



TOWN OF MILFORD, NH
OFFICE OF COMMUNITY DEVELOPMENT

1 UNION SQUARE, MILFORD, NH 03055 TEL: (603)249-0620 WEB: WWW.MILFORD.NH.GOV

Date: October 11, 2022
To: Planning Board
From: Camille Pattison, NRPC Assistant Director
Subject: SP2022-08 Sooner Transportation LLC, for the property located at Tax Map 12, Lot 11, 754-756 Elm Street (Horseless Carriage Site). Public Hearing for a Major Site Plan Application to construct a two-story, 10,320 sf. auto sales and repair facility with associated parking, stormwater management/drainage, landscaping, and lighting improvements on the subject property located in the Commercial ‘C’ Zoning District and West Elm Overlay District.

BACKGROUND/PROPOSAL:

The applicant is before the Planning Board for a major site plan application to raze the existing Horseless Carriage building and construct a new 10,320 sf building with associated, parking, site and drainage improvements in the Commercial Zoning District. The property is also subject to the West Elm Overlay District, Groundwater Protection District and the Shoreland Protection Zone. Attached please find the site plan submittal.

This will be the second meeting for the application. The anticipated focus of this meeting will be for the applicant to provide a summary update on the recent discussions with NHDOT, revised site layout/design, location of the proposed building, internal circulation, and compliance with the West Elm Overlay District.

Attached, please find a revised full submittal package, including responses to staff comments, updated drainage plans and revised site plan (revision date 10/7/22) from the applicant’s engineer, Bedford Design Consultants.

EXISTING USE/CONDITIONS:

The proposed parcel, Tax Map 12, Lot 11 consists of approximately .889 acres (38,615 sq.ft.) with 171.77 linear feet of frontage on Elm Street. The property is located adjacent to two other car dealership/repair facilities, just north of the Pan Am Railway and south of the Souhegan River. The property is currently home to an existing car dealership and repair facility, with wetlands to the south and some limited tree coverage primarily at the rear of the lot. The existing facility has 3 garage doors with 8 bays inside, and the proposed facility will have 4 garage doors with 7 bays inside, including a paint booth.

A small wetland resource area has been delineated along the southern property line, but will not be impacted by the project. A proposed pond for roof water will be located in the southeastern corner of the lot.

LOT AREA:

Proposed Tax Map 12, Lot 11 = ±.889 acres (38,615 sq.ft.)

APPLICATION STATUS:

The application was accepted on August 16, 2022, and the applicant presented updated information at the September 20, 2022 board meeting. The Board voted that the project posed no regional impact.

NOTICES:

Notices were sent to all property abutters.

WAIVERS:

- 1) Request for waivers from the Development Regulations, Section 6.05.4 to allow a reduction of required off

street parking spaces from 28 or 21 (depending on the calculation of bays vs. employees) to a proposed number of 19 spaces.

- 2) Request for waivers from Development Regulations, Section 6.08.5(B)1 and 6.08.7(A) from the required landscaping buffer for the east and north side of the property.

Additional waivers requested:

- 3) **From the Development Regulations Section 6.05.6.E for parking within 10' of the Right of Way.**
- 4) **From the Development Regulations Section 6.05.6.E and Section 6.05.3.A.4 from accessible Parking Space Dimensions.**
- 5) **From the Zoning Ordinance, Section 6.07.6.E.5.a – Parking lots should be located on side or rear of buildings and Section 6.07.6.E.1.a – Proposed buildings shall maintain an appropriate street edge to adjacent structures for the location of the proposed building and parking.**

ZONING DISTRICTS:

The proposed parcel lies within the Commercial Zoning District. This District is intended to be the area in which vehicular oriented business can occur. Pursuant to Section 5.05.01 of the Zoning Ordinance, motor vehicle sales and motor vehicles repairs are allowed.

The property also falls within the West Elm Street Gateway Overlay District. The intent of this District is to encourage economic development in the West Elm Street Gateway District by enhancing, preserving and protecting the natural, historic and cultural resources of the Town. The purpose of this Ordinance is to implement the Town of Milford's vision for the future as set forth in the Community Character Chapter of the Master Plan 2007 Update: Goal No. 2: Foster the traditional character of Milford's neighborhoods by encouraging a human scale of development that is similar in setbacks, size and height, and that is comfortable and safe for pedestrians and non-motorized vehicles while allowing for an efficient and safe roadway network.

The current configuration of the new facility will allow the property owner to maintain current operations during construction. However, upon review, the design, layout, and architecture of the building conflicts with the spirit/intent of the West Elm Street Gateway Overlay District, which aims to orient buildings with the roadway and towards the front of the lot, with parking shielded in the rear and to the side of the property in addition to appropriate building design.

The property falls within the Groundwater Protection District, with most of the lot falling within the Level 2 Area and a small portion to the northwest located within the Level 1 Area. The parcel must comply with the performance standards of Section 6.01.2 and the Shoreland Protection Zone in the Zoning Ordinance.

TRAFFIC AND ACCESS MANAGEMENT:

Access and egress from the retail facility will be from a 24' wide driveway curb cut on Elm Street, in the same location as the current entrance. This will be a consolidation of the existing 2 curb cuts. The driveway egress design allows for both east and west movement and a single lane access into the property. **The project did not receive approval from NH DOT for drop off in the ROW and therefore will now unload in front of the building.**

Anticipated versus existing peak traffic volume data is provided in a letter dated August 9, 2022 and provided to NH DOT.

PARKING:

The project proposes to provide 20 parking spaces for staff and customers with 40 spaces for display cars. The requirement is for a minimum of 28 or 21 spaces depending on the calculation of bays versus employees.

DRAINAGE/STORMWATER MANAGEMENT:

The stormwater and drainage design has been reviewed by the Town's engineering consultant. See attached August 11, 2022 Site Plan Drainage Review letter.

Of note, is the proposed installation of pervious paver infiltration and underground located in the middle of the display

parking spaces.

A new drainage submittal package has been provided to the Town's engineering consultant and is waiting feedback.

UTILITIES:

The proposed retail establishment will access the Milford waste water system via the existing sewer line. The applicant has shown an additional water line from the street.

INTERDEPARTMENTAL REVIEWS:

Ambulance: No comments

Assessing: No comments.

Building Department: No comments.

Code Enforcement/ Health: No comments.

Conservation Commission: No Comments.

Environmental Programs/Stormwater: See attached October 13, 2022 Site Plan Drainage review by the Town Engineering Consultant, KV Partner stating that all comments have been addressed.

Fire Department:

1. The total square footage of the building is actually 10,320 as proposed. *Resolved*
2. The Life Safety Code occupancy classification is a Special-Purpose Industrial Occupancy and would need to comply with Chapter 40 of the LSC. – *Noted*
3. The Building Code occupancy classification is an S-1 storage occupancy. - *Noted*
4. An automatic fire detection and notification (fire alarm) system is required. – *Resolved Note17 on Cover Sheet*
5. An automatic sprinkler system is required for this building. I did note a proposed 6” fire service on the grading and utility plan, so I’m assuming that they’re planning on providing one. – *Resolved Note17 on Cover Sheet*
6. Milford Fire Department requires a fire hydrant within 100’ of fire department connections, which will require a private hydrant to be installed as part of the project. – *Applicant noted a proposed hydrant – exact location should be found. Comment not addressed.*
7. The engineer must design the fire department access to accommodate our ladder truck weight, approach/departure angles, and turning radius. Specifications available from MFD upon request. – *Resolved see sheet 7.*
8. Fire lane markings will be required on the fire department access. – *Resolved see sheet 7.*

Heritage Commission: No comments.

Police Department: No comments.

Public Works:

1. Project will require a NH DOT and Driveway Permit. – *Noted*

Water/Sewer Utilities:

1. The service needs to be from the main in the street, not stubbed off the fire line. The domestic should be 1 inch CTS running 18 inches from the 6-inch fire line. Please revise plan accordingly. *Resolved*
2. The Sewer once it reaches the Town right of way must turn from 4 inch to 6-inch SDR 35. Please revise plan accordingly. *Resolved*
3. The Water line out in the road is a 12 inch, not 8 inches. Please revise plan accordingly. *Resolved*

Community Development / Planning:

1. A discrepancy appears on the plan regarding the lot size. This needs to be addressed and made consistent. *Resolved*
2. Ensure that all plantings do not impact sight distance for access/egress along Wilton Road. *Resolved*
3. The current configuration of the new facility will allow the property owner to maintain current operations during construction, however is not meeting the spirit of the West Elm Street Gateway Overlay District, which aims to orient buildings with the roadway and towards the front of the lot, with parking shielded in the rear and to the side of the property.
4. The property falls within the Groundwater Protection District, with most of the lot falling within the Level 2 Area and a small portion to the Northwest lies within the Level 1 Area. The parcel must comply with the performance standards of Section 6.01.2 and the Shoreland Protection Zone. The applicant will need to identify what chemicals and materials will be stored on-site.
5. No additional traffic related information has been submitted as part of this site plan, and anticipated versus existing peak traffic volumes should be reviewed. *See letter dated August 9, 2022.*
6. Add a note to the plan indicating that in the event the proposed snow removal areas are not sufficient, snow will be relocated off-site. *Resolved*
7. Please amend the note saying demolition will be completed under the rules of the town of Manchester. – *Resolved*
8. Sheet 1, Note 8 – Please update the site plan to include the existing and resulting open space calculations in the *Dimensional Regulations: Commercial* table. *Resolved*
9. Please revise site plan to include all proposed signage for the project. *Resolved*
10. Sheets 1 and 10 – The site plan shows the proposed location of the dumpster and pad encircled by an 8' stockade fence within the 15-foot westerly side dimensional setback. The proposed 8' stockade fence and concrete pad are defined as structures in the Town's Zoning Ordinance and such, would require a Special Exception by the Board of Adjustment for relief of this requirement. The alternative would be amending the plan and relocate the dumpster outside of the dimensional setback. *Resolved*
11. Sheet 1 – The plan displays a 12' wide loading area within the state owned right of way. The proposal may effect and impact the traffic conditions on Elm Street and abutting properties. Further as stated in Section 6.05.6.E, all off-street parking and loading or unloading spaces shall be at least ten feet (10') from any public street right-of-way and shall be separated from such right-of-way by a granite curb and landscaping as required in 6.08 LANDSCAPING STANDARDS. The proposal require a waiver of these requirements and require NHDOT approval and dedicated easement to use the area within the right of way.

Staff suggests further discussion with the applicant to determine if the proposed loading area could be designed to have a dual purpose for a loading area and bus stop (Subject to NHDOT approval).

The loading area has been relocated in front of the building, NH DOT requested that the existing island be extended to the property line. The display area is entirely located on the lot, but within the 10' setback with no landscaping buffer. Is there an easement with the adjacent property?
12. Sheet 1 – The plan shows display parking space partially located off the property and within the State right-of-way. As stated in the Zoning Ordinance, Section 6.05.6.E, Parking and loading spaces shall be arranged so that cars will not back into public roads. The plan will need to amended to relocate said parking spaces within the subject property at least 10 feet from the Elm Street public right-of-way.
13. Sheet 1 - The applicant should explain the internal circulation, access/egress patterns for the 40 display vehicles. At a minimum, the plan should be revised to include the appropriate isle widths between the rows of display vehicle parking. *One row of cars has been removed and a 14' aisle added.*
14. Sheet 1 – Staff questions if the adequate snow storage areas have been provided. A note should be added to the plan stating that snow shall be removed to an offsite location using best management practices. *Resolved see not 16 on Sheet 1.*
15. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.1.a, a new structures shall maintain an

appropriate street edge in relationship to adjacent structures. The location of the proposed building is set within the rear section of the property, which conflicts with buildings located to the east and west.

16. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.2.b, the proposed building should enhance the definition of each floor of the building through terracing, material changes, articulated structural elements, changes in materials and horizontal trim bands. The applicant should explain how the proposed building complies with this section. *Waiting on architectural plans.*
17. Sheets 1 and 6 – Please explain how the proposed building architectural features and materials comply with the Zoning Ordinance, Section 6.07.6.E.3. *Waiting on architectural plans.*
18. Sheets 1 and 6 – Pursuant to the Zoning Ordinance, Section 6.07.6.E.4.a, the proposed building design should avoid blank walls at ground-floor levels through the use of windows, trellises, material changes, arcades, or other features to increase visual interest. The applicant should be prepared to discuss how the proposed building complies with the section. *Waiting on architectural plans.*
19. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.5.a, parking lots should be located to the rear or side of a building whenever possible. The proposed parking area is located along the front the property. The applicant should be prepared to discuss the alternatives considered.

Site Walk on 9/6/22

The Planning Board conducted a site walk on 9/6/22 with the applicant and their engineering team. The following issues and potential solutions were discussed:

1. DOT ROW – The NH Department of Transportation will not approve a loading zone for the property in the existing DOT ROW. Therefore, the applicant needs to present an internal circulation plan for the site. This plan shall indicate the largest size vehicle anticipated and the maneuverability of said vehicle through the property, as well as loading areas.
2. Access into the property is limited, and currently shown as one entrance from Elm Street with an access to the adjacent property to the east. The applicant needs to explore options to improve access and overall maneuverability and internal circulation through the site.
3. Telephone Pole with Associated Lighting – The existing telephone pole with associated lighting to both the proposed site and property to the east could be relocated with new lighting installed. Said lighting could be designed and located to be mutually beneficial for both properties. This would create the potential for another access point in this location.
4. Location of Proposed Building – The applicant has expressed the desire to locate the building at the rear of the property to maintain operations in the existing building during construction. However, a different building location, such as parallel to the western property line, would move the building closer to the road, in keeping with the performance measures of the 6.07.6.E.1 Building Orientation - New structures shall orient their main entrance or storefront to a public street. The applicant should be prepared to discuss alternative site layouts and designs that are more in compliance with the West Elm Street Gateway Overlay District.
5. Feasibility of Easement on Adjacent Property – Due to the limited size of the site and the high percentage of the site dedicated to parked cars, the applicant should coordinate with adjacent properties to determine if a mutually beneficial easement could be developed to assist both properties with vehicle loading and unloading.
6. Clean Up – the site needs to be cleaned up including removal of the storage units at the rear of the property.

STAFF RECOMMENDATIONS:

The applicant should be prepared to address the comments raised by the Planning Board, Conservation Commission, Heritage Commission, Town Consultants, Staff, and public pertaining to the Site Plan. The Planning Board will need to determine if the proposed site development meets the purpose and intent of the overlay. Staff recommends continuing the application to the next scheduled Planning Board meeting.

