EACH WORKING DAY.
 b. WHEN A STORM EVENT IS PREDICTED, STOCKPILES SHOULD BE PROTECTED WITH AN ANCHORED PROTECTIVE COVERING.



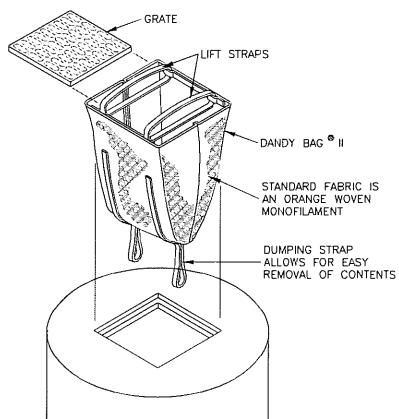
SILT SOCK

NOT TO SCALE



SILT FENCE

NOT TO SCALE



INSTALLATION

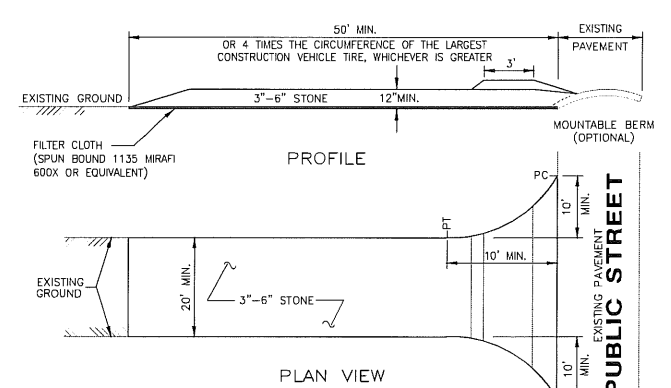
INSTALLATION: REMOVE THE GRATE FROM CATCH BASIN. IF USING OPTIONAL OIL ABSORBENTS; PLACE ABSORBENT PILLOW IN UNIT. STAND THE GRATE ON END. MOVE THE TOP LIFTING STRAPS OUT OF THE WAY AND PLACE THE GRATE INTO THE DANDY BAG. BE SURE THE GRATE IS BELOW THE TOP STRAPS AND ABOVE THE LOWER STRAPS. HOLDING THE LIFTING DEVICES, INSERT THE GRATE INTO THE INLET.

MAINTENANCE

MAINTENANCE: REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM VICINITY OF UNIT AFTER EACH STORM EVENT. AFTER EACH STORM EVENT AND AT REGULAR INTERVALS, LOOK INTO THE DANDY BAG II. IF THE CONTAINMENT AREA IS MORE THAN 1/3 FULL OF SEDIMENT, THE UNIT MUST BE EMPTIED. TO EMPTY UNIT, LIFT THE UNIT OUT OF THE INLET USING THE LIFTING STRAPS AND REMOVE THE GRATE. IF USING OPTIONAL OIL ABSORBENTS; REPLACE ABSORBENT WHEN NEAR SATURATION.

DANDY BAG® II

NOT TO SCALE



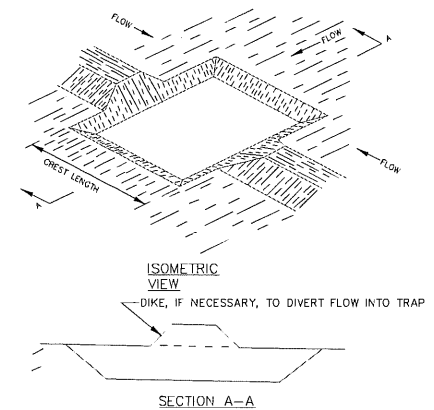
NOTES

- FILTER CLOTH - WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE SURFACE.
- WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- WASHING - WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN STORM EVENT.

USDA - SCS STABILIZED CONSTRUCTION ENTRANCE

SEE PLAN FOR PROPOSED LOCATION

NOT TO SCALE



- SPECIFICATIONS - TEMPORARY SEDIMENT TRAPS SHOULD MEET THE FOLLOWING REQUIREMENTS:
- SEDIMENT TRAPS SHOULD BE LOCATED SO THAT THEY CAN BE INSTALLED PRIOR TO DISTURBING THE AREA THEY ARE TO PROTECT.
 - THE TRAP SHOULD BE INSTALLED AS CLOSE TO THE DISTURBED AREA OR SOURCE OF SEDIMENT AS POSSIBLE.
 - THE MAXIMUM CONTRIBUTING DRAINAGE AREA TO THE TRAP SHOULD BE LESS THAN 5 ACRES.
 - THE MINIMUM VOLUME OF THE TRAP SHOULD BE 3,600 CUBIC FEET OF STORAGE FOR EACH ACRE OF DRAINAGE AREA.
 - THE SIDE SLOPES OF THE TRAP SHOULD BE 3:1 OR FLATTER, AND SHOULD BE STABILIZED IMMEDIATELY AFTER THE CONSTRUCTION.

EXCAVATED EARTH OUTLET SEDIMENT TRAP DETAIL

NOT TO SCALE

TAX MAP 26 LOT 169
DETAIL SHEET
MILFORD INDEPENDENT SENIOR HOUSING
 54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
 SCALE: AS SHOWN MARCH 22, 2021

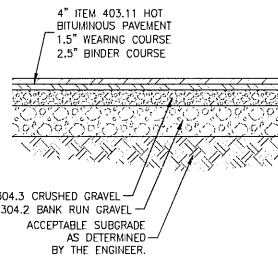


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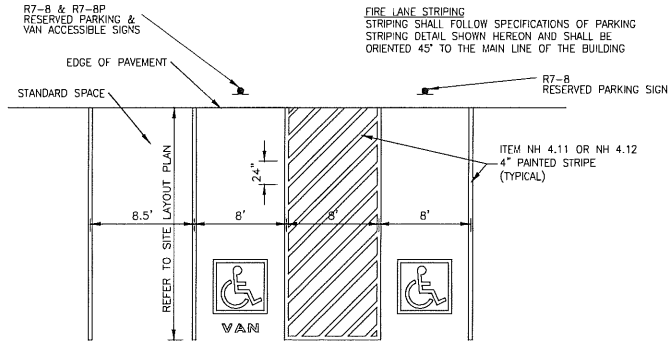




NOTES:
 REMOVE ALL LOAM, CLAY, MUCK, STUMPS, AND OTHER IMPROPER PAVEMENT FOUNDATION MATERIAL WITHIN 2' OF SUBGRADE. REPLACE WITH COMPACTED GRANULAR FILL MATERIAL ACCEPTABLE TO APPROVING AGENCY. COMPACTION TO BE AT LEAST 95% OF STANDARD PROCTOR.
 ALL PAVEMENT, BASE MATERIALS AND WORKMANSHIP TO BE IN COMPLIANCE WITH N.H.D.O.T. "STANDARDS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION.
 ALL PAVEMENT TO MEET AASHTO H-20 LOADING.

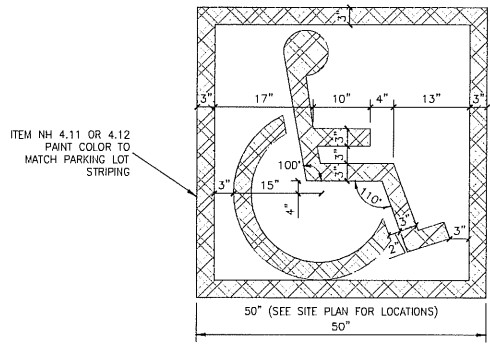
TYPICAL PAVEMENT SECTION

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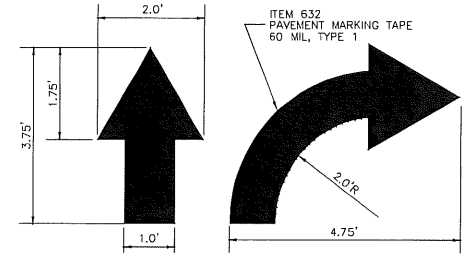
PARKING STRIPING DETAIL

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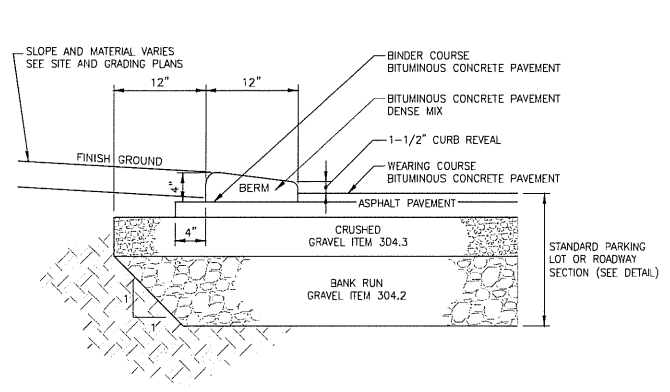
PAINTED HANDICAP SYMBOL

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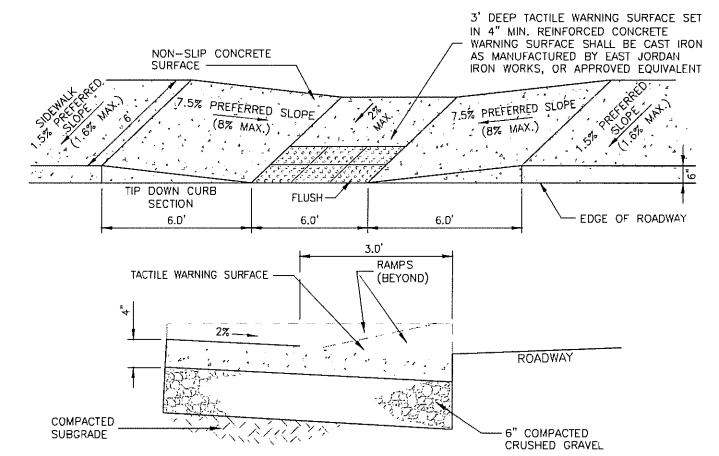
TRAFFIC FLOW ARROW