Aerial Photographs of Map 12, Lot 11



Street Photographs of Map 12, Lot 11

Subject Property Looking Southwest



Subject Property Looking South



Subject Property Looking Southeast



October 13, 2022

Camille Pattison, NRPC Planning Consultant
Town of Milford
1 Union Square
Milford, NH 03055

**Re: Sooner Milford Site Plan (Tax Map 12, Lot 11) – 754 and 756 Elm Street
Site Plan Drainage Review**

Dear Ms. Pattison:

We reviewed the revised twelve-sheet plan set and Stormwater Management Report, both dated October 7, 2022, prepared by Bedford Design Consultants, Inc. in accordance with our agreement with the Town. Based on that review, we find that all our previous comments were addressed and the project is now acceptable from an engineering perspective.

If you have any questions or need any additional information, please feel free to contact me at 603-413-6650 or on my cell phone at 603-731-1562 or by email at MVignale@kvpllc.com.

Sincerely,

KV Partners LLC



Michael S. Vignale, P.E.
Principal Engineer

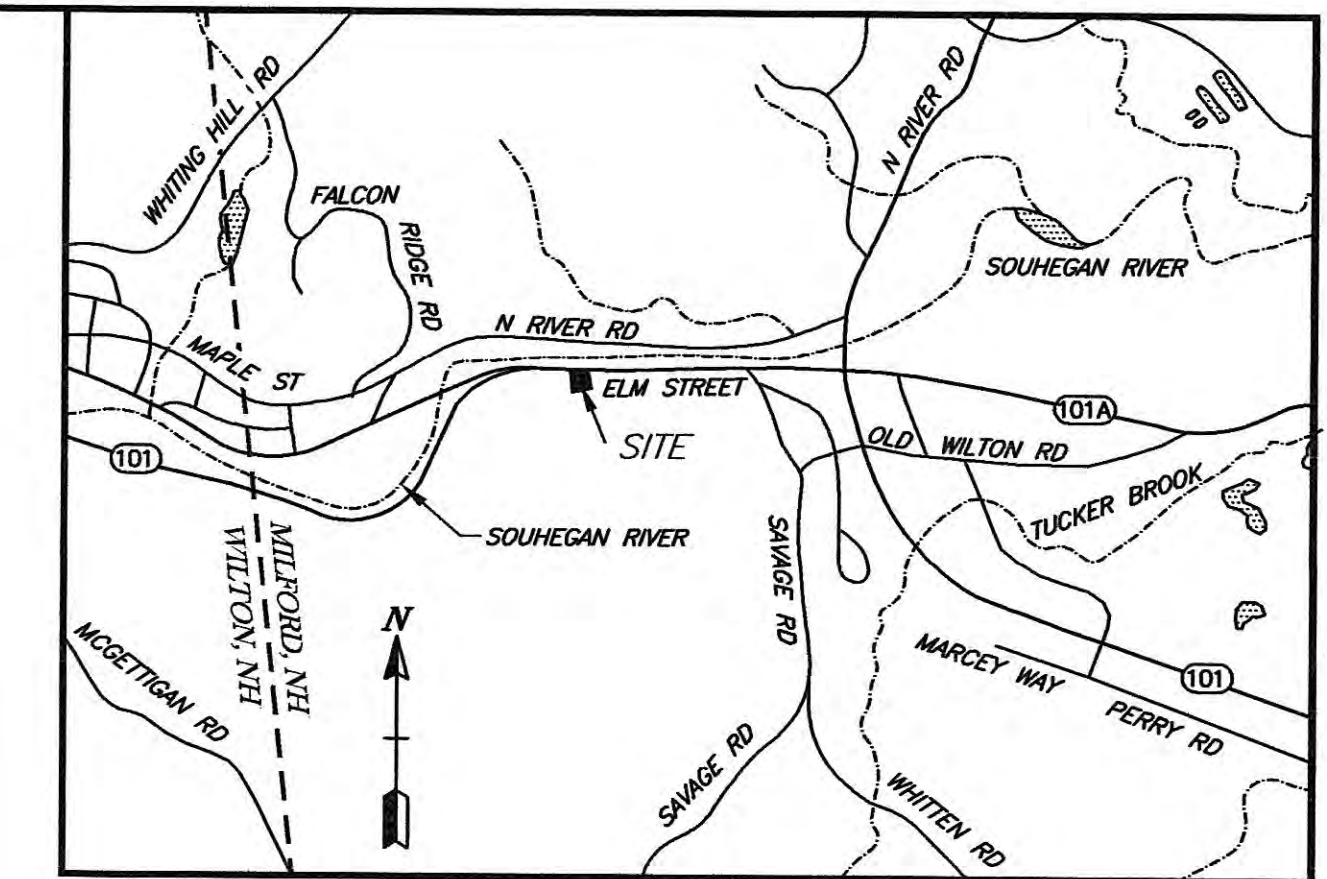
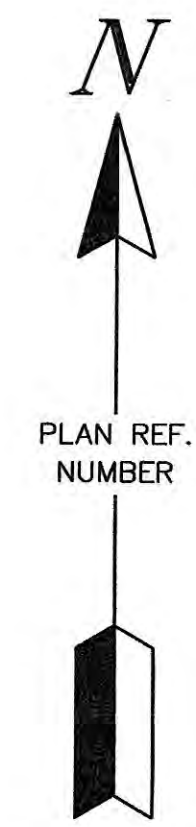


SOONER MILFORD SITE PLAN

754 & 756 ELM ST MILFORD, NEW HAMPSHIRE

TAX MAP 6 LOT 17
RIVER BED TRUST
DAWN J. ARNSTEIN, TRUSTEE
P.O. BOX 95
WILTON, NH 03086-0095
BOOK 5167 PAGE 1545

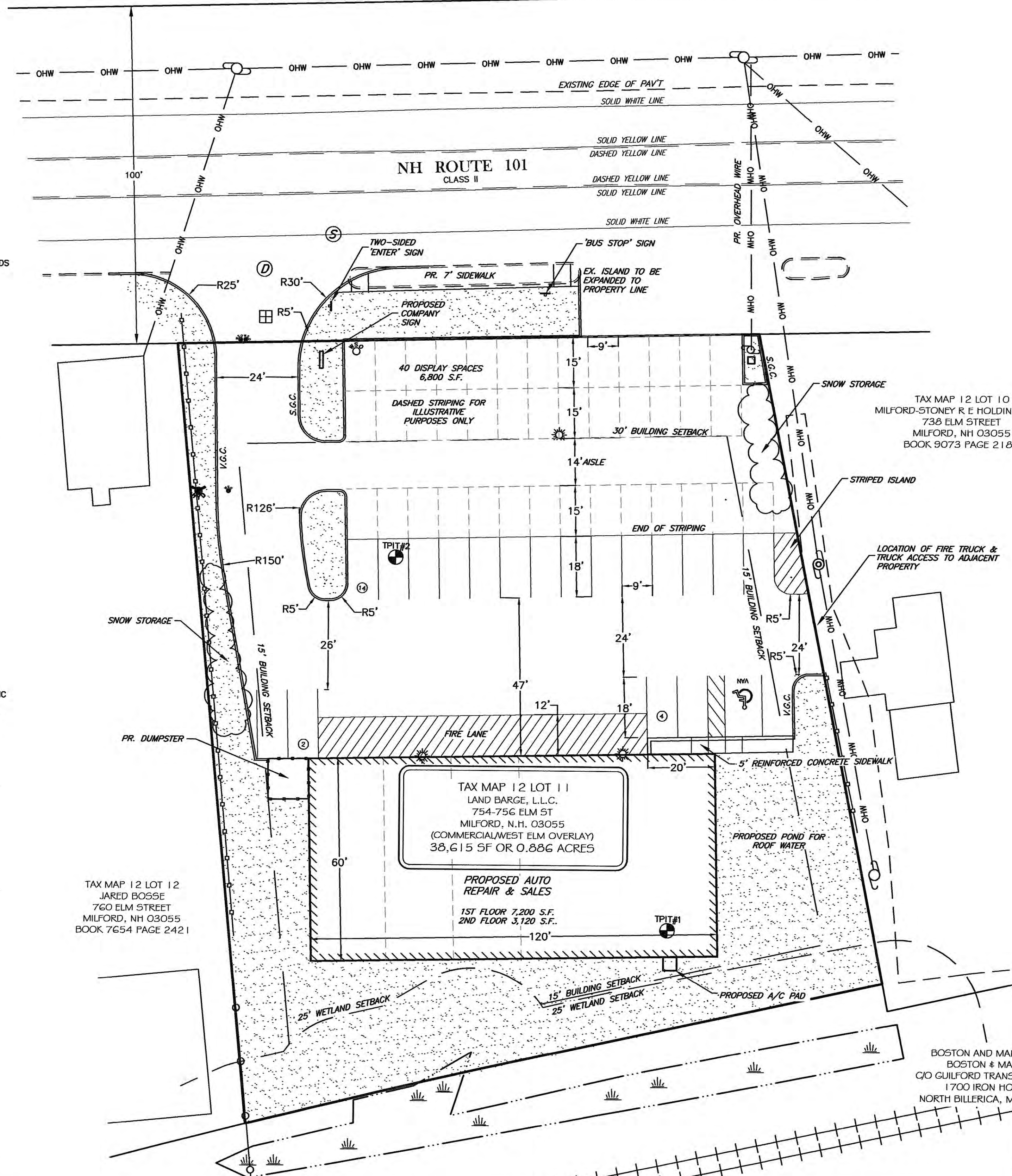
TAX MAP 12 LOT 10-1
STATE OF NEW HAMPSHIRE
P.O. BOX 483
CONCORD, NH 03302-0483
BOOK 1307 PAGE 0182



LOCUS MAP
(1" = 2000')

LEGEND

- [290] PROPOSED CONTOUR
- 290 - EXISTING CONTOUR
- [430.3] PROPOSED SPOT GRADE
- 282.6x EXISTING SPOT GRADE
- LOT LINE
- SETBACK LINE
- EDGE OF JURISDICTIONAL WETLANDS
- WETLAND SYMBOL
- WETLAND BUFFER
- EXISTING ROADWAY
- PROPOSED CURBING
- EXISTING CATCH BASIN
- PROPOSED RIP RAP
- PROPOSED BUILDING
- PROPOSED GRASS
- PROPOSED PERVIOUS PAVERS
- EXISTING GATE VALVE
- PROPOSED GATE VALVE
- PROPOSED LIGHT POLE
- SEWER MANHOLE
- DRAIN MANHOLE
- # OF PARKING SPACES
- UD PROPOSED UNDERDRAIN
- GAS PROPOSED GAS LINE
- GAS EXISTING GAS LINE
- W PROPOSED WATER LINE
- W EXISTING WATER LINE
- UGE PROPOSED UNDERGROUND ELECTRIC
- E EXISTING UNDERGROUND ELECTRIC
- S PROPOSED SEWER LINE
- S EXISTING SEWER LINE
- D PROPOSED DRAIN LINE
- D EXISTING DRAIN LINE
- EXISTING UTILITY POLE
- EXISTING GUY WIRE
- REBAR FOUND
- STONE BOUND FOUND
- REBAR TO BE SET
- GRANITE BOUND TO BE SET
- IRON PIPE FOUND
- BENCHMARK SET
- WELL
- MAILBOX
- STREET ADDRESS
- SCS SOIL LINE
- EXISTING WATER SHUTOFF
- PROPOSED WATER SHUTOFF
- TEST PIT
- ABUTTING PROPERTY LINE
- OHW EX. OVERHEAD WIRES
- OHW PR. OVERHEAD WIRES

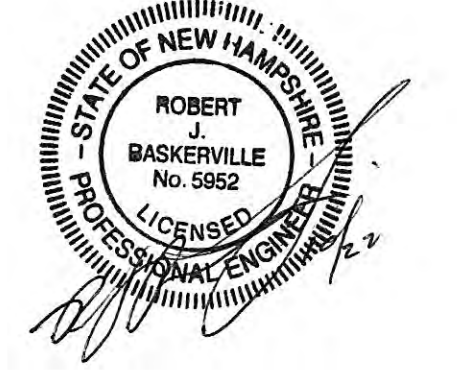


NOTES:

- OWNER OF RECORD:
TAX MAP 12 LOT 11
LAND BARGE, LLC
754-756 ELM ST
MILFORD, NH 03055
BOOK: 8664 PAGE: 2557
- THE PURPOSE OF THIS PLAN IS TO SHOW A PROPOSED AUTO REPAIR AND AUTO SALES SHOP WITH ACCOMPANYING PARKING AND CIRCULATION AND OUTDOOR DISPLAY.
- THE EXISTING LOT IS AN AUTO REPAIR AND SALES SHOP WITH ASSOCIATED PARKING CIRCULATION AND STORAGE. THE FRONT 1/3 OF THE PROPERTY ARE PAVED. BEHIND THE BACK FENCE IS GRASS. THE MAJORITY OF IMPROVEMENTS ARE TO BE REMOVED. SEE THE DEMOLITION PLAN.
- EXISTING DRAINAGE INCLUDES A CATCH BASIN ON ROUTE 101 AND A WETLAND SWALE AT THE BACK OF THE PROPERTY. THE PROPOSED DRAINAGE INCLUDES AN ABOVE GROUND STORMWATER BASIN AND PERMEABLE PAVEMENTS AND THE EXISTING DRAINAGE STRUCTURES. THE SITE IS FLAT AND ALL WATER EVENTUALLY REACHES ROUTE 101. THE SWALE AT THE BACK OF PROPERTY FLOWS ONTO THE ADJACENT PROPERTY AND TO ROUTE 101.
- TOTAL PARCEL AREA = 38,615 SQ. FT. OR 0.886 ACRES.
- ZONED: COMMERCIAL & WEST ELM OVERLAY DISTRICT. THE PARCEL ABUTS THE INDUSTRIAL ZONE. IT IS IN THE GROUNDWATER PROTECTION DISTRICT AND THE SHORELAND PROTECTION ZONE.
- THE SITE IS LOCATED OUTSIDE OF THE 100 YEAR FLOOD BOUNDARY AS SHOWN ON FLOOD INSURANCE RATE MAP, HILLSBOROUGH COUNTY, MAP NUMBER 33011C0454D, EFFECTIVE DATE SEPTEMBER 25, 2009.
- THE SITE IS LOCATED IN THE LEVEL II PROTECTION AREA AS SHOWN ON THE MAP TITLED 'GROUNDWATER PROTECTION AREA: MILFORD, NH' PREPARED BY NASHUA REGIONAL PLANNING COMMISSION, PRINTED 10/24/02.
- DIMENSIONAL REGULATIONS: COMMERCIAL