NOT TO SCALE



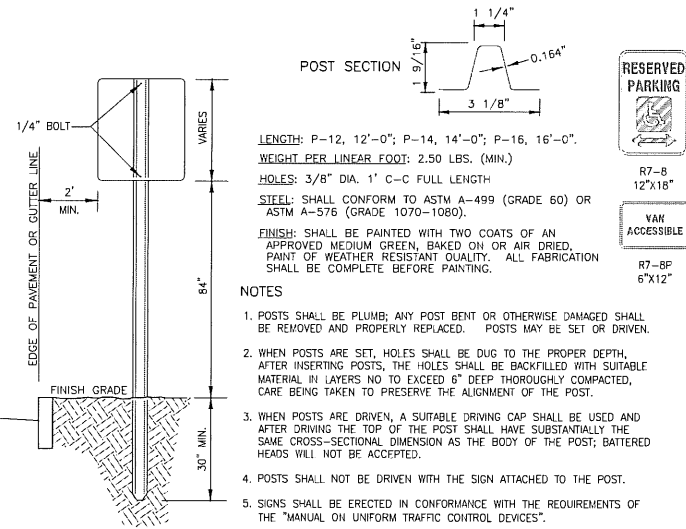
CAPE COD BERM

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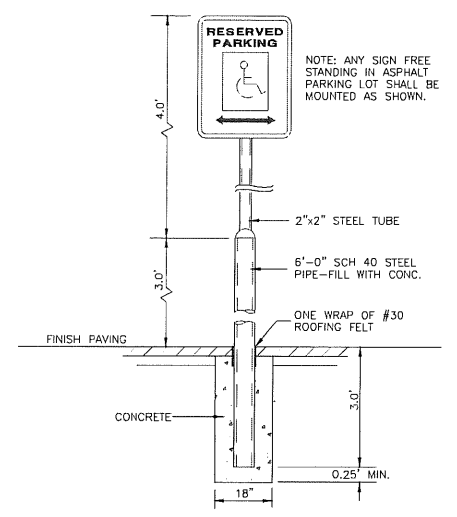
ACCESSIBLE RAMP RECESSED IN WALK

WITH DETECTABLE WARNING SURFACE (CAST IRON) NOT TO SCALE



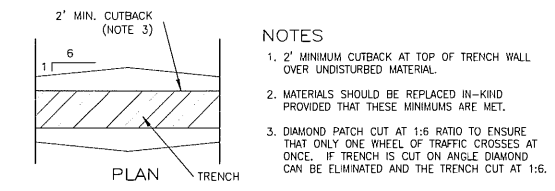
TRAFFIC SIGN POST IN GRADE

NOT TO SCALE

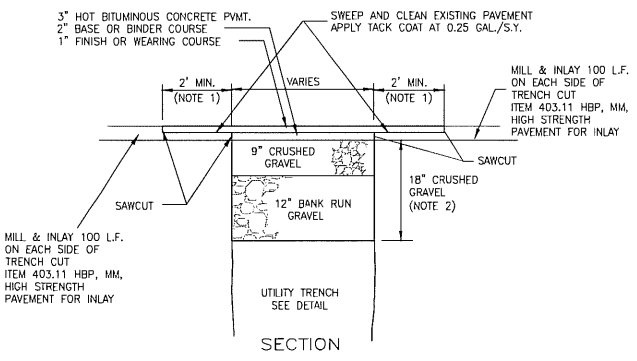


SIGN POLE IN PAVEMENT

NOT TO SCALE

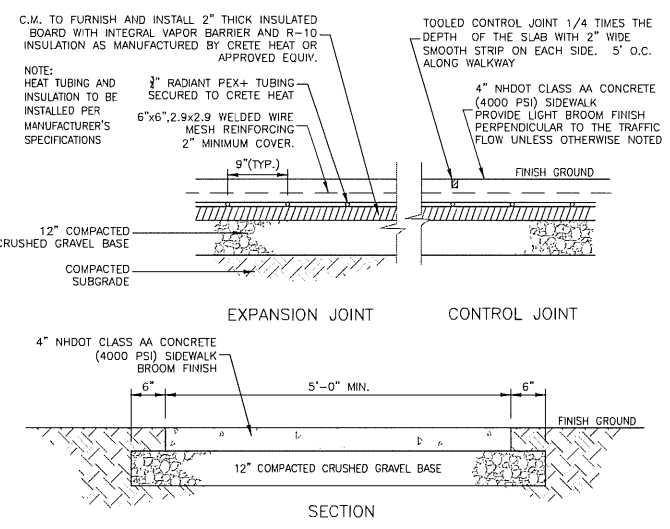


NOTES:
 1. 2" MINIMUM CUTBACK AT TOP OF TRENCH WALL OVER UNDISTURBED MATERIAL.
 2. MATERIALS SHOULD BE REPLACED IN-KIND PROVIDED THAT THESE MINIMUMS ARE MET.
 3. DIAMOND PATCH CUT AT 1:6 RATIO TO ENSURE THAT ONLY ONE WHEEL OF TRAFFIC CROSSES AT ONCE. IF TRENCH IS CUT ON ANGLE DIAMOND CAN BE ELIMINATED AND THE TRENCH CUT AT 1:6.



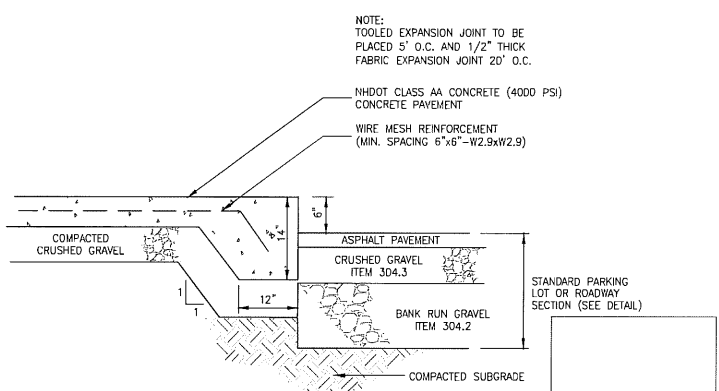
PAVEMENT TRENCH PATCH

FLUSH NOT TO SCALE



CONCRETE SIDEWALK

NOT TO SCALE



CONCRETE CURB AT SIDEWALK

NOT TO SCALE

TAX MAP 26 LOT 169
DETAIL SHEET
MILFORD INDEPENDENT SENIOR HOUSING
 54 SCHOOL STREET, MILFORD, NH
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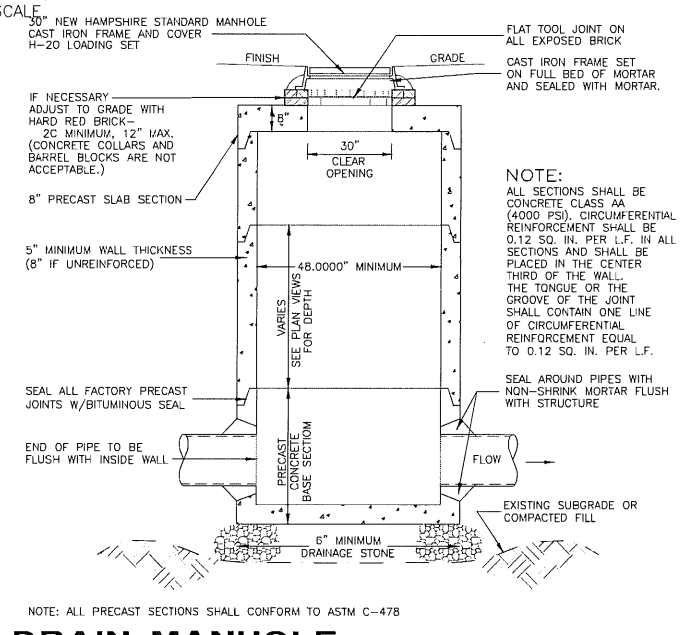
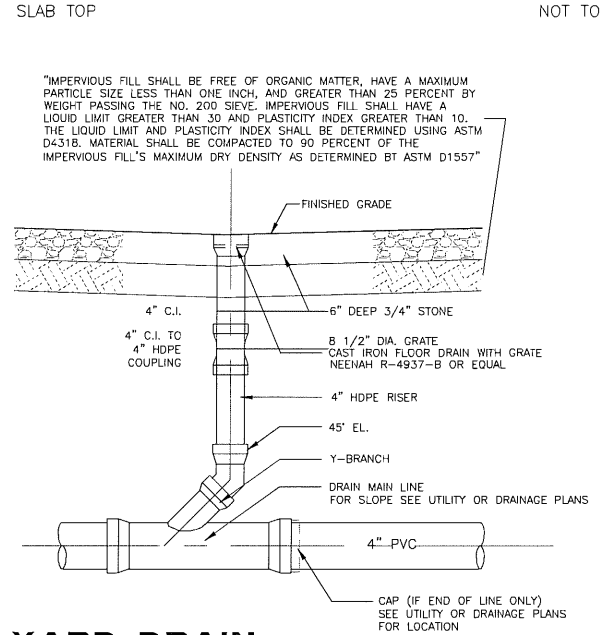
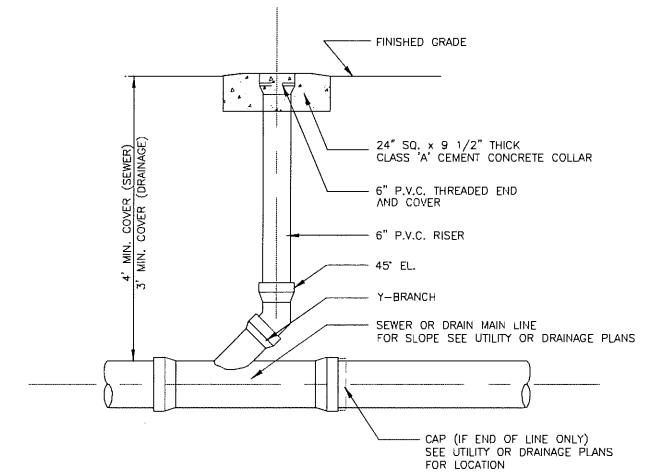
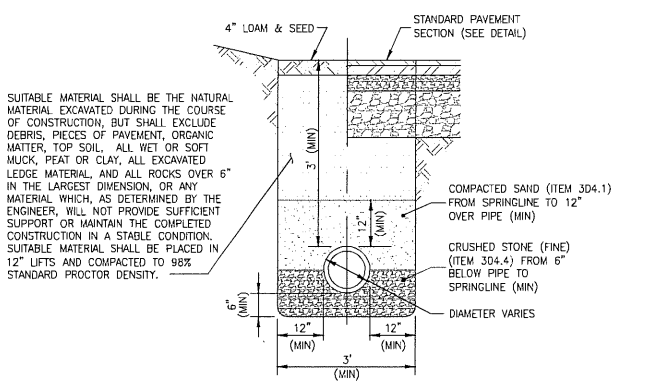
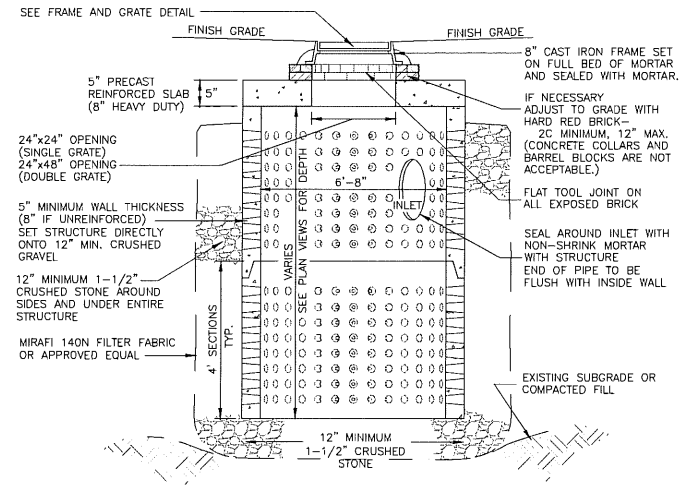
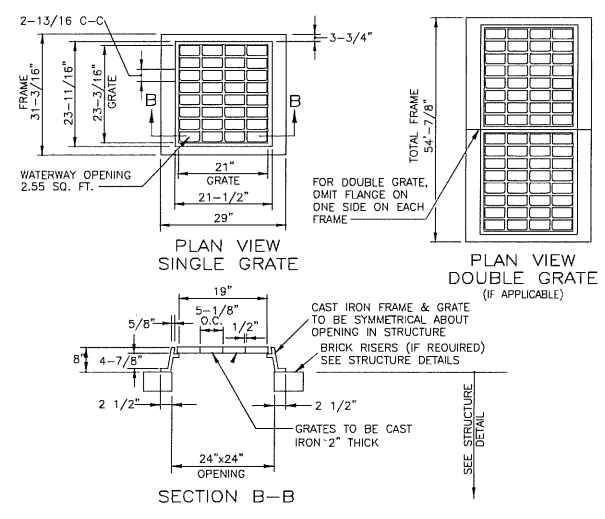
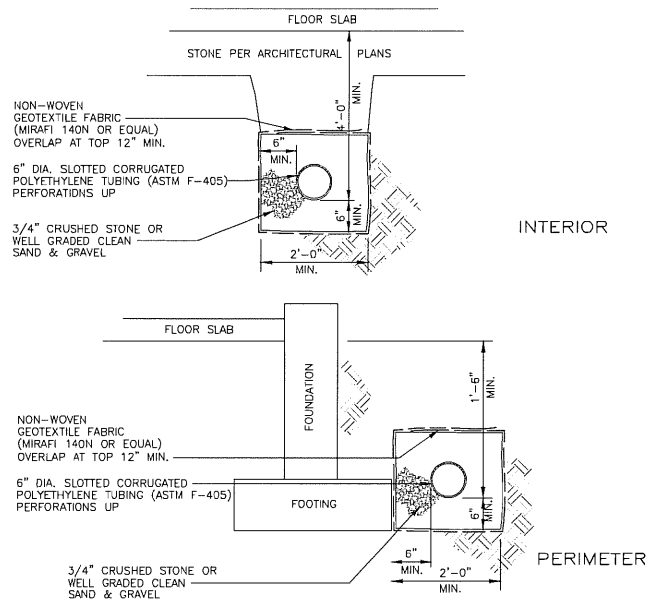
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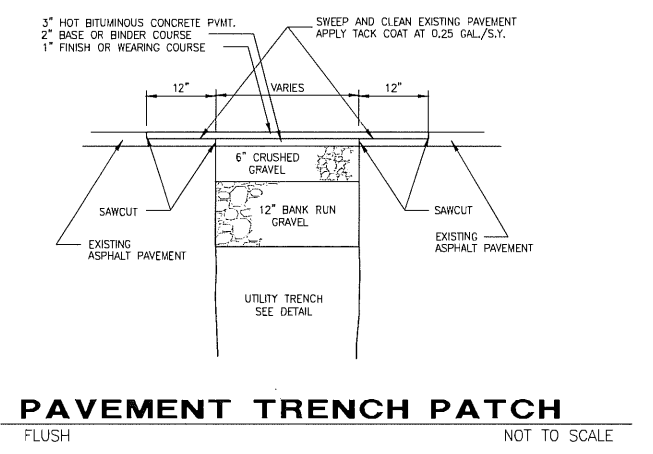
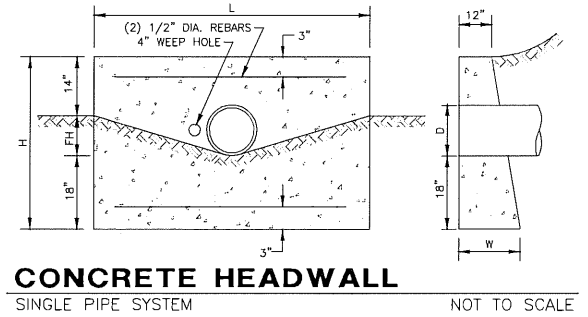
REV	DATE	DESCRIPTION	DR	CK

76451.21 OR SRP FB
 CK JK CADFILE 76451-21 COVER-DETAILS SHEET 11 OF 15

Mar 22, 2021 - 8:35am
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 Milford Elderly\76451-21 Housing Initi - Site Plan & Permitting - Milford, NH\76451-21_CAD\Design\Production Drawings\76451-21_Cover-Details.dwg



NOMINAL PIPE SIZE D	LENGTH OF BARS	L	H	FH	W
12	3'-2"	3'-6"	3'-6"	0'-10"	1'-10.5"
15	4'-2"	4'-6"	3'-9"	1'-1"	1'-11.25"
18	5'-2"	5'-6"	4'-0"	1'-4"	2'-0"
24	7'-2"	7'-6"	4'-6"	1'-10"	2'-1.5"
30	9'-2"	9'-6"	5'-0"	2'-4"	2'-3"
36	11'-2"	11'-6"	5'-6"	2'-10"	2'-4.5"
42	13'-2"	13'-6"	6'-0"	3'-4"	2'-6"
48	15'-2"	15'-6"	6'-6"	3'-10"	2'-7.5"



TAX MAP 26 LOT 169
DETAIL SHEET
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
SCALE: AS SHOWN
MARCH 22, 2021

REV	DATE	DESCRIPTION	DR	CK

TFM
Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists
48 Constitution Drive
Bedford, NH 03110
Phone (603) 472-4488
Fax (603) 472-9747
www.tfmcor.com

76451.21 DR SRP FB
CK JK CADFILE 76451-21 COVER-DETAILS SHEET 12 OF 15

Mar 22, 2021 - 12:00pm
 F:\TFM\Projects\76451.21 Milford Elderly\76451-21 Housing Inits - Site Plan & Permitting - Milford, NH\76451-21_CSD\Design\Production Drawings\76451-21_Cover-Details.dwg

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GENERAL NOTES

- MINIMUM SIZE PIPE FOR SEWER SERVICE SHALL BE FOUR INCHES.
- PIPE AND JOINT MATERIALS:
 - PLASTIC SEWER PIPE
 - PIPE AND FITTINGS SHALL CONFORM TO THE FOLLOWING ASTM STANDARDS:

ASTM STANDARDS	GENERIC PIPE MATERIAL	SIZES APPROVED
D3034	*PVC (SOLID WALL)	8" THROUGH 15" (SDR 35)
F679	PVC (SOLID WALL)	18" THROUGH 27" (T-1 & T-2)
F789	PVC (SOLID WALL)	4" THROUGH 18" (T-1 TO T-3)
F794	PVC (RIBBED WALL)	8" THROUGH 36"
D2680	*ABS (COMPOSITES WALL)	8" THROUGH 15"