REQ.	EX.	PR.
MIN. LOT SIZE	20,000 S.F.	38,615 S.F.
MIN. FRONTAGE	150'	172'
FRONT SETBACK	30'	30'
SIDE/REAR SETBACK	15'	15'
OPEN SPACE	30%	70%
WETLAND NON-DIST.	25'	25'
- PARKING:
9'x18' SPACES WITH 24' ACCESS AISLES.
AUTO REPAIR/AUTO DEALER:
4 SPACES PER BAY = 4 X 7 = 28 SPACES
OR
1 SPACE/EMPLOYEE + 1 SPACES PER 1,000 S.F.
= 10 EMPLOYEES + 10.8 PER S.F.
= 21 SPACES REQUIRED
= 20 SPACES PROPOSED
- THIS PROJECT REQUIRES THE FOLLOWING PERMITS:
NHDES SHORELAND PERMIT
NHDES SEWER DISCHARGE PERMIT
TOWN OF MILFORD STORMWATER PERMIT
NHDOT DRIVEWAY PERMIT
- WATER, SEWER, ROAD (INCLUDING PARKING LOT) AND DRAINAGE WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TOWN OF MILFORD'S WATER UTILITIES DEPARTMENT AND PUBLIC WORKS DEPARTMENT STANDARDS.
- AS-BUILT PLANS SHALL BE DELIVERED TO THE BUILDING DEPARTMENT PRIOR TO A CERTIFICATE OF OCCUPANCY BEING ISSUED.
- THIS PROJECT MAY BE TO THE TOWN OF MILFORD IMPACT FEES.
- WAVERS ARE REQUESTED FOR THE NUMBER OF REQUIRED PARKING SPACES IN SECTION 6.05.4 TABLE OF OFF-STREET PARKING, FROM THE PERIPHERY LANDSCAPE STRIP SECTION 6.08.5(B)(1) & 6.08.7(A)(1), AND FROM THE ACCESSIBLE PARKING LENGTH SECTION 6.05.3(A)(4) & 6.05.5(E), AS WELL AS WAVERS FROM THE ZONING ORDINANCE SECTION 6.07.6.5.A PARKING LOTS SHOULD BE LOCATED ON THE SIDE OR REAR OF BUILDINGS AND SECTION 6.07.6.7.A PROPOSED BUILDINGS SHALL MAINTAIN AN APPROPRIATE STREET EDGE TO ADJACENT STRUCTURES.
- IN THE EVENT THE PROPOSED SNOW REMOVAL AREAS ARE NOT SUFFICIENT, SNOW SHALL BE RELOCATED OFF SITE USING BEST MANAGEMENT PRACTICES. ALL SNOW REMOVAL SHALL BE DONE BY A GREEN SNOW PRO CERTIFIED COMPANY.
- THIS SITE PLAN REQUIRES AN AUTOMATIC FIRE ALARM SYSTEM AS WELL AS AN AUTOMATIC SPRINKLER SYSTEM.
- THE LOADING AREA SHALL BE ALONG THE MAIN ENTRANCE DRIVE.

PLAN INDEX	SHEET NO
OVERVIEW & SITE PLAN SHEET	1
EXISTING CONDITIONS PLAN	2
DEMOLITION & EROSION CONTROL PLAN	3
GRADING, UTILITY PLAN	4
SEWER PROFILE PLAN	5
LIGHTING, & LANDSCAPE PLAN	6
ARCHITECTURAL & TRUCK TURNING PLANS	7
SIGHT DISTANCE PLAN	8
CONSTRUCTION DETAILS	9-12



OWNER'S SIGNATURE
NAME _____ DATE _____

APPROVED BY THE TOWN OF MILFORD PLANNING BOARD
SIGNATURE _____ DATE _____
SIGNATURE _____ DATE _____

TAX MAP 12 LOT 11
OVERVIEW & SITE PLAN
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055
 OWNER: LAND BARGE, LLC
 754-756 ELM STREET
 MILFORD, NH 03055
 APPLICANT: SOONER TRANSPORTATION LLC
 3 EXECUTIVE PARK DRIVE, SUITE 201G
 BEDFORD, NH 03110

SCALE: 1" = 20'	APRIL 25, 2022	SHEET 1 OF 12
DESIGN: KAW	DRAWN: KAW	CHECKED: RJB
		FB: 580
		PG: 60
		1667-01

Bedford Design Consultants Inc.
 ENGINEERS AND SURVEYORS
 592 Harvey Road, Manchester, NH 03103
 Telephone: (603) 622-5533
 www.bedforddesign.com

SCALE: 1" = 20'
GRAPHIC SCALE

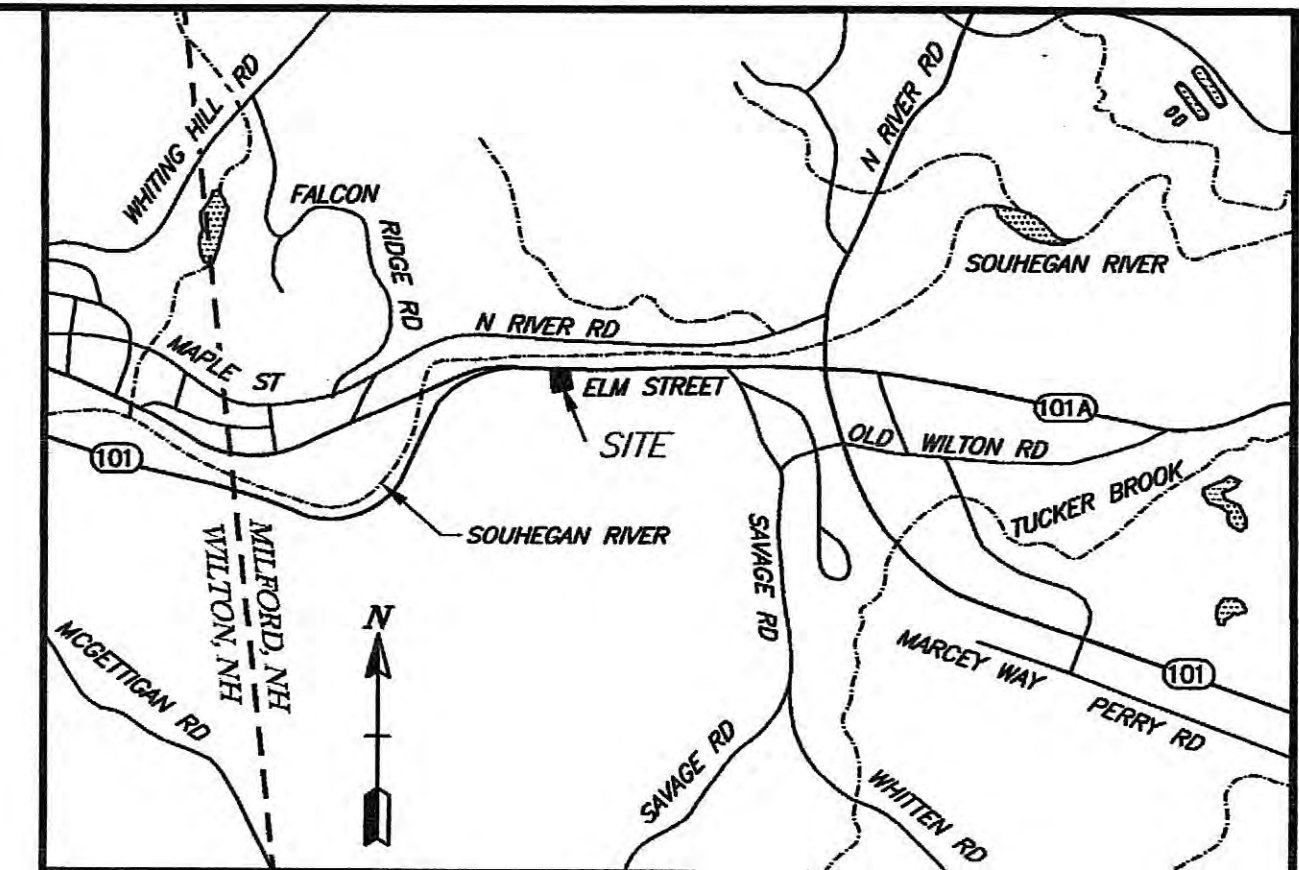
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A
DATE	DESCRIPTION	BY	REV.

PLAN REFERENCES:

- "PLAN OF LAND PETER D. HARDWICK MILFORD NH" DATED APRIL 25, 1978, PREPARED BY THOMAS F. MORAN, INC. HCRD PLAN #11135.
- "BOUNDARY PLAN OF LAND TAX MAP PARCELS 7-33-3, 11-12 & 12-1 HILLSBOROUGH MILLS MILFORD, NH" DATED JUNE 11, 1980, PREPARED BY THOMAS F. MORAN, INC. HCRD PLAN #13775.
- "PLAN OF LAND ELM STREET-ROUTE 101 MILFORD, N.H. SURVEYED FOR HOWARD R. GREENE, JR. DATED MAY 1975, PREPARED BY ROBERT NOLTE AND ASSOCIATES, HCRD PLAN #18469.
- "PLAT OF MARY E. PARKER LAND MILFORD, NH" DATED FEBRUARY 1982, PREPARED BY ROBERT L. TODD, HCRD PLAN #26162.
- "LOT LINE ADJUSTMENT PLAN OF LAND LOTS 11-11 & 12-15 GRANITE BANK MILFORD, NEW HAMPSHIRE" DATED APRIL 26, 1999, LAST REVISED MAY 14TH, 1999, PREPARED BY MANADNOCK SURVEY, INC. HCRD PLAN #29874.
- "RIGHT-OF-WAY AND TRACK MAP, WILTON R.R. CO., OPERATED BY THE BOSTON AND MAINE R.R. STATION 733+40 TO STATION 786+20". SCALE: 1"=100'-FT., DATED JUNE 30, 1914, PREPARED BY THE OFFICE OF VALUATION ENGINEER IN BOSTON, MA, ON FILE AT NHDOT, BUREAU OF RAIL & TRANSIT.
- "STATE OF NEW HAMPSHIRE, DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS, PLANS OF PROPOSED FEDERAL AID PRIMARY PROJECT NO. F 233(11), NH PROJ. NO. P-2051-A, ON FILE AT NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION.

TAX MAP 6 LOT 17
RIVER BED TRUST
DAWN J. ARNSTEIN, TRUSTEE
P.O. BOX 95
WILTON, NH 03086-0095
BOOK 5167 PAGE 1545

TAX MAP 12 LOT 10-1
STATE OF NEW HAMPSHIRE
P.O. BOX 483
CONCORD, NH 03302-0483
BOOK 1307 PAGE 0182



NOTES: LOCUS MAP
1" = 2000'

- OWNER OF RECORD:
TAX MAP 12 LOT 11
LAND BARGE, LLC
754-756 ELM ST
MILFORD, NH 03055
BOOK: 8664 PAGE: 2557
 - THE INTENT OF THIS PLAN IS TO SHOW THE EXISTING CONDITIONS ON TAX MAP 12 LOT 11.
 - TOTAL PARCEL AREA = 38,615 SQ. FT. OR 0.886 ACRES
 - ZONE: COMMERCIAL (C) & WEST ELM STREET OVERLAY DISTRICT
- | DIMENSIONAL REQUIREMENTS | | REQUIRED |
|--------------------------|--|-----------|
| MINIMUM LOT SIZE | | 20,000 SF |
| MINIMUM FRONTAGE | | 150' |
| FRONT BUILDING SETBACK | | 30' |
| SIDE SETBACK | | 15' |
| REAR SETBACK | | 15' |
| WETLAND BUFFER | | 25' |
- THE SITE IS LOCATED OUTSIDE OF THE 100 YEAR FLOOD BOUNDARY AS SHOWN ON FLOOD INSURANCE RATE MAP, HILLSBOROUGH COUNTY, MAP NUMBER 3301100454D, EFFECTIVE DATE SEPTEMBER 25, 2009.
 - THE SITE IS ENTIRELY WITHIN THE LEVEL II PROTECTION AREA AS SHOWN ON THE MAP TITLED "GROUNDWATER PROTECTION AREA: MILFORD, NH", PREPARED BY NASHUA REGIONAL PLANNING COMMISSION, PRINTED 10/24/02.
 - THE SITE IS IN THE SHORELAND PROTECTION AREA.

SOILS LEGEND

MAP UNIT SYMBOL	MAP UNIT NAME	% SLOPES
HsA	HINCKLEY LOAMY SAND	0-3%
HsC	HINCKLEY LOAMY SAND	8-15%
HsD	HINCKLEY LOAMY SAND	15-35%