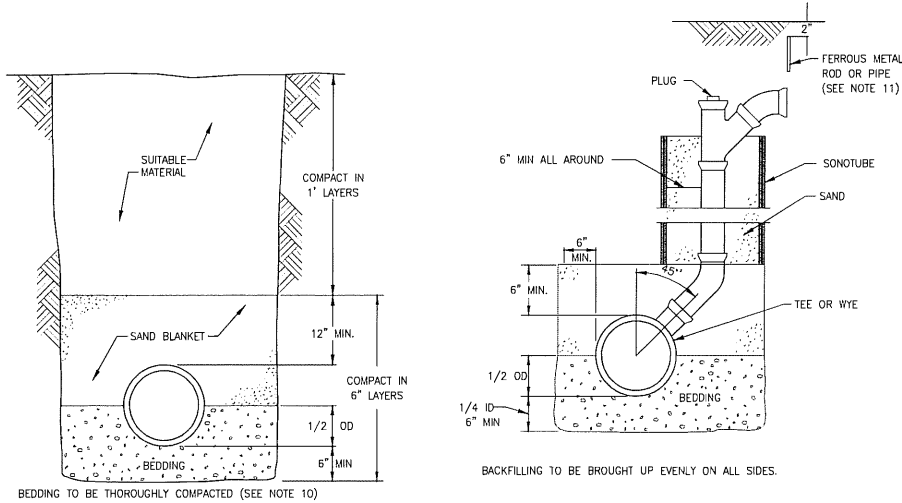
*PVC: POLY VINYL CHLORIDE
*ABS: ACRYLONITRILE-BUTADIENE-STYRENE
 - JOINTS SEALS FOR PVC PIPE SHALL BE OIL RESISTANT COMPRESSION RINGS OF ELASTOMERIC MATERIAL CONFORMING TO ASTM D-3212 AND SHALL BE PUSH-ON, BELL AND SPIGOT TYPE.
 - ABS TRUSS PIPE AND FITTINGS SHALL CONFORM TO ASTM D-2680, POLYMER COMPOUNDING SHALL BE TO ASTM D-1788 (CLASS 322).
 - JOINTS FOR ABS TRUSS PIPE SHALL BE CHEMICAL WELDED COUPLINGS TYPE SC IN ACCORDANCE WITH ASTM D-2680, FORMING A CHEMICAL WELDED JOINT.
 - DUCTILE-IRON PIPE, FITTINGS AND JOINTS.
 - DUCTILE IRON PIPE AND FITTINGS SHALL CONFORM TO THE FOLLOWING STANDARDS OF THE UNITED STATES OF AMERICA STANDARDS INSTITUTE:
 - A21.50 THICKNESS DESIGN OF DUCTILE IRON PIPE AND WITH ASTM A-536 DUCTILE IRON CASTINGS.
 - A21.51 DUCTILE IRON PIPE, CENTRIFUGALLY CAST IN METAL MOLDS OR SAND-LINED MOLDS FOR WATER OR OTHER LIQUIDS.
 - JOINTS SHALL BE OF THE MECHANICAL OR PUSH-ON TYPE. JOINTS AND GASKETS SHALL CONFORM TO:
 - A21.11 RUBBER GASKETS JOINTS FOR CAST IRON PRESSURE PIPE & FITTINGS
- DAMAGED PIPE SHALL BE REJECTED AND REMOVED FROM THE JOB SITE.
- JOINTS SHALL BE DEPENDENT UPON A NEOPRENE OR ELASTOMERIC GASKET FOR WATER-TIGHTNESS. ALL JOINTS SHALL BE PROPERLY MATCHED WITH THE PIPE MATERIALS USED, WHERE DIFFERING MATERIALS ARE TO BE CONNECTED, AS AT THE STREET SEWER WYE OR AT THE FOUNDATION WALL, APPROPRIATE MANUFACTURED ADAPTERS SHALL BE USED.
- TEES AND WYES: WHERE A TEE OR WYE IS NOT AVAILABLE IN THE EXISTING STREET SEWER, AN APPROPRIATE CONNECTION SHALL BE MADE, FOLLOWING MANUFACTURERS' INSTRUCTIONS USING A BOLTED, CLAMPED OR EPOXY-CEMENTED SADDLE TAPPED INTO A SMOOTHLY DRILLED OR SAWN OPENING IN THE SEWER. THE PRACTICE OF BREAKING AN OPENING WITH A SLEDGE HAMMER, STUFFING CLOTH OR OTHER SUCH MATERIAL AROUND THE JOINT, OR APPLYING MORTAR TO HOLD THE CONNECTION, AND ANY OTHER SIMILAR CRUDE PRACTICES OR INEPT OR HASTY IMPROVISATIONS WILL NOT BE PERMITTED. THE CONNECTION SHALL BE CONCRETE ENCASED AS SHOWN IN THE DETAIL UP TO AND INCLUDING 15" DIAMETER.
- SEWER SERVICE INSTALLATION: THE PIPE SHALL BE HANDLED, PLACED AND JOINTED IN ACCORDANCE WITH INSTALLATION GUIDES OF THE APPROPRIATE MANUFACTURER. IT SHALL BE CAREFULLY BEDDED ON A 6 INCH LAYER OF CRUSHED STONE AND/OR GRAVEL AS SPECIFIED IN NOTE 10. BEDDING AND RE-FILL FOR DEPTH OF 12 INCHES ABOVE THE TOP OF THE PIPE SHALL BE CAREFULLY AND THOROUGHLY TAMPED BY HAND OR WITH APPROPRIATE MECHANICAL DEVICES.

THE PIPE SHALL BE LAID AT A CONTINUOUS AND CONSTANT GRADE FROM THE STREET SEWER CONNECTION TO THE FOUNDATION AT A GRADE OF NOT LESS THAN 1/4" INCH PER FOOT. PIPE JOINTS MUST BE MADE UNDER DRY CONDITIONS. IF WATER IS PRESENT, ALL NECESSARY STEPS SHALL BE TAKEN TO DEWATER THE TRENCH.
- TESTING: THE COMPLETED SEWER SERVICE SHALL BE SUBJECTED TO A LEAKAGE TEST IN ANY OF THE FOLLOWING MANNERS: (PRIOR TO BACKFILLING)
 - AN OBSERVATION TEE SHALL BE INSTALLED AS SHOWN AND WHEN READY FOR TESTING, AN INFLATABLE BLADDER OR PLUG SHALL BE INSERTED JUST UPSTREAM FROM THE OPENING IN THE TEE. AFTER INFLATION, WATER SHALL BE INTRODUCED INTO THE SYSTEM ABOVE THE PLUG TO A HEIGHT OF 5 FEET ABOVE THE LEVEL OF THE PLUG.
 - THE PIPE SHALL BE LEFT EXPOSED AND LIBERALLY HOSED WITH WATER, TO SIMULATE, AS NEARLY AS POSSIBLE, WET TRENCH CONDITIONS OR, IF TRENCH IS WET, THE GROUND WATER SHALL BE PERMITTED TO RISE IN THE TRENCH OVER THE PIPE. INSPECTIONS FOR LEAKS SHALL BE MADE THROUGH THE CLEANOUT WITH A FLASHLIGHT.
 - DRY FLUORESCENCE DYE SHALL BE SPRINKLED INTO THE TRENCH OVER THE PIPE. IF THE TRENCH IS DRY, THE PIPE SHALL BE LIBERALLY HOSED WITH WATER, OR IF THE TRENCH IS WET, GROUND WATER SHALL BE PERMITTED TO RISE IN THE TRENCH OVER THE PIPE. OBSERVATION FOR LEAKS SHALL BE MADE IN THE FIRST DOWN-STREAM MANHOLE.

LEAKAGE OBSERVED IN ANY ONE OF THE ABOVE ALTERNATE TESTS SHALL BE CAUSE FOR NON-ACCEPTANCE AND THE PIPE SHALL BE DUG-UP IF NECESSARY AND RE-LAID SO AS TO ASSURE WATER TIGHTNESS.
- ILLEGAL CONNECTIONS: NOTHING BUT SANITARY WASTE FLOW FROM TOILETS, SINKS, LAUNDRY ETC. SHALL BE PERMITTED. ROOF LEADERS, FOOTING DRAINS, SUMP PUMPS OR OTHER SIMILAR CONNECTIONS CARRYING RAIN WATER, DRAINAGE OR GROUND WATER SHALL NOT BE PERMITTED.
- WATER SERVICE SHALL NOT BE LAID IN SAME TRENCH AS SEWER SERVICE.
- BEDDING: SCREENED GRAVEL AND/OR CRUSHED STONE FREE FROM CLAY, LOAM, ORGANIC MATERIAL AND MEETING ASTM C33-87.

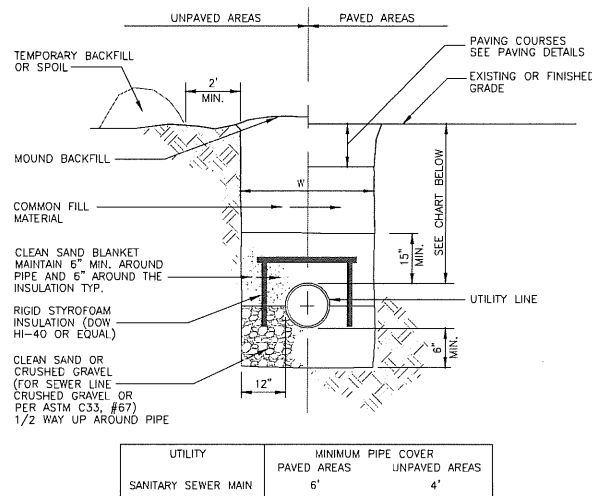
100% PASSING	1 INCH SCREEN
90%-100% PASSING	3/4 INCH SCREEN
20%-85% PASSING	3/8 INCH SCREEN
0%-10% PASSING	#4 SIEVE
0%-5% PASSING	#8 SIEVE

WHERE ORDERED BY THE ENGINEER TO STABILIZE THE TRENCH BASE, SCREENED GRAVEL OR CRUSHED STONE 1/2 INCH TO 1 1/2 INCH SHALL BE USED.



TRENCH CROSS-SECTION
NOT TO SCALE

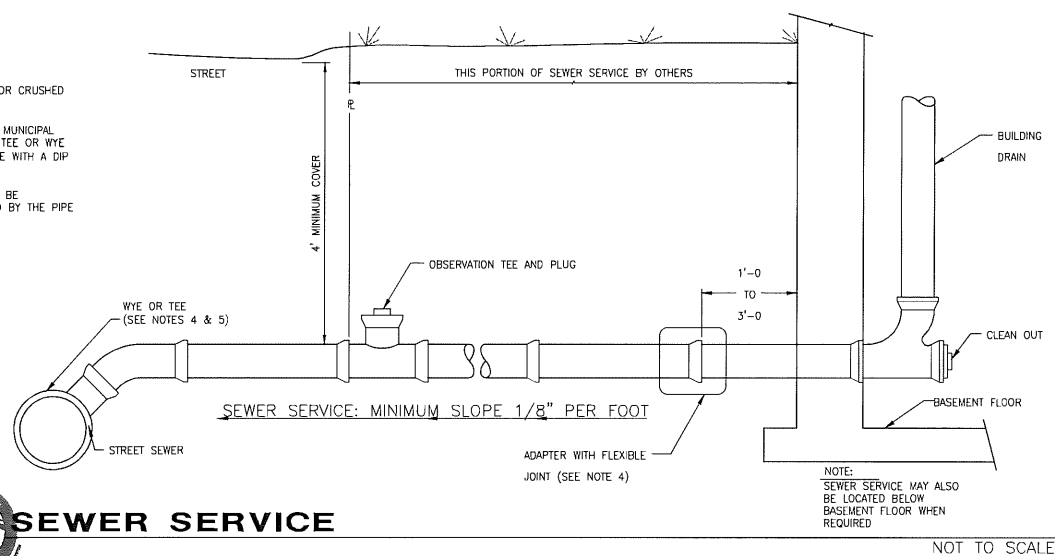
CHIMNEY (SEE NOTE 12)
NOT TO SCALE



NOTE:
W=MAXIMUM ALLOWABLE TRENCH WIDTH TO A PLANE 12 INCHES ABOVE THE PIPE. FOR PIPES 15 INCHES NOMINAL DIAMETER OR LESS, W SHALL BE NO MORE THAN 36 INCHES; FOR PIPES GREATER THAN 15 INCHES NOMINAL DIAMETER, W SHALL BE 24 INCHES PLUS PIPE O.D. W SHALL ALSO BE THE PAYMENT WIDTH FOR LEDGE EXCAVATION AND FOR ORDERED EXCAVATION BELOW GRADE.

PIPE INSULATION

8" SEWER SERVICE NOT TO SCALE



SEWER SERVICE DETAILS

GRAVITY SEWER NOTES

- MINIMUM SIZE PIPE FOR GRAVITY SEWER SHALL BE 8-INCHES.
- PIPE AND JOINT MATERIALS FOR PLASTIC SEWER PIPE SHALL CONFORM TO THE FOLLOWING ASTM STANDARDS:

ASTM STANDARDS	GENERIC PIPE MATERIAL	SIZES APPROVED
D3034-04a	*PVC (SOLID WALL)	8" THROUGH 15" (SDR 35)
F679-03	PVC (SOLID WALL)	18" THROUGH 27" (T-1 & T-2)
F794-03	PVC (RIBBED WALL)	8" THROUGH 36"
F1760-01(2005)e1	PVC, RECYCLED	ALL DIAMETERS

*PVC: POLY VINYL CHLORIDE
- PLASTIC SEWER PIPE SHALL HAVE A PIPE STIFFNESS RATING OF AT LEAST 46 POUNDS PER SQUARE INCH AT 5 PERCENT PIPE DIAMETER DEFLECTION, AS MEASURED IN ACCORDANCE WITH ASTM D2412-02 DURING MANUFACTURE.
- JOINTS SEALS FOR PVC PIPE SHALL BE OIL RESISTANT COMPRESSION RINGS OF ELASTOMERIC MATERIAL CONFORMING TO ASTM D-3212-96(a)(2003)e1 AND SHALL BE PUSH-ON, BELL AND SPIGOT TYPE.
- DUCTILE-IRON PIPE, FITTINGS AND JOINTS SHALL CONFORM TO THE FOLLOWING STANDARDS OF THE AMERICAN WATER WORKS ASSOCIATION (AWWA).

AWWA C151/A21.51-02 THICKNESS DESIGN OF DUCTILE IRON PIPE AND WITH ASTM A-536-84 (2004) DUCTILE IRON CASTINGS.

AWWA C151/A21.51-02 DUCTILE IRON PIPE, CENTRIFUGALLY CAST IN METAL MOLDS OR SAND-LINED MOLDS FOR WATER OR OTHER LIQUIDS.

JOINTS SHALL BE OF THE MECHANICAL OR PUSH-ON TYPE. JOINTS AND GASKETS SHALL CONFORM TO AWWA C151/A21.11 RUBBER GASKETS JOINTS FOR CAST IRON PRESSURE PIPE & FITTINGS.
- CONCRETE PIPE SHALL CONFORM TO AWWA C302-04.
- PRESTRESSED CONCRETE CYLINDER PIPE AND FITTINGS SHALL CONFORM TO AWWA C301-99.

JOINTS SEALS FOR CONCRETE CYLINDER PIPE SHALL BE OIL RESISTANT ELASTOMERIC MATERIAL CONFORMING TO AWWA C301-99 SPECIFICATIONS.
- DAMAGED PIPE SHALL BE REJECTED AND REMOVED FROM THE JOB SITE.
- GRAVITY SEWER PIPE TESTING SHALL BE AS FOLLOWS:

ALL NEW GRAVITY SEWERS SHALL BE TESTED FOR WATER TIGHTNESS BY THE USE OF LOW-PRESSURE AIR TESTS.

LOW PRESSURE AIR TESTING SHALL BE IN CONFORMANCE WITH:

ASTM F1417-92(2005) "STANDARD TEST METHOD FOR INSTALLATION ACCEPTANCE OF PLASTIC GRAVITY SEWER LINES USING LOW PRESSURE AIR".

UNI-BELL PVC PIPE ASSOCIATION UNI-B-6, "LOW PRESSURE AIR TESTING OF INSTALLED SEWER PIPE".
- ALL NEW GRAVITY SEWERS SHALL BE CLEANED AND VISUALLY INSPECTED AND SHALL BE TRUE TO LINE AND GRADE FOLLOWING INSTALLATION AND PRIOR TO USE.
- ALL PLASTIC SEWER PIPE SHALL BE DEFLECTION TESTED NOT LESS THAN 30 DAYS FOLLOWING INSTALLATION.
- THE MAXIMUM ALLOWABLE DEFLECTION OF FLEXIBLE SEWER PIPE SHALL BE 5 PERCENT OF THE AVERAGE INSIDE DIAMETER.
- TRENCH CONSTRUCTION SHALL CONFORM TO THE FOLLOWING:

TRENCH DIMENSIONS FOR SEWER PIPE LESS THAN 15 INCHES IN DIAMETER, THE ALLOWABLE TRENCH WIDTH AT A PLANE 12 INCHES ABOVE THE PIPE SHALL BE NO MORE THAN 36 INCHES AND FOR PIPE 15 INCHES AND LARGER, THE ALLOWABLE WIDTH SHALL BE EQUAL TO THE PIPES OUTSIDE DIAMETER PLUS 24 INCHES.

PIPE TRENCH BEDDING MATERIAL AND FILL MATERIAL FOR EXCAVATION BELOW GRADE SHALL BE SCREENED GRAVEL OR CRUSHED STONE TO ASTM C33-03 STONE SIZE NO. 67. THE PIPE SAND BLANKET MATERIAL SHALL BE GRADED SAND FREE FROM ANY ORGANIC MATERIALS, GRADED SUCH THAT 100 PERCENT PASSED THE 1/2-INCH SIEVE AND A MAXIMUM OF 15 PERCENT PASSES A #200 SIEVE. IN LIEU OF A SAND BLANKET, A STONE ENVELOPE 6 INCHES THICK COMPLETELY AROUND THE PIPE USING 3/4-INCH STONE MAY BE USED.

PIPE BEDDING MATERIAL SHALL EXTEND FROM A HORIZONTAL PLANE THROUGH THE PIPE AXIS TO 6-INCHES BELOW THE BOTTOM OF THE OUTSIDE SURFACE OF THE PIPE.

PIPE SAND BLANKET MATERIAL SHALL COVER THE PIPE A MINIMUM OF 12 INCHES ABOVE THE CROWN OF THE OUTSIDE SURFACE.

COMPACTION SHALL BE IN 12-INCH LAYERS FOR BEDDING AND BLANKET MATERIALS.

BACKFILL MATERIAL SHALL BE IN 3-FOOT LAYERS TO THE GROUND SURFACE EXCEPT FOR ROAD CONSTRUCTION WHERE THE FINAL 3-FEET SHALL BE COMPACTED IN 12-INCH LAYERS TO THE ROAD BASE SURFACE.

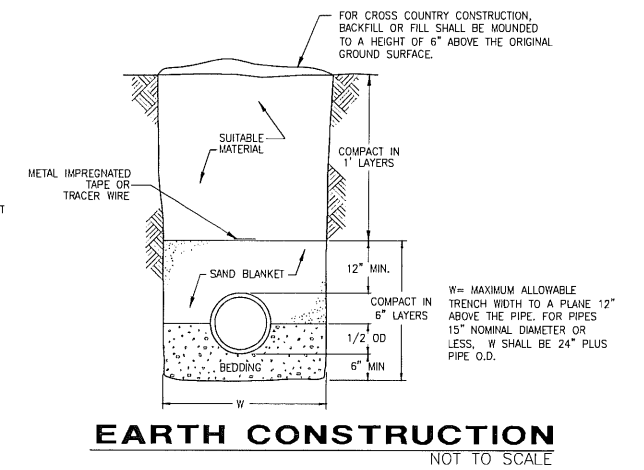
TRENCH BACKFILL MATERIAL IN ROADWAY LOCATIONS SHALL BE NATURAL MATERIALS EXCAVATED FROM THE TRENCH DURING CONSTRUCTION, EXCLUDING DEBRIS, PAVEMENT PIECES, ORGANIC MATTER, TOP SOIL, WET OR SOFT MUCK, PEAT, CLAY, EXCAVATED LEDGE, ROCKS OVER 6 INCHES IN THE LARGEST DIMENSION, OR ANY OTHER UNSUITABLE MATERIAL NOT APPROVED BY THE ENGINEER.