WETLAND CERTIFICATION

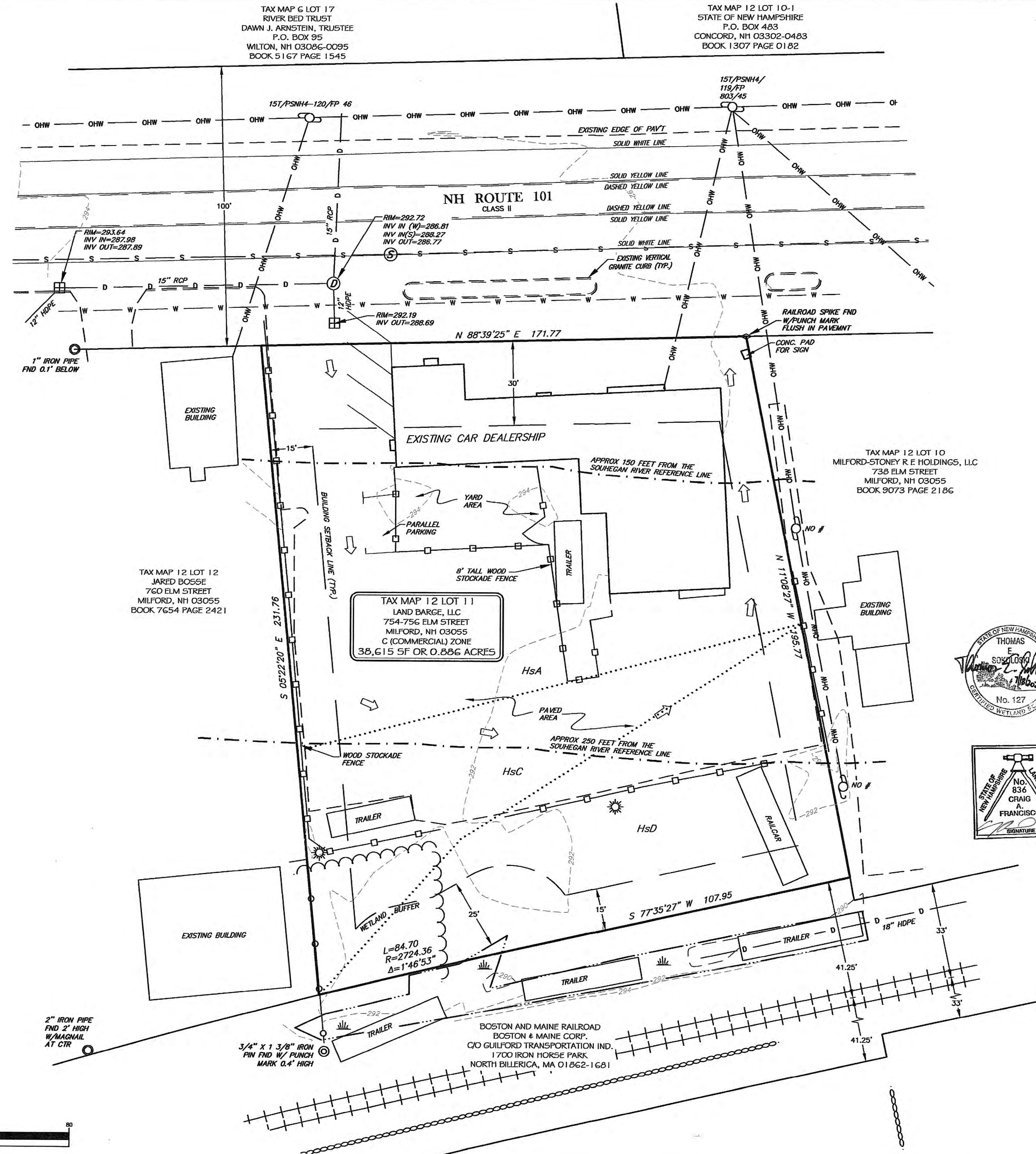
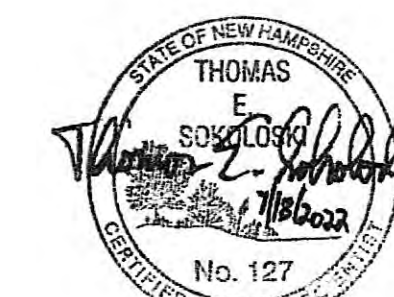
THOMAS SOKOLOSKI, CERTIFIED WETLAND SCIENTIST #127, OF TES ENVIRONMENTAL CONSULTANTS, L.L.C. OF BOW, NH, PERFORMED THE WETLAND IDENTIFICATION AND DELINEATION ON APRIL 15, 2022 ACCORDING TO THE CORPS OF ENGINEERS WETLAND DELINEATION MANUAL AND THE REGIONAL SUPPLEMENT TO THE CORPS OF ENGINEERS WETLAND DELINEATION MANUAL, NORTH-CENTRAL AND NORTHEAST REGION, VERSION 2.0, JANUARY 2012, US ARMY CORPS OF ENGINEERS.

SURVEYOR CERTIFICATION

"I HEREBY CERTIFY THAT THIS MAP AND SURVEY HAS BEEN MADE UNDER MY SUPERVISION. THE DATE OF THE SURVEY WAS 4.22.22."

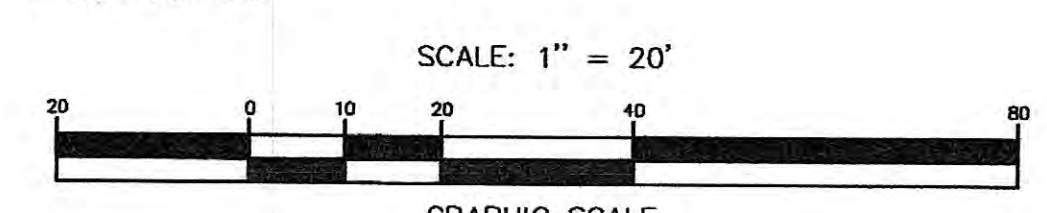
"I HEREBY CERTIFY THAT THIS PLAN IS THE RESULT OF AN ACTUAL FIELD SURVEY MADE ON THE GROUND AND HAS AN ERROR OF CLOSURE OF GREATER ACCURACY THAN ONE PART IN TEN THOUSAND (1:10,000)."

Thomas E. Sokoloski
LICENSED LAND SURVEYOR DATE 4.22.22



LEGEND:

- SIGN
- MAILBOX
- EXISTING HYDRANT
- IRON PIPE FOUND
- ROAD ARROWS
- UTILITY POLE
- EXISTING SEWER MANHOLE
- WATER SHUTOFF
- REBAR FOUND
- RAILROAD SPIKE FOUND
- DRAIN MANHOLE
- WETLAND SYMBOL
- STONE BOUND FOUND
- EXISTING LIGHT POLE
- EXISTING CATCH BASIN
- BOUNDARY LINE
- ABUTTING PROPERTY LINE
- SETBACK LINE
- EXISTING ROAD/DRIVEWAY
- EXISTING VERTICAL GRANITE CURB
- EXISTING CONTOUR
- EXISTING SEWER LINE
- EXISTING WATER LINE
- EXISTING STONE WALL
- EDGE OF JURISDICTIONAL WETLANDS
- WETLAND BUFFER
- DRAIN LINE
- CHAINLINK FENCE
- WOODEN STOCKADE FENCE
- OVERHEAD WIRE
- EXISTING RAILROAD
- SOUHEGAN RIVER REFERENCE LINE
- NRCS WEB SOILS



DATE	DESCRIPTION	BY	REV.

TAX MAP 12 LOT 11
EXISTING CONDITIONS PLAN
LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055

OWNER:
LAND BARGE, LLC
754-756 ELM STREET
MILFORD, NH 03055

APPLICANT:
SOONER TRANSPORTATION LLC
3 EXECUTIVE PARK DRIVE, SUITE 2016
BEDFORD, NH 03110

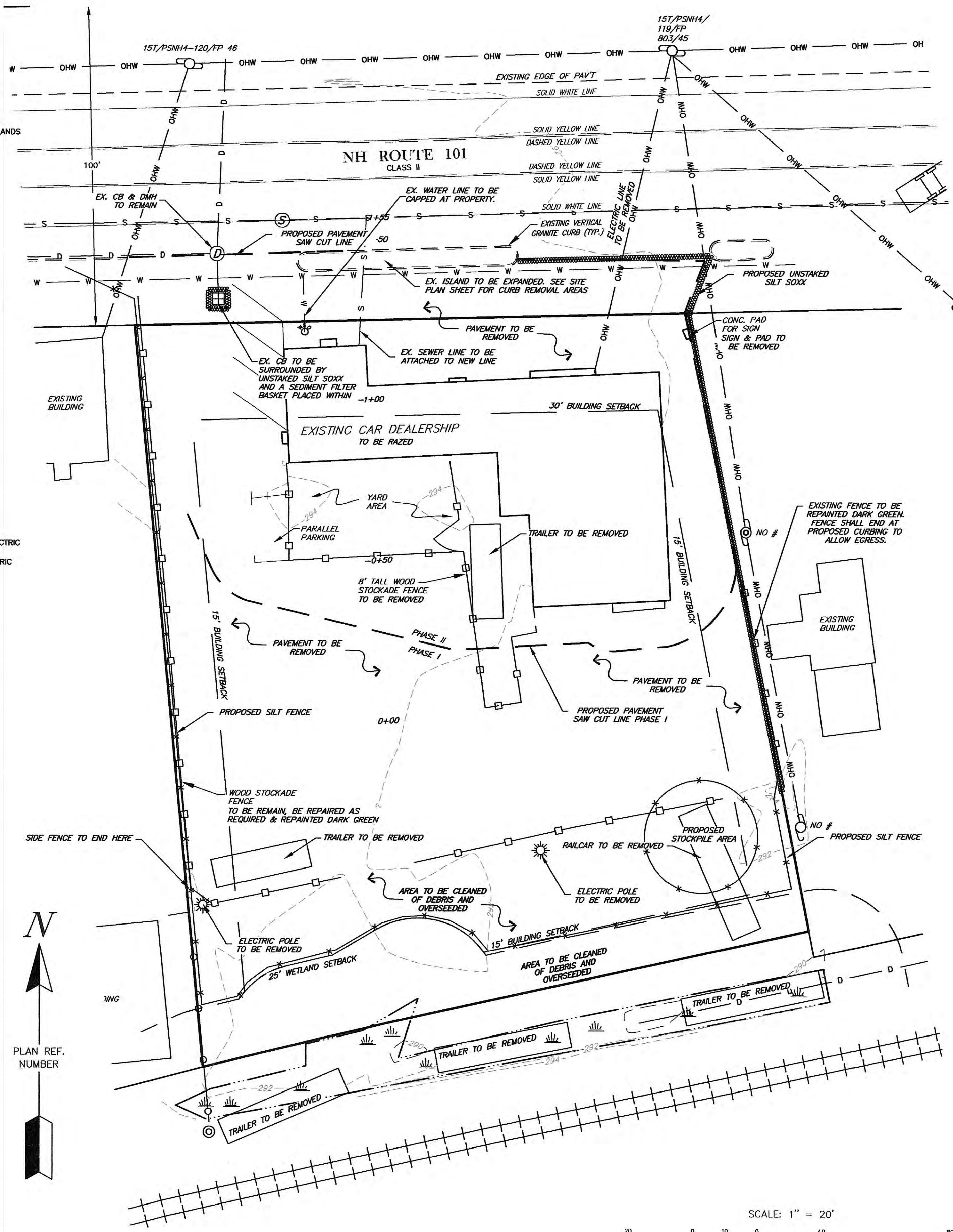
SCALE: 1" = 20' APRIL 25, 2022 SHEET 2 OF 9

DESIGN: C.A.F.	DRAWN: K.M.B.	CHECKED: C.A.F.	FB: 580	PG: 60	1667-01
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Bedford Design Consultants Inc.
ENGINEERS AND SURVEYORS
592 Harvey Road, Manchester, NH 03103
Telephone: (603) 622-5533
www.bedforddesign.com

LEGEND

- 290 — PROPOSED CONTOUR
- - - 290 - - - EXISTING CONTOUR
- X 430.3 PROPOSED SPOT GRADE
- 282.6x EXISTING SPOT GRADE
- LOT LINE
- SETBACK LINE
- - - EDGE OF JURISDICTIONAL WETLANDS
- WETLAND SYMBOL
- WETLAND BUFFER
- - - EXISTING ROADWAY
- PROPOSED CURBING
- EXISTING CATCH BASIN
- PROPOSED RIP RAP
- PROPOSED BRICK
- PROPOSED GRASS
- PROPOSED PERVIOUS PAVERS
- EXISTING GATE VALVE
- PROPOSED GATE VALVE
- PROPOSED LIGHT POLE
- SEWER MANHOLE
- DRAIN MANHOLE
- # OF PARKING SPACES
- UD — PROPOSED UNDERDRAIN
- GAS — PROPOSED GAS LINE
- GAS — EXISTING GAS LINE
- W — PROPOSED WATER LINE
- W — EXISTING WATER LINE
- UGE — PROPOSED UNDERGROUND ELECTRIC
- E — EXISTING UNDERGROUND ELECTRIC
- S — PROPOSED SEWER LINE
- S — EXISTING SEWER LINE
- D — PROPOSED DRAIN LINE
- D — EXISTING DRAIN LINE
- EXISTING UTILITY POLE
- EXISTING GUY WIRE
- REBAR FOUND
- STONE BOUND FOUND
- REBAR TO BE SET
- GRANITE BOUND TO BE SET
- IRON PIPE FOUND
- BENCHMARK SET
- WELL
- MAILBOX
- STREET ADDRESS
- SCS SOIL LINE
- EXISTING WATER SHUTOFF
- PROPOSED WATER SHUTOFF
- TEST PIT
- ABUTTING PROPERTY LINE
- OHW — EX. OVERHEAD WIRES
- OHW — PR. OVERHEAD WIRES

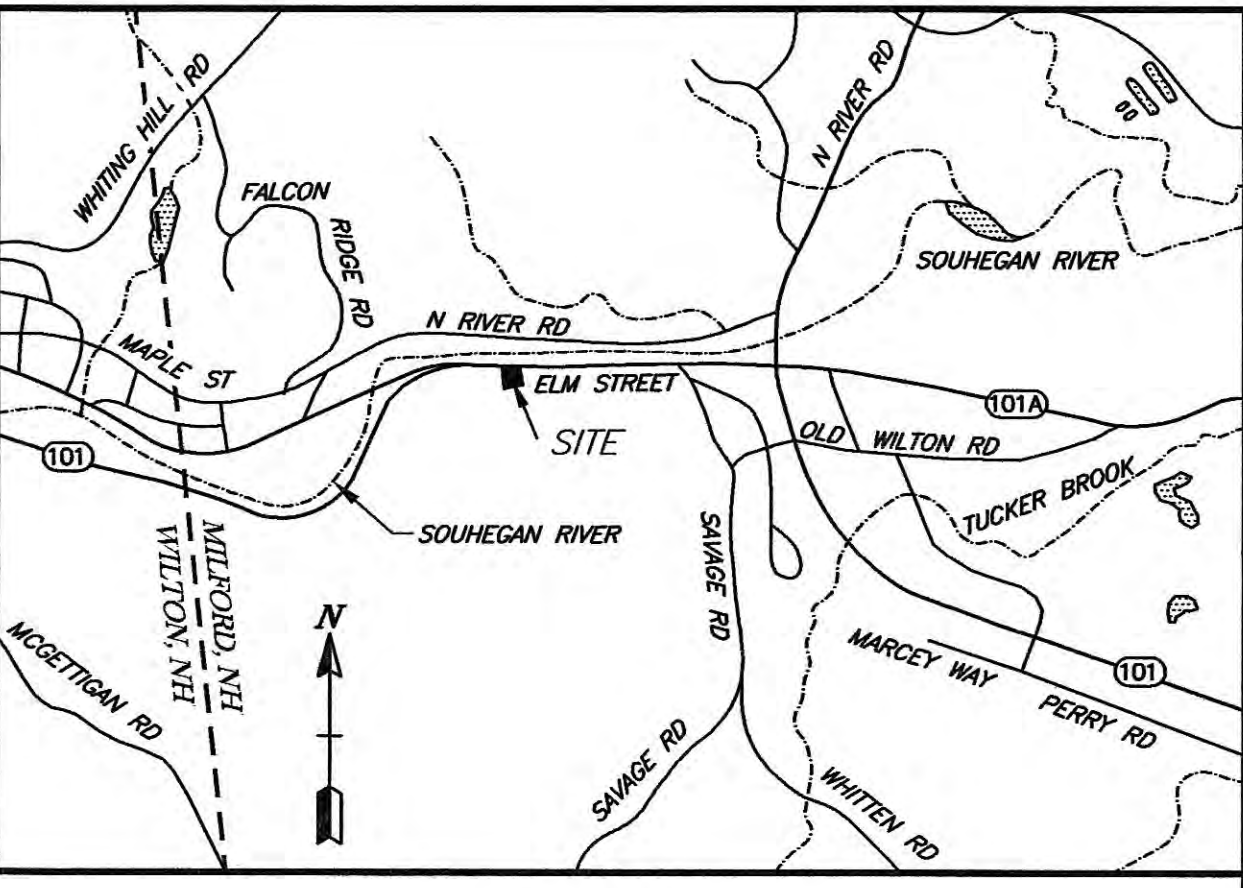


PHASING NOTES:

THE DEMOLITION SHALL BE PHASED. THE AREA BEHIND THE EXISTING BUILDING SHALL BE REMOVED FIRST WITH ROOM TO ALLOW TRAFFIC FLOW BEHIND IT. ONCE THIS AREA HAS BEEN DEMOLISHED, THE NEW BUILDING WILL BE BUILT. ONCE THE NEW BUILDING IS COMPLETED, DEMOLITION OF THE FRONT OF THE SITE WILL OCCUR. EROSION CONTROL MEASURES SHALL BE USED APPROPRIATELY FOR EACH PHASE OF THE DEMOLITION TO ENSURE THAT WATER IS DETAINED BEFORE LEAVING THE SITE.