TRENCH BACKFILL AT CROSS-COUNTRY LOCATIONS SHALL BE AS DESCRIBED ABOVE EXCEPT THAT THE ENGINEER MAY PERMIT THE USE OF TOP SOIL, LOAM, MUCK OR PEAT, IF HE IS SATISFIED THAT THE COMPLETED CONSTRUCTION WILL BE ENTIRELY STABLE AND PROVIDED THAT EASY ACCESS TO THE SEWER FOR MAINTENANCE AND POSSIBLE RECONSTRUCTION, WHEN NECESSARY WILL BE PRESERVED. BACKFILL SHALL BE MOUND 6-INCHES ABOVE ORIGINAL GROUND.

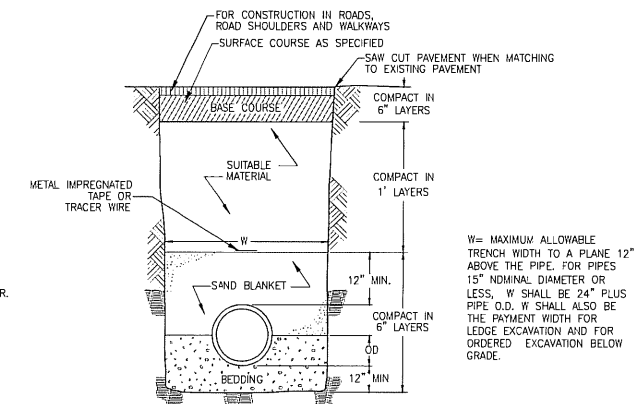
BASE COURSE MATERIALS FOR TRENCH REPAIRS SHALL MEET THE REQUIREMENTS OF DIVISION 300 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION.

WHERE SHEETING IS PLACED ALONG SIDE OF THE PIPE AND EXTENDS BELOW MID-DIAMETER, THE SHEETING SHALL BE CUT OFF AND LEFT IN PLACE TO AN ELEVATION NOT LESS THAN ONE FOOT ABOVE THE TOP OF THE PIPE AND AT LEAST 3 FEET BELOW FINISH GRADE.

TRENCHES FOR SEWER PIPES WITH SLOPES OVER 0.08 FEET PER FOOT AND TRENCHES FOR SEWER PIPES BELOW THE SEASONAL HIGH GROUND WATER LEVEL SHALL HAVE IMPERVIOUS TRENCH DAMS CONSTRUCTED EVERY 300 FEET TO PREVENT POTENTIAL DISTURBANCE TO PIPE BEDDING AND BLANKET MATERIALS.



EARTH CONSTRUCTION
NOT TO SCALE



LEDGE CONSTRUCTION
NOT TO SCALE

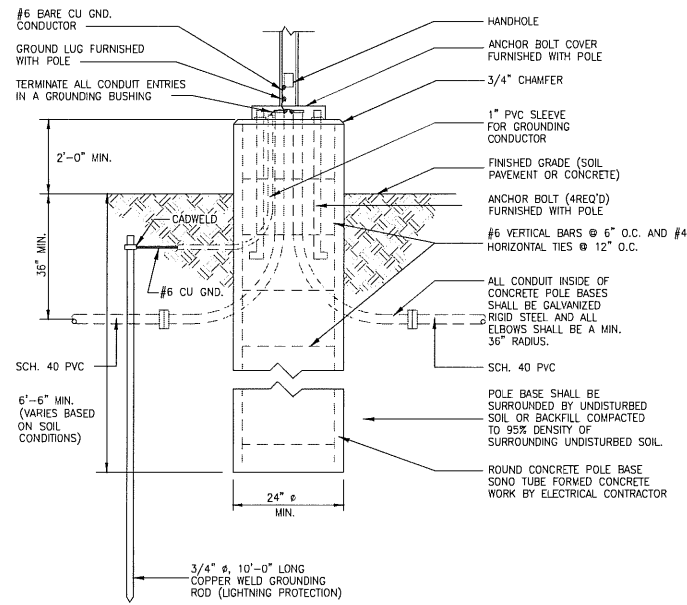
TAX MAP 26 LOT 169
DETAIL SHEET
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.
SCALE: AS SHOWN
MARCH 22, 2021

TFM Civil Engineers
Structural Engineers
Traffic Engineers
Land Surveyors
Landscape Architects
Scientists

48 Constitution Drive
Bedford, NH 03110
Phone (603) 472-4488
Fax (603) 472-9747
www.tfmoran.com

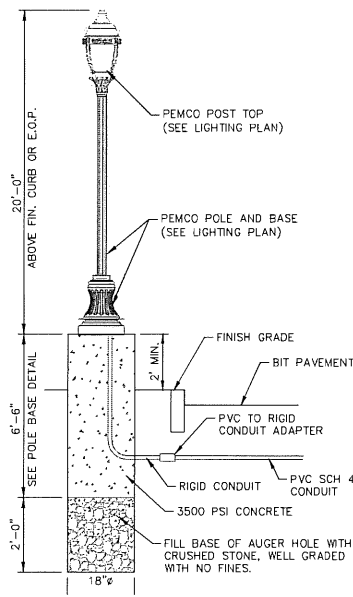
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CK JK CADFILE 76451-21 COVER-DETAILS SHEET 13 OF 15

REV	DATE	DESCRIPTION	DR	CK



LIGHT POLE BASE

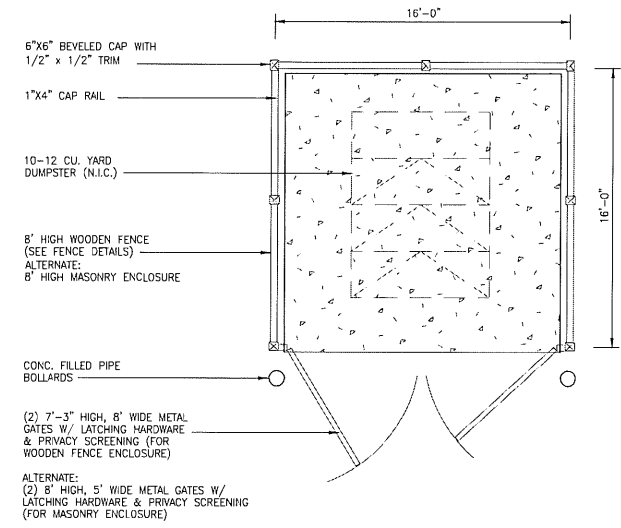
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LAMP AND POST

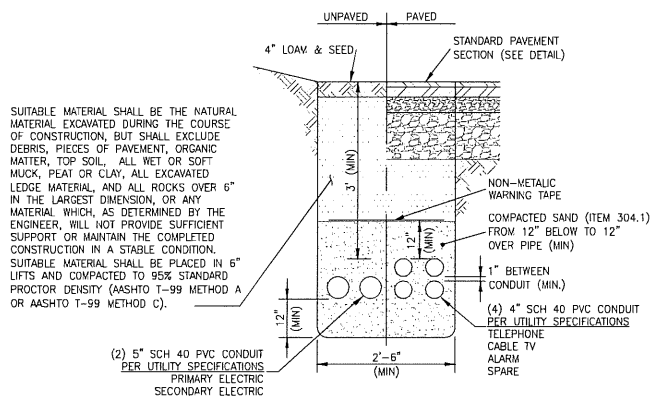
NOT TO SCALE

- NOTES**
1. FIXTURES MUST BE GROUNDED IN ACCORDANCE WITH LOCAL ELECTRICAL CODES, OR THE NATIONAL ELECTRICAL CODE.
 2. CENTER OF POLE BASES TO BE SET 4'-0" FROM EDGE OF PAVEMENT, EXCEPT WHERE OTHERWISE INDICATED ON DRAWING.
 3. LIGHTING SHOWN HERE IS AREA LIGHTING, SUPPLEMENTAL BUILDING-MOUNTED FIXTURES AT DOORWAYS, ETC. MAY BE REQUIRED.
 4. SEE LIGHTING PLAN FOR FIXTURE SCHEDULE.