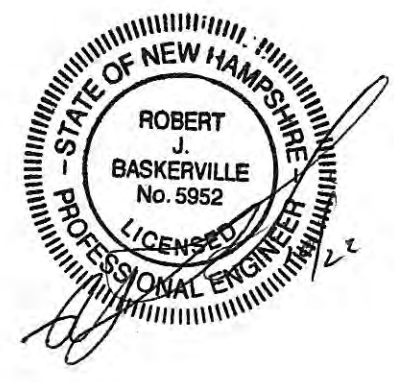
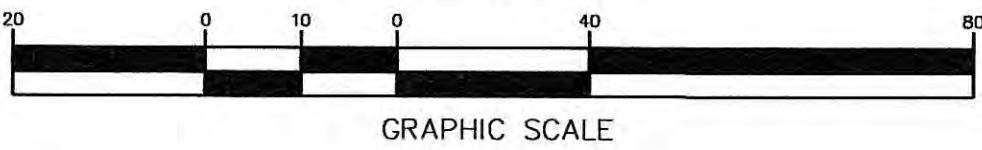
DEMOLITION NOTES:

1. THE PURPOSE OF THIS PLAN IS TO SHOW THE EXISTING IMPROVEMENTS TO BE REMOVED.
2. THIS PLAN IS INTENDED TO PROVIDE MINIMUM GUIDELINES FOR SITE DEMOLITION. IT SHOULD BE NOTED THAT ALL MANMADE FEATURES, PAVEMENT, SIGNS, POLES, CURBING, CONCRETE WALKS, UTILITIES, ETC. SHALL BE REMOVED AS NECESSARY TO CONSTRUCT WORK, UNLESS OTHERWISE NOTED "TO REMAIN".
3. THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE AND THE LOCATIONS ARE NOT GUARANTEED BY THE OWNER OR ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UTILITIES, ANTICIPATE CONFLICTS, REPAIR EXISTING UTILITIES AND RELOCATE EXISTING UTILITIES REQUIRED TO COMPLETE THE WORK.
4. THROUGHOUT THE CONSTRUCTION PROCESS, THE CONTRACTOR SHALL INFORM THE ENGINEER IMMEDIATELY OF ANY FIELD DISCREPANCIES FROM THE DATA SHOWN ON THESE DESIGN PLANS. THIS INCLUDES ANY UNFORESEEN CONDITIONS, SUBSURFACE OR OTHERWISE FOR EVALUATION AND RECOMMENDATIONS. ANY CONTRADICTION BETWEEN ITEMS OF THIS PLAN SET, OR BETWEEN THE PLANS AND ON-SITE CONDITIONS MUST BE RESOLVED BEFORE RELATED CONSTRUCTION HAS BEEN INITIATED.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND DETERMINING THE LOCATION, SIZE AND ELEVATION OF ALL EXISTING UTILITIES SHOWN OR NOT SHOWN ON THESE PLANS. PRIOR TO THE START OF ANY CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING "DIG SAFE" AT LEAST 72 HOURS BEFORE DIGGING.
6. ALL EXISTING STRUCTURES WITHIN THE CONSTRUCTION AREA, UNLESS OTHERWISE NOTED TO REMAIN, SHALL BE REMOVED. ALL MATERIALS SCHEDULED TO BE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL DISPOSE OF ALL MATERIALS OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS, ORDINANCES AND CODES. ANY BURNING ON-SITE SHALL BE SUBJECT TO LOCAL ORDINANCES.
7. CONTRACTOR TO INSTALL EROSION AND SEDIMENT CONTROL DEVICES AS NECESSARY IN ACCORDANCE WITH STORMWATER BEST MANAGEMENT PRACTICES PRIOR TO BEGINNING DEMOLITION ACTIVITIES.
8. PROVIDE INLET PROTECT BARRIERS AT ALL EXISTING CATCH BASINS WITHIN CONSTRUCTION LIMITS AS WELL AS CATCH BASINS THAT MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INLET PROTECTION BARRIERS SHALL BE MAINTAINED FOR THE DURATION OF THE PROJECT. INLET PROTECTION BARRIERS SHALL BE "HIGH FLOW SILT SACK" BY ACF ENVIRONMENTAL OR APPROVED EQUAL. INSPECT BARRIERS WEEKLY AND AFTER EACH 0.5 INCH OR GREATER RAINFALL. SEDIMENT DEPOSITS TO BE REMOVED AFTER EACH STORM EVENT OR MORE OFTEN IF THE FABRIC BECOMES CLOGGED.
9. CONDUCT ALL DEMOLITION ACTIVITIES IN A MANNER WHICH WILL PREVENT INJURY, DAMAGE TO STRUCTURES, ADJACENT BUILDINGS AND ALL PERSONS. ANY EXISTING WORK OR PROPERTY DAMAGED OR DISRUPTED BY THE CONSTRUCTION/DEMOLITION ACTIVITIES SHALL BE REPLACED OR REPAIRED TO MATCH ORIGINAL CONDITIONS BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
10. THE CONTRACTOR SHALL PHASE DEMOLITION AND CONSTRUCTION AS REQUIRED TO PROVIDE CONTINUOUS SERVICE TO EXISTING BUSINESSES AND HOMES THROUGHOUT THE CONSTRUCTION PERIOD. EXISTING SERVICES INCLUDE, BUT ARE NOT LIMITED TO, ELECTRICAL, COMMUNICATION, FIRE PROTECTION, DOMESTIC WATER AND SEWER SERVICES. TEMPORARY SERVICES, IF REQUIRED, SHALL COMPLY WITH ALL FEDERAL, STATE, LOCAL, AND UTILITY COMPANY STANDARDS.
11. CONDUCT DEMOLITION SERVICES IN SUCH A MANNER AS TO INSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS, AND OTHER ADJACENT FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS, OR OTHER OCCUPIED FACILITIES WITHOUT WRITTEN PERMISSION OF THE OWNERS AND APPLICABLE GOVERNMENTAL AUTHORITIES. PROVIDE ALTERNATIVE ROUTES AROUND CLOSED OR OBSTRUCTED TRAFFIC WAYS IF REQUIRED BY APPLICABLE GOVERNMENTAL REGULATIONS.
12. ACCOMPLISH AND PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE UNAUTHORIZED ENTRY OF PERSONS AT ANY TIME.
13. REMOVE FROM THE DESIGNATED SITE, AT THE EARLIEST POSSIBLE TIME, ALL DEBRIS, RUBBISH, SALVAGEABLE ITEMS, HAZARDOUS AND COMBUSTIBLE MATERIALS. REMOVED MATERIALS MAY NOT BE STORED, SOLD OR BURNED ON-SITE. REMOVAL OF HAZARDOUS OR COMBUSTIBLE MATERIALS SHALL BE ACCORDANCE WITH THE PROCEDURES AS AUTHORIZED BY THE FIRE DEPARTMENT AND/OR OTHER APPROPRIATE REGULATORY AGENCIES AND DEPARTMENTS.
14. MARK FOR POSITION ALL EXISTING UTILITY, DRAINAGE, AND SANITARY SEWER LINES AND PROTECT ALL ACTIVE LINES. CLEARLY IDENTIFY BEFORE THE COMMENCEMENT OF DEMOLITION SERVICES THE REQUIRED INTERRUPTION OF ACTIVE SYSTEMS THAT MAY AFFECT OTHER PARTIES, AND NOTIFY ALL APPLICABLE UTILITY COMPANIES TO INSURE THE CONTINUATION OF SERVICE.
15. UTILITIES SHALL BE TERMINATED AT THE MAIN LINE PER UTILITY COMPANY STANDARDS. THE CONTRACTOR SHALL REMOVE ALL ABANDONED UTILITIES LOCATED WITHIN THE LIMITS OF WORK. CONTRACTOR SHALL VERIFY ORIGIN OF ALL DRAINS AND UTILITIES PRIOR TO REMOVAL/TERMINATION TO DETERMINE IF DRAINS OR UTILITY IS ACTIVE AND SERVICES ANY ON OR OFF-SITE STRUCTURE TO REMAIN. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY SUCH UTILITY FOUND AND SHALL MAINTAIN THESE UTILITIES UNTIL A PERMANENT SOLUTION IS IN PLACE.
16. THE CONTRACTOR SHALL PROTECT ALL PROPERTY MONUMENTATION THROUGHOUT THE DEMOLITION AND CONSTRUCTION OPERATIONS. SHOULD ANY MONUMENTATION BE DISTURBED BY THE CONTRACTOR, HE SHALL EMPLOY A LICENSED LAND SURVEYOR TO REPLACE IT.
17. IN AREAS WHERE CONSTRUCTION IS PROPOSED ADJACENT TO ABUTTING PROPERTIES, THE CONTRACTOR SHALL INSTALL ORANGE CONSTRUCTION FENCING ALONG PROPERTY LINES IN ALL AREAS WHERE SILT FENCING IS NOT REQUIRED.
18. USE WATERING, TEMPORARY ENCLOSURES AND OTHER SUITABLE METHODS, AS NECESSARY TO LIMIT THE AMOUNT OF DUST AND DIRT RISING AND SCATTERING IN THE AIR. CLEAN ADJACENT STRUCTURES AND IMPROVEMENTS OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. RETURN ALL ADJACENT AREAS TO THE CONDITIONS EXISTING PRIOR TO THE START OF WORK.
19. COMPLETELY FILL BELOW GRADE AREAS AND VOIDS RESULTING FROM THE DEMOLITION OF STRUCTURES AND FOUNDATIONS WITH SOIL MATERIALS CONSISTING OF STONE, GRAVEL, AND SAND FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. STONES LARGER THAN 6 INCHES IN DIMENSION MATERIAL FROM BUILDING DEMOLITION MAY NOT BE USED AS FILL. PRIOR TO PLACEMENT OF FILL MATERIALS UNDERTAKE ALL NECESSARY ACTIONS TO INSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROZEN MATERIALS, TRASH AND DEBRIS. PLACE FILL MATERIALS IN LAYERS NOT EXCEEDING 6 INCHES IN LOOSE DEPTH AND COMPACT EACH LAYER AT PLACEMENT TO 95% OPTIMUM DENSITY. GRADE SURFACE TO MEET ADJACENT CONTOURS AND PROVIDE SURFACE DRAINAGE.
20. REMOVE ALL ASPHALT, VEGETATION, TREES, SHRUBS, UTILITIES, SIGNAGE AND STRUCTURES AS NOTED.
21. EXISTING ASPHALT REMOVED FROM THE SITE SHALL BE GROUND AND REUSED AS FILL OR TRUCKED OFF SITE AND DISPOSED OF BY APPROVED METHODS. PAVEMENT SHALL BE SAWCUT AT ALL ENTRANCES AND LIMITS OF REMOVAL.
22. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE CITY OF MANCHESTER AND SHALL BE BUILT IN A WORKMAN LIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION APPROVED AND ADOPTED 2006 ARE HEREBY INCORPORATED BY REFERENCE.
23. THE CONTRACTOR SHALL APPLY FOR AN EXCAVATION PERMIT FOR WORK WITHIN THE CITY RIGHT-OF-WAY OR ON CITY OWNED PROPERTY. CONTACT THE CITY MANCHESTER DEPARTMENT OF PUBLIC WORKS PRIOR TO CONSTRUCTION TO APPLY FOR A PERMIT.
24. THE CONTRACTOR SHALL REQUEST A PRECONSTRUCTION MEETING WITH THE DEPARTMENT OF PUBLIC WORKS AND THE PLANNING DEPARTMENT PRIOR TO THE START OF ANY CONSTRUCTION/DEMOLITION ACTIVITIES ON-SITE TO DISCUSS SITE INSPECTIONS, SCHEDULES, SPECIAL CONDITIONS, ETC.
25. ALL WORK MUST CONFORM TO THE TOWN OF MILFORD STANDARD SPECIFICATIONS AND ANY WORK WITHIN THE CITY RIGHT-OF-WAY REQUIRES AN EXCAVATION PERMIT.



LOCUS MAP
1" = 200'

SCALE: 1" = 20'



TAX MAP 12 LOT 11
DEMOLITION & EROSION CONTROL PLAN
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055
 OWNER: LAND BARGE, LLC
 754-756 ELM STREET
 MILFORD, NH 03055
 APPLICANT: SOONER TRANSPORTATION LLC
 3 EXECUTIVE PARK DRIVE, SUITE 2016
 BEDFORD, NH 03110

SCALE: 1" = 20'	APRIL 25, 2022	SHEET 3 OF 12
DESIGN: KAW	DRAWN: KAW	CHECKED: RJB
		FB: 580
		PG: 60
		1667-01

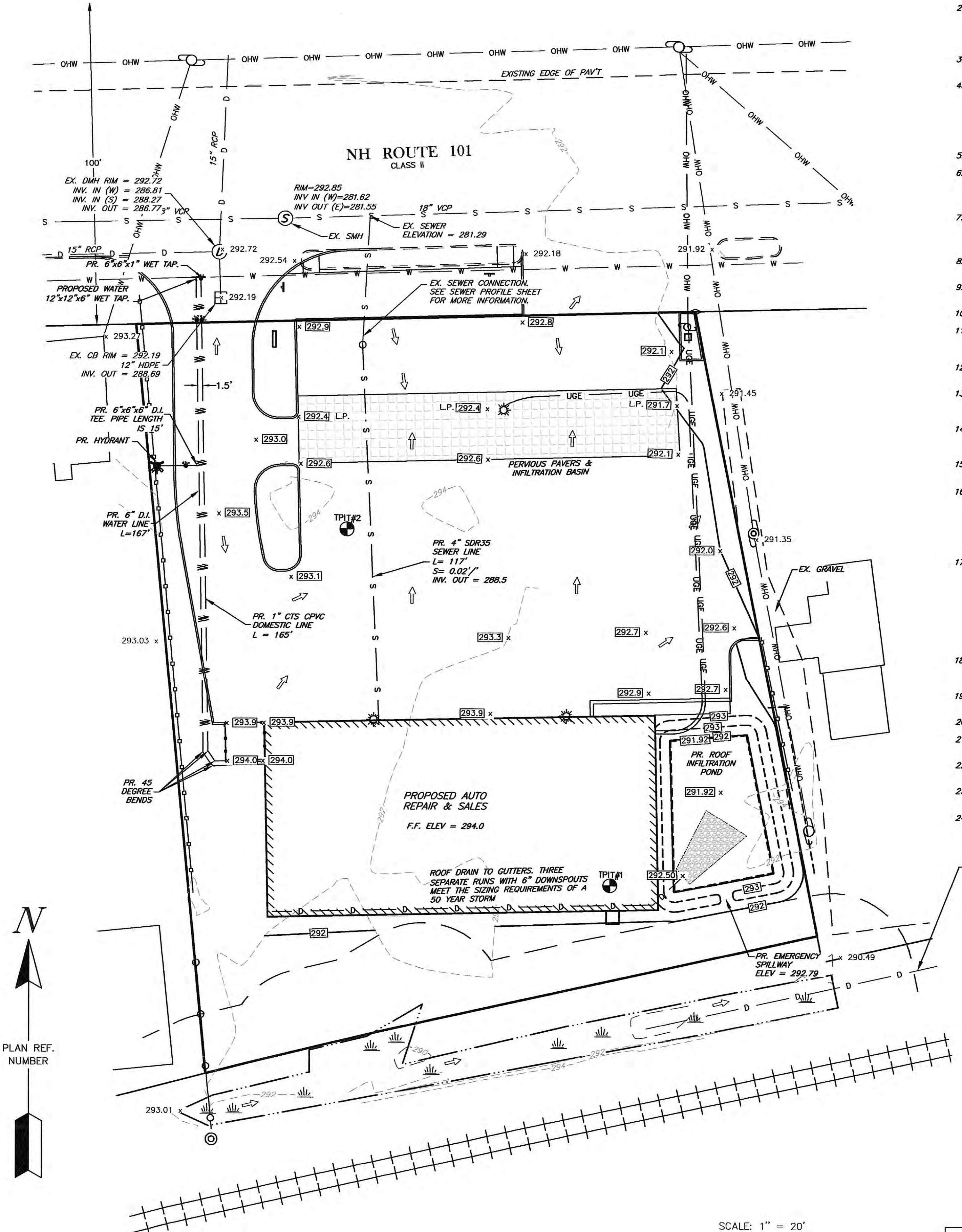
Bedford Design Consultants Inc.
ENGINEERS AND SURVEYORS
 592 Harvey Road, Manchester, NH 03103
 Telephone: (603) 622-5533
 www.bedforddesign.com

DATE	DESCRIPTION	BY	REV.
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A



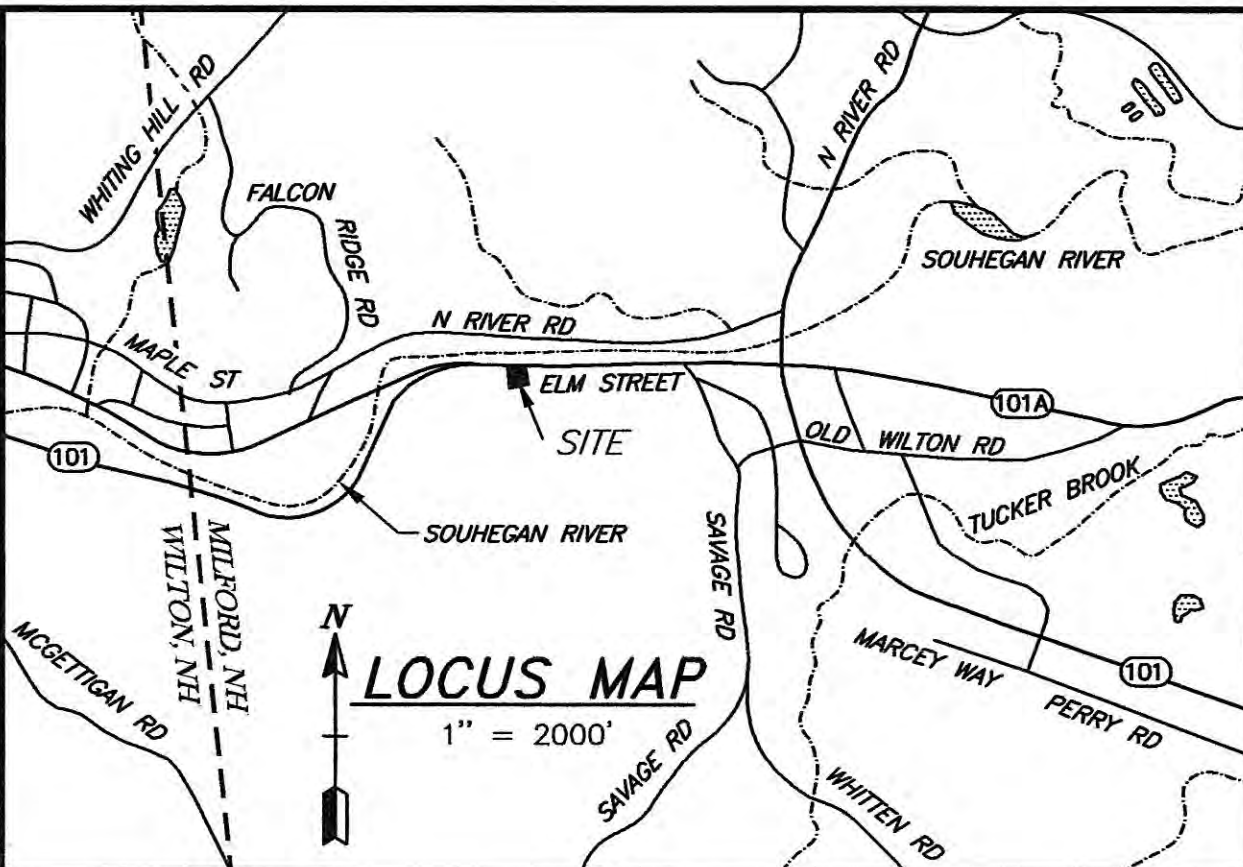
LEGEND

- PROPOSED CONTOUR
- EXISTING CONTOUR
- PROPOSED SPOT GRADE
- EXISTING SPOT GRADE
- LOT LINE
- SETBACK LINE
- EDGE OF JURISDICTIONAL WETLANDS
- WETLAND SYMBOL
- WETLAND BUFFER
- EXISTING ROADWAY
- PROPOSED CURBING
- EXISTING CATCH BASIN
- PROPOSED RIP RAP
- PROPOSED BUILDING
- PROPOSED GRASS
- PROPOSED PERVIOUS PAVERS
- EXISTING GATE VALVE
- PROPOSED GATE VALVE
- PROPOSED LIGHT POLE
- SEWER MANHOLE
- DRAIN MANHOLE
- # OF PARKING SPACES
- PROPOSED UNDERDRAIN
- PROPOSED GAS LINE
- EXISTING GAS LINE
- PROPOSED WATER LINE
- EXISTING WATER LINE
- PROPOSED UNDERGROUND ELECTRIC
- EXISTING UNDERGROUND ELECTRIC
- PROPOSED SEWER LINE
- EXISTING SEWER LINE
- PROPOSED DRAIN LINE
- EXISTING DRAIN LINE
- EXISTING UTILITY POLE
- EXISTING GUY WIRE
- REBAR FOUND
- STONE BOUND FOUND
- REBAR TO BE SET
- GRANITE BOUND TO BE SET
- IRON PIPE FOUND
- BENCHMARK SET
- WELL
- MAILBOX
- STREET ADDRESS
- SCS SOIL LINE
- EXISTING WATER SHUTOFF
- PROPOSED WATER SHUTOFF
- TEST PIT
- ABUTTING PROPERTY LINE
- EX. OVERHEAD WIRES
- PR. OVERHEAD WIRES



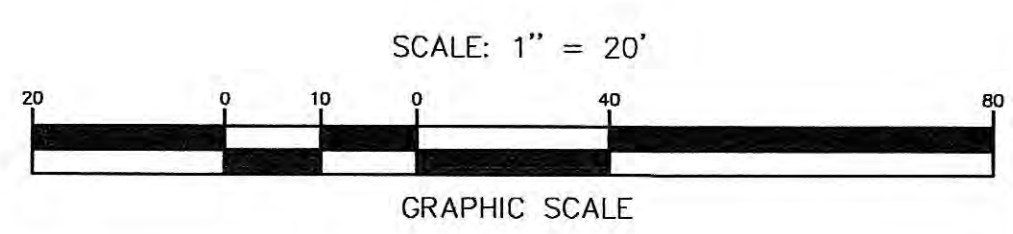
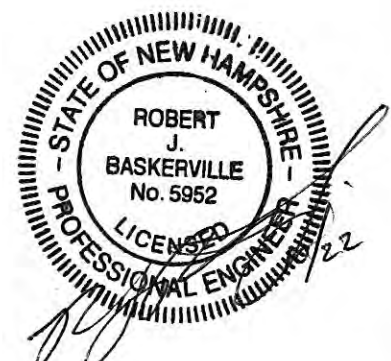
GRADING AND DRAINAGE NOTES:

1. THE PURPOSE OF THIS PLAN IS TO SHOW THE GRADING AND DRAINAGE IMPROVEMENTS ASSOCIATED WITH THE PROPOSED SITE PLAN.
2. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF SALEM AND SHALL BE BUILT IN A WORKMAN LIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION APPROVED AND ADOPTED 2016 ARE HEREBY INCORPORATED BY REFERENCE.
3. ALL DRAINAGE PIPE SHALL BE INSTALLED FOLLOWING MANUFACTURERS INSTALLATION INSTRUCTIONS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND DETERMINING THE LOCATION, SIZE AND ELEVATION OF ALL EXISTING UTILITIES SHOWN OR NOT SHOWN ON THESE PLANS. PRIOR TO THE START OF ANY CONSTRUCTION, THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES FOUND INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROXIMATE REMEDIAL ACTION TAKEN BEFORE PROCEEDING WITH THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTRACTING "DIG SAFE" AT 1-800-344-7233 AT LEAST 72 HOURS BEFORE DIGGING.
5. REFER TO UTILITY SHEETS FOR DRAINAGE RIMS, INVERTS & PIPE SLOPES.
6. ALL DRAINAGE PIPE SHALL BE NON-PERFORATED ADS N-12 HIGH-DENSITY POLYETHYLENE PIPE SMOOTH INTERIOR (OR APPROVED EQUAL), OR RCP CLASS IV, UNLESS NOTED ON THE PLANS. ALL DRAINAGE PIPE SHALL BE INSTALLED FOLLOWING MANUFACTURERS INSTALLATION INSTRUCTIONS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RELOCATION AND/OR PROTECTION OF ALL UTILITIES, EITHER OVERHEAD OR UNDERGROUND, WITHIN THE CONSTRUCTION AREA. THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES OWNING UTILITIES WITHIN THE CONSTRUCTION AREA AND SHALL COORDINATE WITH THE OWNER OF SAID UTILITIES.
8. THE CONTRACTOR SHALL MAINTAIN EMERGENCY ACCESS TO ALL AREAS AFFECTED BY HIS CONSTRUCTION WORK AT ALL TIMES.
9. THE CONTRACTOR SHALL THOROUGHLY SECURE ALL EXCAVATIONS ON A DAILY BASIS AT THE COMPLETION OF CONSTRUCTION OPERATIONS IN THE IMMEDIATE AREA.
10. CONTRACTOR SHALL VERIFY T.B.M. ELEVATION PRIOR TO THE START OF CONSTRUCTION.
11. ALL SWALES AND ANY SLOPES GREATER THAN 3:1 SHALL BE STABILIZED WITH NORTH AMERICAN GREEN S75 EROSION CONTROL BLANKETS (OR AN EQUIVALENT APPROVED IN WRITING BY THE ENGINEER), UNLESS OTHERWISE SPECIFIED.
12. THE CONTRACTOR SHALL STABILIZE ALL DITCHES, PONDS, AND SWALES PRIOR TO DIRECTING RUNOFF TO THEM.
13. ALL PROPOSED AND EXISTING CATCH BASINS WHICH MAY RECEIVE STORMWATER RUNOFF FROM THE DEVELOPMENT DURING CONSTRUCTION SHALL BE OUTFITTED WITH STONE INLET PROTECTION OR SILT SACKS (SEE DETAILS SHEETS).
14. ALL CATCH BASIN SUMPS SHALL BE KEPT CLEAN DURING CONSTRUCTION. ALL CATCH BASIN SUMPS AND PIPING SHALL BE THOROUGHLY CLEANED TO REMOVE ALL SEDIMENT AND DEBRIS AFTER THE PROJECT HAS BEEN PAVED.
15. THE CONTRACTOR SHALL DISPOSE OF ANY UNSUITABLE MATERIAL FOUND ON-SITE (I.E. TRASH, STUMPS, ETC.) IN ACCORDANCE WITH APPLICABLE STATE AND LOCAL REGULATIONS.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN IN THE PLAN SET THROUGHOUT THE DURATION OF THE PROJECT IN ACCORDANCE WITH APPLICABLE N.H.D.E.S. STANDARDS. IF DURING CONSTRUCTION, IT BECOMES APPARENT THAT ADDITIONAL EROSION CONTROL MEASURES ARE REQUIRED TO STOP ANY EROSION ON THE CONSTRUCTION SITE DUE TO ACTUAL SITE CONDITIONS, THE CONTRACTOR SHALL BE REQUIRED TO INSTALL THE NECESSARY EROSION PROTECTION AT NO EXPENSE TO THE CITY/TOWN.
17. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THIS PLAN SET, PREPARED BY BEDFORD DESIGN CONSULTANTS, INC. DOES NOT EXTEND TO OR INCLUDE METHODS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR THEIR EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF THE SURVEYOR AND/OR ENGINEER AS INCLUDED IN THE PLAN SET DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE AND/OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND/OR LOCAL REQUIREMENTS.
18. ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO N.H.D.O.T., LOCAL CITY/TOWN POLICE DEPARTMENT, AND DEPARTMENT OF PUBLIC WORKS SHALL BE EMPLOYED FOR ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY.
19. ADJUST ALL MANHOLES, CATCH BASIN, CURB BOXES, ETC. WITHIN THE LIMITS OF WORK TO FINISH GRADE PRIOR TO INSTALLATION OF FINISHED PAVEMENT.
20. THE SITE SHALL BE GRADED SO ALL FINISHED PAVEMENT HAS POSITIVE DRAINAGE.
21. CONTRACTOR TO PROVIDE FINISHED PAVEMENT SURFACE FREE OF LOW SPOTS AND PONDING AREAS. CRITICAL AREAS INCLUDE BUILDING ENTRANCE, RAMPS, AND LOADING AREAS.
22. ALL ELEVATIONS SHOWN AT THE CURB ARE TO THE BOTTOM OF THE CURB UNLESS OTHERWISE NOTED.
23. ALL SIDEWALK AND OTHER CURB REVEALS SHALL BE 6 INCHES. WHERE SIDEWALK IS TO BE FLUSH, THE PAVEMENT REVEAL SHALL BE 1/4" WITH A TOLERANCE OF 1/8".
24. THE FINISHED GRADE AT THE BOTTOM OF ALL ACCESSIBLE RAMPS SHALL BE FLUSH WITH THE PAVEMENT, WITH A TOLERANCE OF PLUS OR MINUS 1/4".