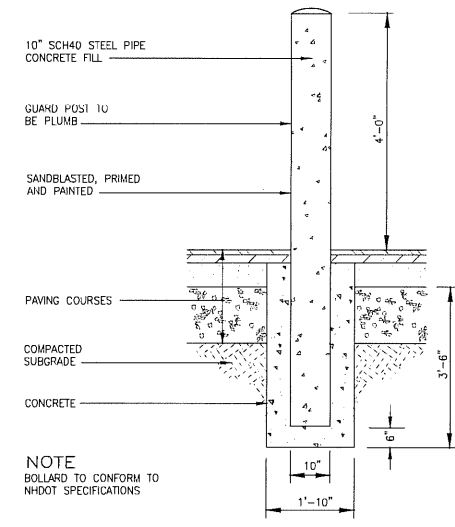
DUMPSTER ENCLOSURE - WOOD

NOT TO SCALE



UTILITY TRENCH

NOT TO SCALE



BOLLARD

NOT TO SCALE

TAX MAP 26 LOT 169
DETAIL SHEET
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
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SCALE: AS SHOWN
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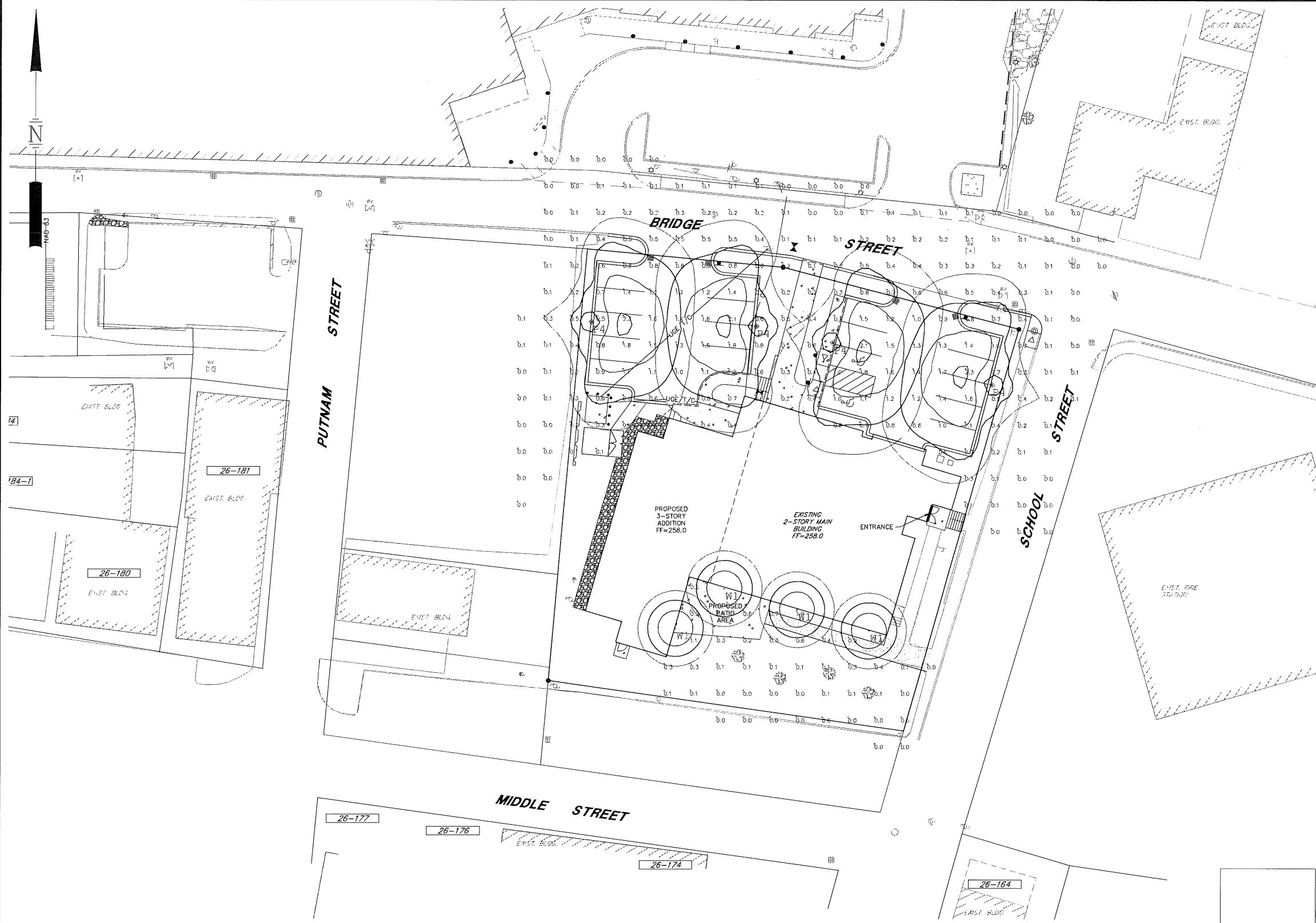
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	CK	JK	CADFILE	76451-21 COVER-DETAILS	SHEET 14 OF 15

REV	DATE	DESCRIPTION	DR	CK

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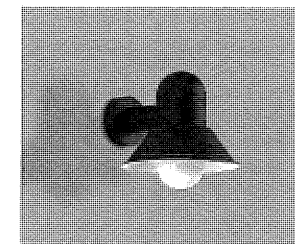
Mar 22, 2021 - 12:23pm
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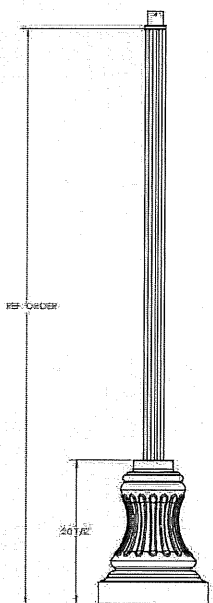
Symbol	Qty	Label	Arrangement	Description
★	4	P4	SINGLE	LEX-NL-PLC5-40W27K-U-4-A1-T2-N-N-BK / PLB130-4-12-S-.188-1300-N-BK (12' POLE)
◀	4	W1	SINGLE	66410-K27-BLK / WALL MTD 10' AFG

StatArea_1
 WEST PARKING LOT
 Illuminance (Fc)
 Average = 1.27
 Maximum = 2.3
 Minimum = 0.5
 Avg/Min Ratio = 2.54
 Max/Min Ratio = 4.60

StatArea_2
 EAST PARKING LOT
 Illuminance (Fc)
 Average = 1.31
 Maximum = 2.3
 Minimum = 0.7
 Avg/Min Ratio = 1.87
 Max/Min Ratio = 3.29



BEGA LED WALL LUMIERE



PEMCO POLE LAMPS

TAX MAP 26 LOT 169
LIGHTING PLAN (BY CHARRON LIGHTING)
MILFORD INDEPENDENT SENIOR HOUSING
54 SCHOOL STREET, MILFORD, NH
 OWNED BY/PREPARED FOR
HOUSING INITIATIVES OF NEW ENGLAND CORP.

SCALE: 1" = 20' MARCH 22, 2021

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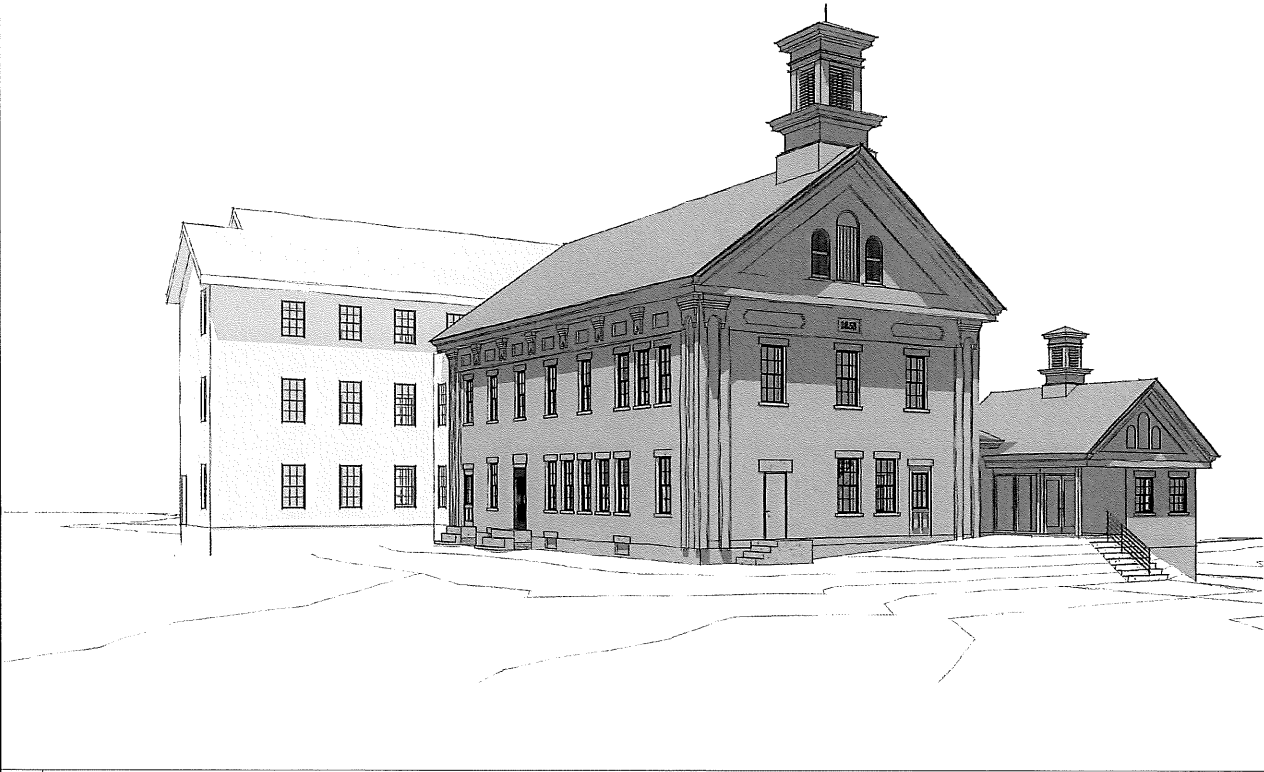


HORIZONTAL SCALE 1"=20'

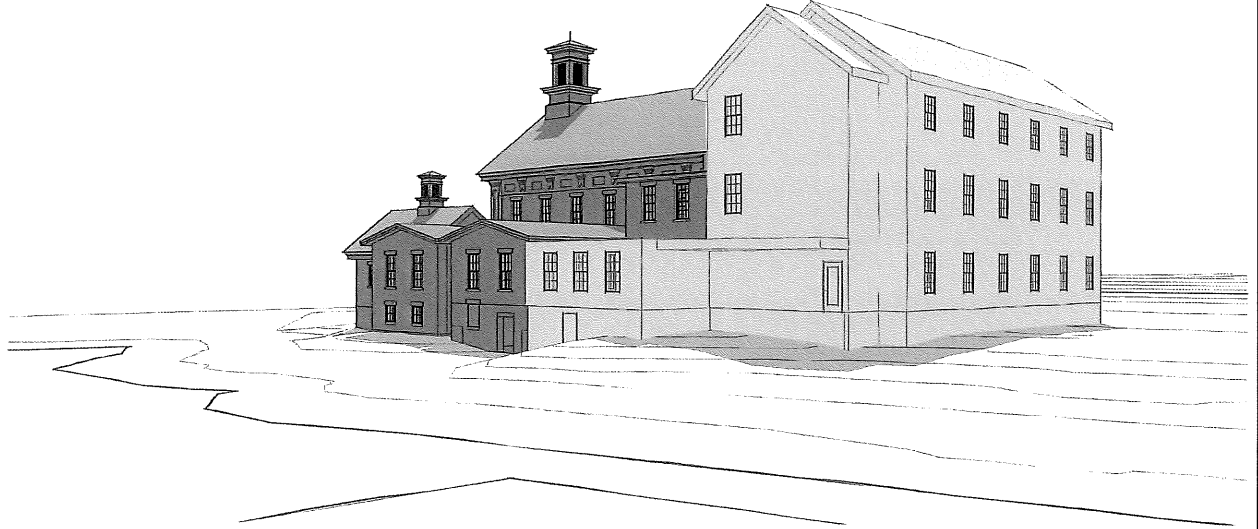
REV	DATE	DESCRIPTION	DR	CK

TFM	Civil Engineers	48 Constitution Drive
	Structural Engineers	Bedford, NH 03110
	Traffic Engineers	Phone (603) 472-4488
	Land Surveyors	Fax (603) 472-9747
	Landscape Architects	www.tfmoran.com
	Scientists	
76451.21	DR SRP FB	
	CK JK CADFILE	76451-21 SITE
		SHEET L-1