UTILITY NOTES:

1. THE PURPOSE OF THIS PLAN IS TO SHOW THE UTILITY IMPROVEMENTS ASSOCIATED WITH THE PROPOSED SITE PLAN.
2. ALL WORK SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS OF THE TOWN OF SALEM AND SHALL BE BUILT IN A WORKMAN LIKE MANNER IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION APPROVED AND ADOPTED 2016 ARE HEREBY INCORPORATED BY REFERENCE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND DETERMINING THE LOCATION, SIZE AND ELEVATION OF ALL EXISTING UTILITIES SHOWN OR NOT SHOWN ON THESE PLANS. PRIOR TO THE START OF ANY CONSTRUCTION, THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES FOUND INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROXIMATE REMEDIAL ACTION TAKEN BEFORE PROCEEDING WITH THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTRACTING "DIG SAFE" AT 1-888-344-7233 AT LEAST 72 HOURS BEFORE DIGGING.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RELOCATION AND/OR PROTECTION OF ALL UTILITIES, EITHER OVERHEAD OR UNDERGROUND, WITHIN THE CONSTRUCTION AREA. THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES OWNING UTILITIES WITHIN THE CONSTRUCTION AREA AND SHALL COORDINATE WITH THE OWNER OF SAID UTILITIES.
5. ALL DRAINAGE PIPE SHALL BE NON-PERFORATED ADS N-12 HIGH-DENSITY POLYETHYLENE PIPE SMOOTH INTERIOR (OR APPROVED EQUAL), OR RCP CLASS IV, UNLESS NOTED ON THE PLANS. ALL DRAINAGE PIPE SHALL BE INSTALLED FOLLOWING MANUFACTURERS INSTALLATION INSTRUCTIONS.
6. ALL CATCH BASIN SUMPS AND PIPING SHALL BE THOROUGHLY CLEANED TO REMOVE ALL SEDIMENT AND DEBRIS AFTER THE PROJECT HAS BEEN PAVED.
7. ALL ROOF DRAINS FROM BUILDING SHALL BE CONSTRUCTED TO WITHIN 5' OF THE OUTSIDE THE BUILDING LIMITS AS SHOWN ON THE PLANS AND SHALL BE PROVIDED WITH A TEMPORARY PLUG AND WITNESS AT THE END. ALL EXTERIOR ROOF DOWNSPOUTS ARE TO BE INSTALLED WITH OVERFLOW DEVICES.
8. ALL DRAINAGE AND SANITARY SEWER STRUCTURE INTERIOR DIAMETERS (4" MIN.) SHALL BE DETERMINED BY THE MANUFACTURER BASED ON THE PIPE CONFIGURATIONS SHOWN ON THESE PLANS. CATCH BASINS SHALL HAVE 3" DEEP SUMPS WITH HOODS, UNLESS OTHERWISE NOTED.
9. ALL DRAINAGE STRUCTURES AND STORM SEWER PIPES SHALL MEET HEAVY DUTY H-29 LOADING AND SHALL BE INSTALLED ACCORDINGLY.
10. NO FOUNDATION DRAINS, ROOF DRAINS, FLOOR DRAINS SHALL BE CONNECTED TO THE MUNICIPAL SEWER.
11. OPERATION AND MAINTENANCE OF THE SEWER SERVICE SHALL BE THE RESPONSIBILITY OF THE APPLICANT.
12. THE MINIMUM SEWER TRENCH WIDTH SHALL BE AT LEAST 2 TIMES OUTER DIAMETER OF THE PIPE.
13. THE SEWER CONNECTIONS SHALL BE INSPECTED BY AUTHORIZED TOWN OF SALEM PUBLIC WORKS AND ENGINEERING DEPARTMENT PERSONNEL BEFORE ANY CONSTRUCTION BEGINS BETWEEN 9 A.M. AND 4:30 P.M., MONDAY - FRIDAY, WITH AT LEAST 2 DAYS OF ADVANCED NOTICE.
14. THE SEWER SERVICE MANHOLES SHALL BE VACUUM TESTED AND THE PVC PIPING PRESSURE TESTED WITH AUTHORIZED TOWN OF SALEM PUBLIC WORKS AND/OR ENGINEERING DEPARTMENT PERSONNEL PRESENT ON SITE AS A WITNESS.
15. SEWER MAINS TO BE 8" SDR 35 PVC MEETING ASTM D3034 OR APPROVED EQUIVALENT. ALL SEWER CONSTRUCTION SHALL CONFORM TO THE STATE OF NEW HAMPSHIRE STANDARD SEWER CONSTRUCTION SPECIFICATIONS AND THE TOWN OF SALEM DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS.
16. A MANDATORY PRECONSTRUCTION MEETING SHALL BE HELD WITH THE TOWN, CONTRACTOR, OWNER, AND ALL UTILITY REPRESENTATIVES PRIOR TO CONSTRUCTION. NO WORK SHALL BEGIN UNTIL APPROVAL BY THE DEPARTMENT OF PUBLIC WORKS HAS BEEN OBTAINED.
17. WATER MAINS TO BE 8" CLASS 52 CEMENT LINED DUCTILE IRON PIPE OR APPROVED EQUIVALENT. WATERLINE CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE TOWN OF SALEM DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS.
18. GAS LINE CONSTRUCTION SHALL BE IN ACCORDANCE WITH UNTIL UTILITIES SPECIFICATIONS.
19. ANY FIELD UTILITY ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE OWNER PRIOR TO INSTALLATION.
20. ALL ELECTRIC, TELEPHONE AND CABLE TV LINES ARE TO BE INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY COMPANY SPECIFICATIONS.
21. ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY THE SPECIFIC UTILITY COMPANY AND THE LOCAL DEPARTMENT OF PUBLIC WORKS.
22. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL PROPOSED BUILDING UTILITY CONNECTIONS WITH THE ARCHITECTURAL PLANS.



TAX MAP 12 LOT 11

GRADING & UTILITIES PLAN
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055

OWNER: LAND BARGE, LLC APPLICANT: SOONER TRANSPORTATION LLC
 754-756 ELM STREET 3 EXECUTIVE PARK DRIVE, SUITE 2016
 MILFORD, NH 03055 BEDFORD, NH 03110

SCALE: 1" = 20' APRIL 25, 2022 SHEET 4 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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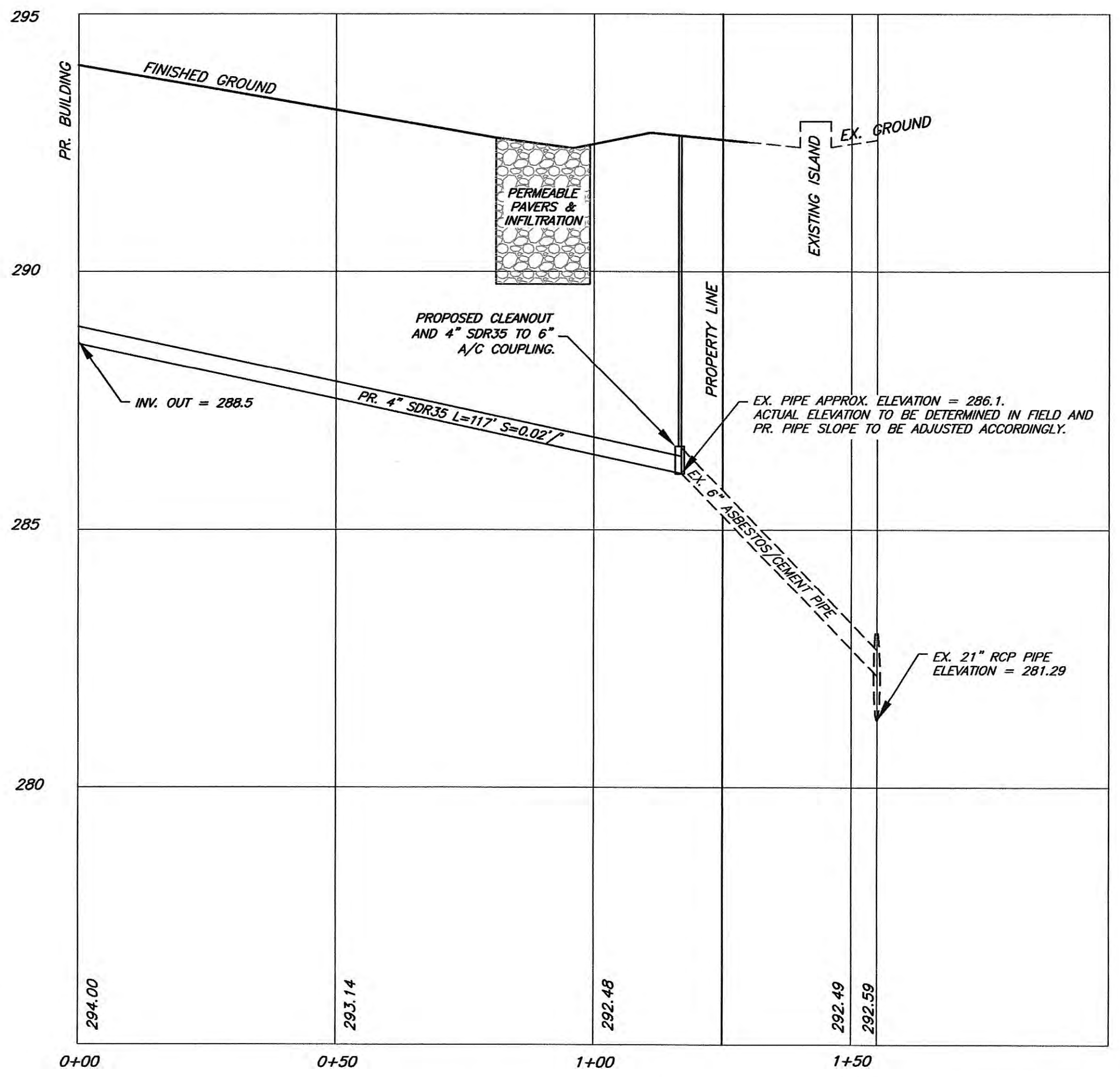
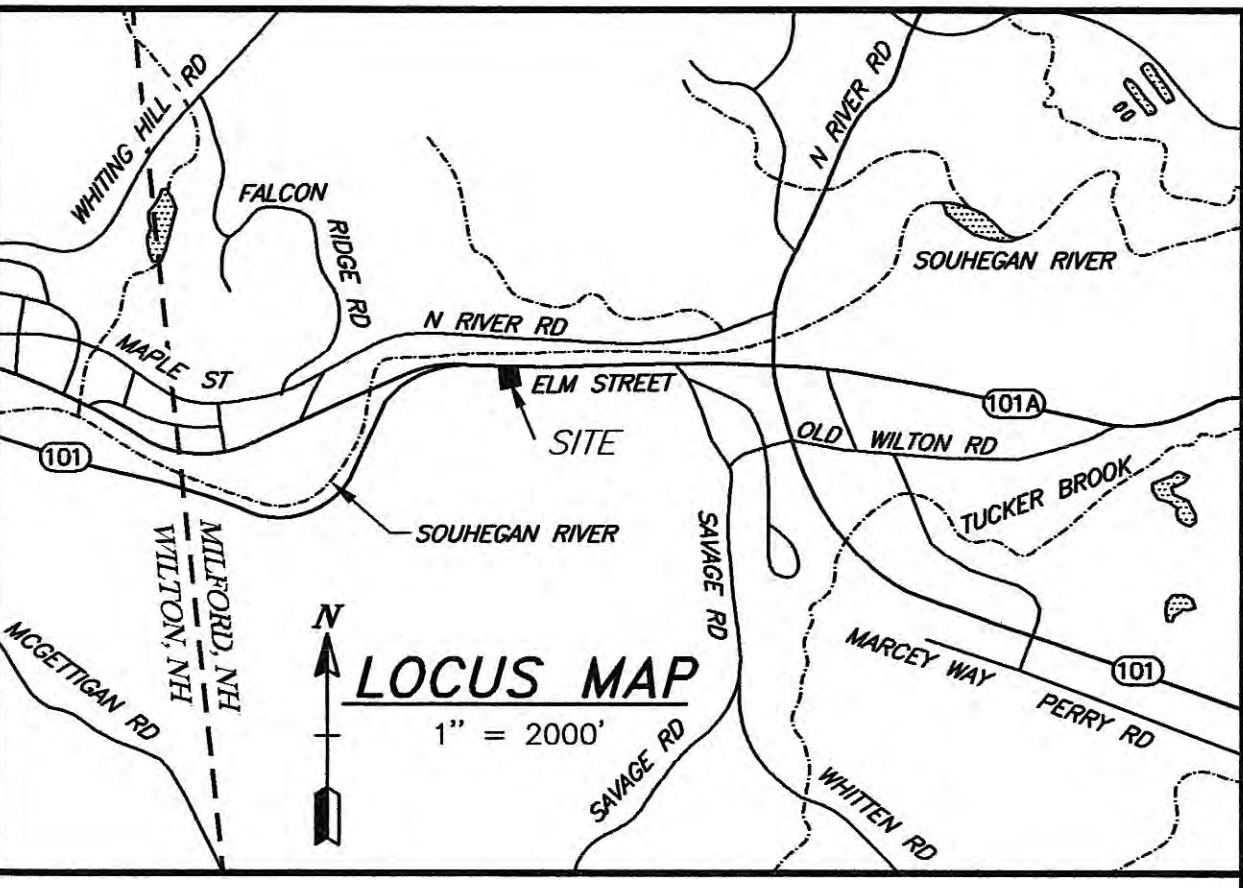
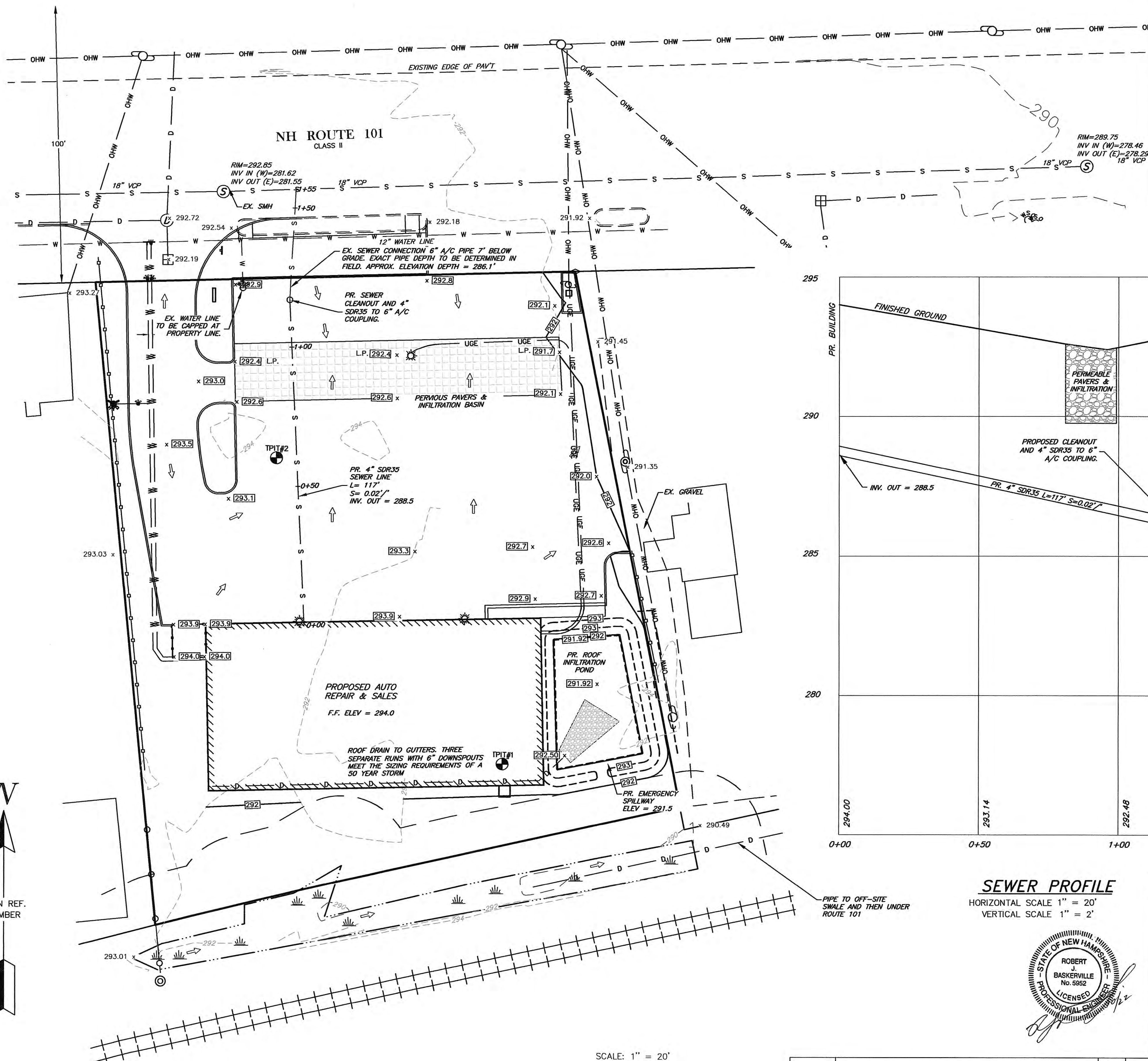
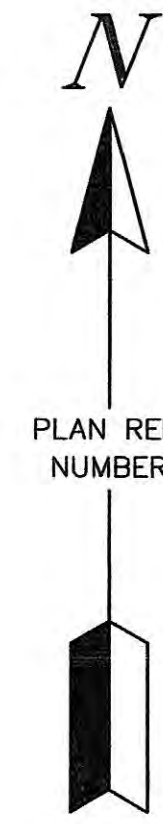
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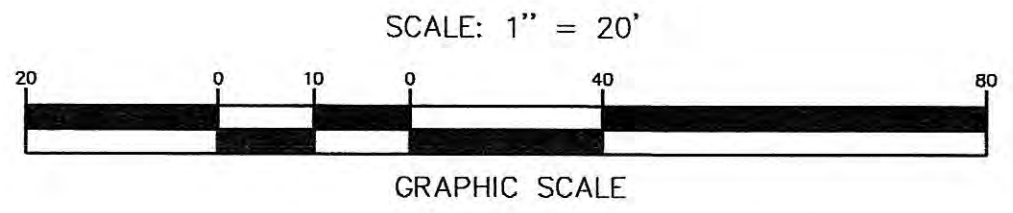
DATE	DESCRIPTION	BY	REV.
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A