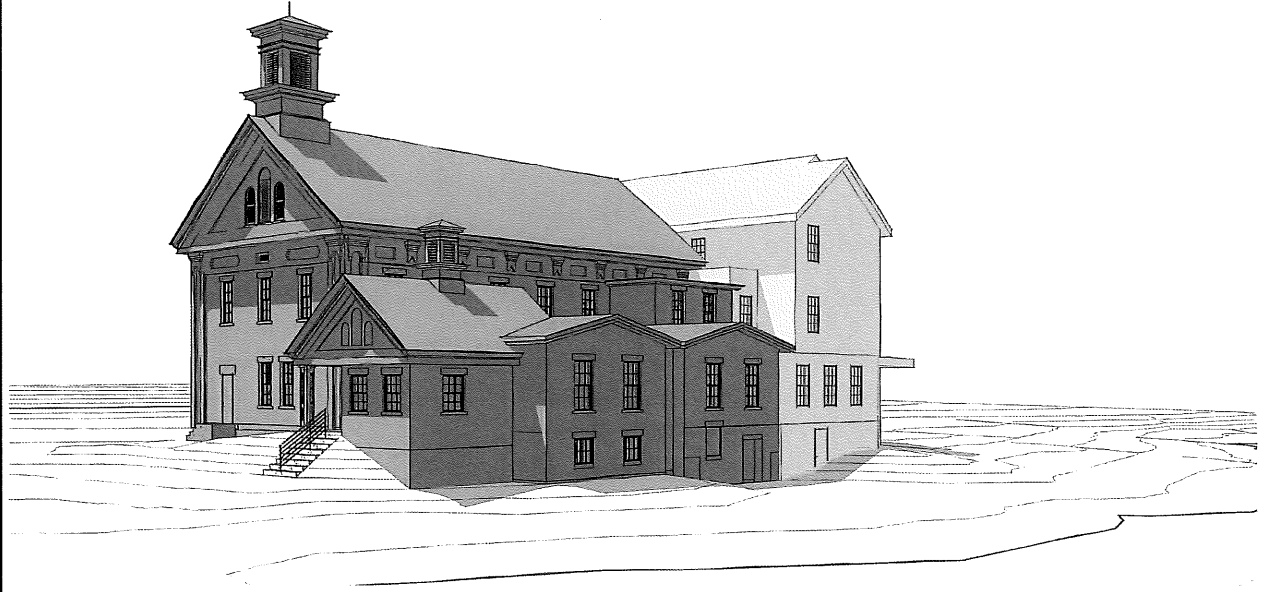
BIM: 20230217 Milford Cabinet Shop 0877 MILFORD CABINET SHOP_0211.rvt
 TEMPLATE DATE: 11/25/2019
 one eighth inch = one foot
 one quarter inch = one foot
 three quarter inch = one foot
 one inch = one foot
 one and one half inches = one foot
 three inches = one foot



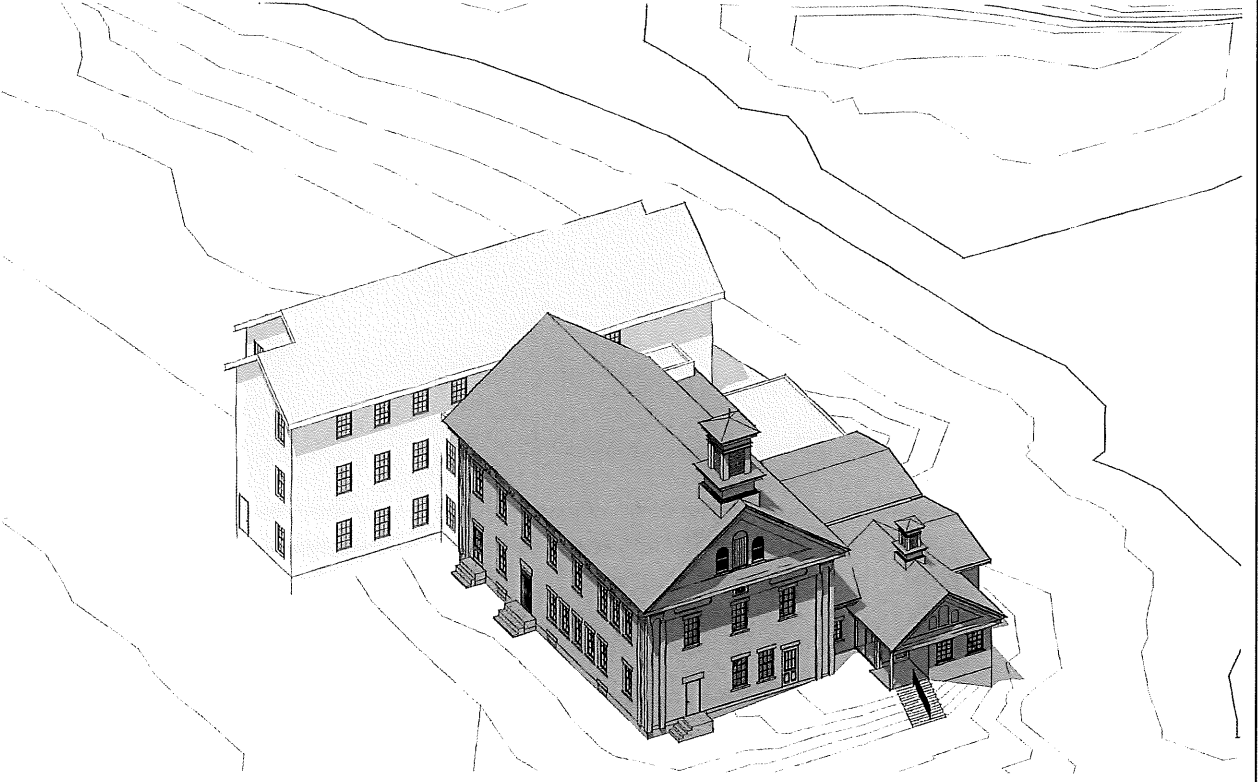
1 VIEW FROM CORNER OF MIDDLE ST. & SCHOOL ST. (SOUTHEAST OF LOT)



2 VIEW FROM LINSEY LANDING PARKING LOT (NORTHWEST OF LOT)



3 VIEW FROM CORNER OF MIDDLE ST. & BRIDGE ST. (NORTHEAST OF LOT)



4 ORTHOGRAPHIC VIEW FROM SOUTHEAST SKY

WA
WARRENSTREET
ARCHITECTS
 27 Warren Street Concord NH 03301
 T 603.225.0640 F 603.225.0621 www.warrenstreet.coop

OWNER:
 HOUSING INITIATIVES OF NEW ENGLAND CORPORATION
 284 U.S. ROUTE 1
 BUILDING 300, SUITE 2A
 SCARBOROUGH, ME 04074
 P. (207) 774-8812 F. (207) 510-6366
 CONSTRUCTION MANAGER:
 N/A

PLAN KEY:

PROJECT TITLE / ADDRESS:
MILFORD CABINET BUILDING
 54 SCHOOL STREET
 MILFORD, NH 03055

SCALE: AS NOTED DWN BY: CC
 JOB #: 3577 CHK BY: JH
 PRINT DATE: 3/2/2021 8:28:33 PM
 ISSUE DATE:

SCHEMATIC DESIGN CONCEPTS

REVISION	DATE	COMMENTS

CONCEPT 6 MASSING STUDY

A16

SHEET NUMBER: 5 OF 5 ARCHITECTURAL
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PROGRESS NOT FOR CONSTRUCTION

REVISION	DATE	COMMENTS

PROGRESS NOT FOR CONSTRUCTION



1 NORTH ELEVATION
 A12 1/8" = 1'-0"



2 SOUTH ELEVATION
 A12 1/8" = 1'-0"

BIM 360/3677 Milford Cabinet Shop3677 MILFORD CABINET SHOP- R21.rvt
 TEMPLATE DATE: 11/25/2019
 one eighth inch = one foot
 one quarter inch = one foot
 one and one half inches = one foot
 three inches = one foot

OWNER
 HOUSING INITIATIVES OF NEW ENGLAND CORPORATION
 264 U.S. ROUTE 1
 BUILDING 300, SUITE 2A
 SCARBOROUGH, ME 04074
 P. (207) 774-8812 F. (207) 510-6366

CONSTRUCTION MANAGER
 N/A

PLAN KEY:

PROJECT TITLE / ADDRESS:
MILFORD CABINET BUILDING
 54 SCHOOL STREET
 MILFORD, NH 03055

SCALE: AS NOTED DWN BY: CC
 JOB #: 3677 CHK BY: JH

PRINT DATE: 3/25/2021 9:53:20 AM

ISSUE DATE:

SCHEMATIC DESIGN CONCEPTS

REVISION	DATE	COMMENTS

CONCEPT 6 - ELEVATIONS

A18

SHEET NUMBER: 7 OF 7 ARCHITECTURAL

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PROGRESS NOT FOR CONSTRUCTION



1 EAST ELEVATION
 A12 1/8" = 1'-0"



2 WEST ELEVATION
 A12 1/8" = 1'-0"

BIM 360/3677 Milford Cabinet Shop3677 MILFORD CABINET SHOP - R21.rvt
 TEMPLATE DATE: 11/25/2019