LEGEND

- PROPOSED CONTOUR
- EXISTING CONTOUR
- PROPOSED SPOT GRADE
- EXISTING SPOT GRADE
- LOT LINE
- SETBACK LINE
- EDGE OF JURISDICTIONAL WETLANDS
- WETLAND SYMBOL
- WETLAND BUFFER
- EXISTING ROADWAY
- PROPOSED CURBING
- EXISTING CATCH BASIN
- PROPOSED RIP RAP
- PROPOSED BUILDING
- PROPOSED GRASS
- PROPOSED PERVIOUS PAVERS
- EXISTING GATE VALVE
- PROPOSED GATE VALVE
- PROPOSED LIGHT POLE
- SEWER MANHOLE
- DRAIN MANHOLE
- # OF PARKING SPACES
- PROPOSED UNDERDRAIN
- PROPOSED GAS LINE
- EXISTING GAS LINE
- PROPOSED WATER LINE
- EXISTING WATER LINE
- PROPOSED UNDERGROUND ELECTRIC
- EXISTING UNDERGROUND ELECTRIC
- PROPOSED SEWER LINE
- EXISTING SEWER LINE
- PROPOSED DRAIN LINE
- EXISTING DRAIN LINE
- EXISTING UTILITY POLE
- EXISTING GUY WIRE
- REBAR FOUND
- STONE BOUND FOUND
- REBAR TO BE SET
- GRANITE BOUND TO BE SET
- IRON PIPE FOUND
- BENCHMARK SET
- WELL
- MAILBOX
- STREET ADDRESS
- SCS SOIL LINE
- EXISTING WATER SHUTOFF
- PROPOSED WATER SHUTOFF
- TEST PIT
- ABUTTING PROPERTY LINE
- OHW - EX. OVERHEAD WIRES
- OHW - PR. OVERHEAD WIRES



SEWER PLAN
1" = 20'



TAX MAP 12 LOT 11

SEWER PLAN AND PROFILE
SOONER-MILFORD
LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055

OWNER: LAND BARGE, LLC
754-756 ELM STREET
MILFORD, NH 03055

APPLICANT: SOONER TRANSPORTATION LLC
3 EXECUTIVE PARK DRIVE, SUITE 2019
BEDFORD, NH 03110

SCALE: 1" = 20' APRIL 25, 2022 SHEET 5 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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- LOT LINE
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- PROPOSED WATER LINE
- EXISTING WATER LINE
- PROPOSED UNDERGROUND ELECTRIC
- EXISTING UNDERGROUND ELECTRIC
- PROPOSED SEWER LINE
- EXISTING SEWER LINE
- PROPOSED DRAIN LINE
- EXISTING DRAIN LINE
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- SCS SOIL LINE
- EXISTING WATER SHUTOFF
- PROPOSED WATER SHUTOFF
- TEST PIT
- ABUTTING PROPERTY LINE
- EX. OVERHEAD WIRES
- PR. OVERHEAD WIRES

LANDSCAPE NOTES:

1. THE PURPOSE OF THIS PLAN IS TO SHOW THE LANDSCAPE ASSOCIATED WITH THE PROPOSED SITE PLAN.
2. CONTRACTOR TO MARK ALL UNDERGROUND UTILITIES ON THE GROUND PRIOR TO CONSTRUCTION.
3. CONTRACTOR IS TO REVIEW TREE LOCATIONS WITH LANDSCAPE ARCHITECT AND OWNERS REPRESENTATIVES PRIOR TO ORDERING AND INSTALLING TREES.
4. FOR PLANTING DETAILS SEE THE PLANTING DETAIL SHEET.
5. IRRIGATION SHALL BE PROVIDED AT THE OWNERS DISCRETION.
6. ALL PLANTS CAN BE FOUND AT MILLICAN NURSERIES.
7. NO SUBSTITUTIONS MAY BE MADE WITHOUT WRITTEN PERMISSION FROM THE LANDSCAPE ARCHITECT.

LANDSCAPE MAINTENANCE & REPLACEMENT:

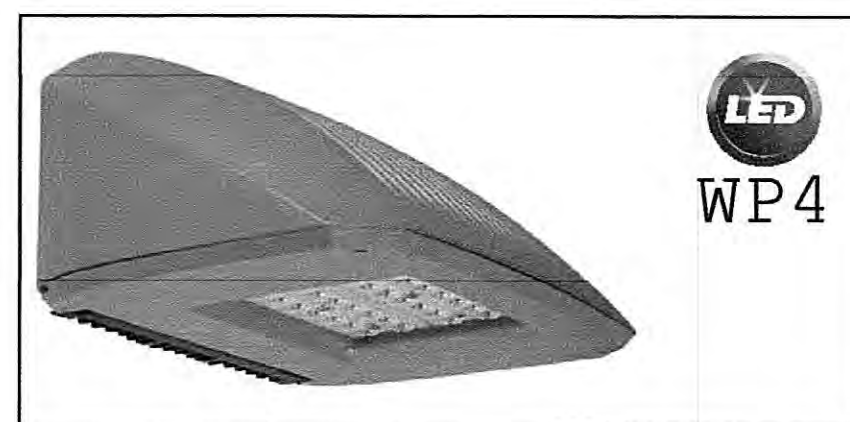
1. PLANT MATERIALS SHALL BE IN ACCORDANCE WITH THE AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.1-2014.
2. INSTALLATION AND MAINTENANCE OF LANDSCAPING SHALL BE IN ACCORDANCE WITH THE NATIONAL ASSOCIATION OF LANDSCAPE PROFESSIONALS INSTALLATION AND MAINTENANCE TRAINING MANUALS.
3. DEAD AND/OR DYING PLANT MATERIALS SHOULD BE REPLACED REGULARLY TO MAINTAIN ORIGINAL DESIGN COUNTS AND LOCATIONS.
4. PLEASE SEE THE TOWN OF MILFORD SITE PLAN REGULATIONS FOR A FULL LIST OF PLANTING REQUIREMENTS.

LANDSCAPE BUFFERS & CALCULATIONS:

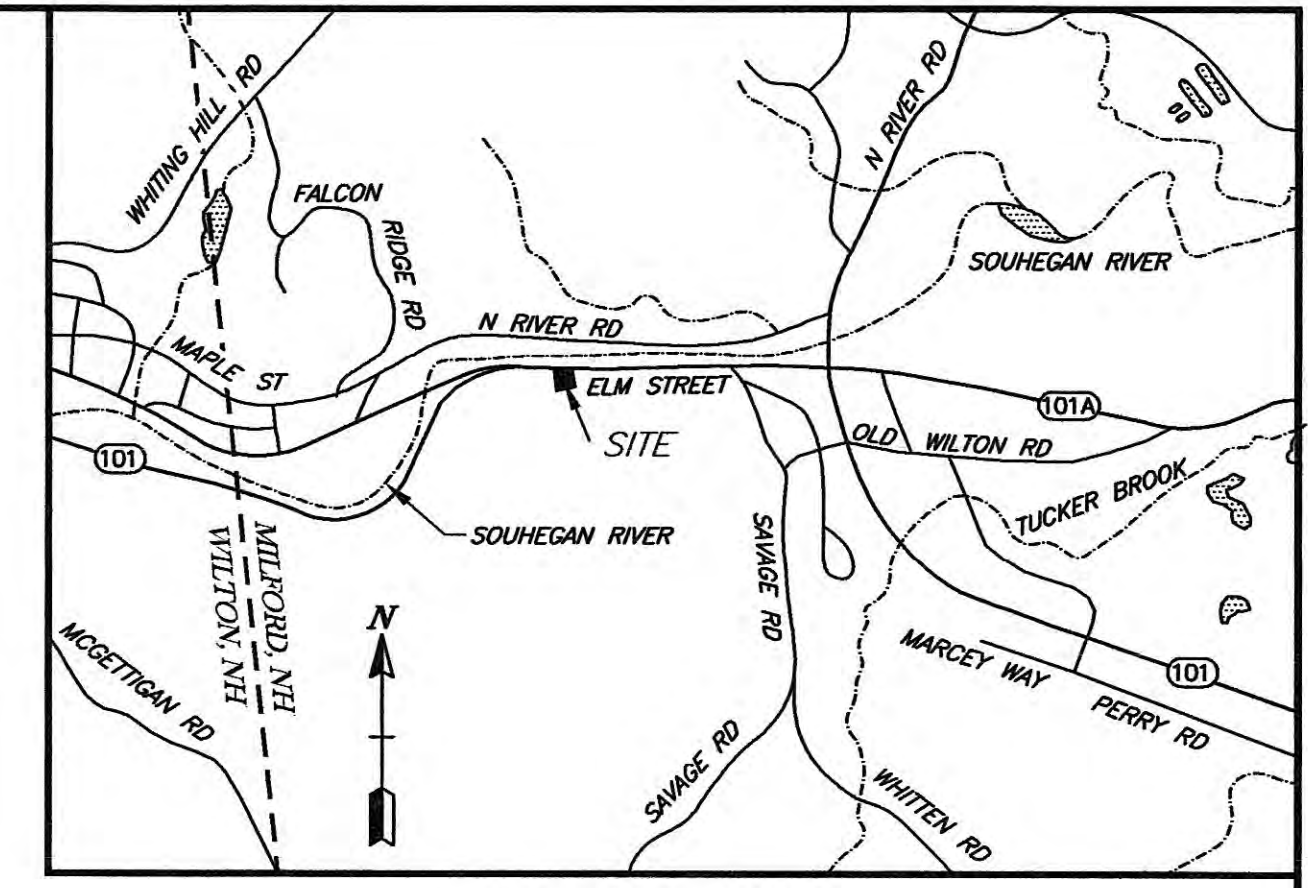
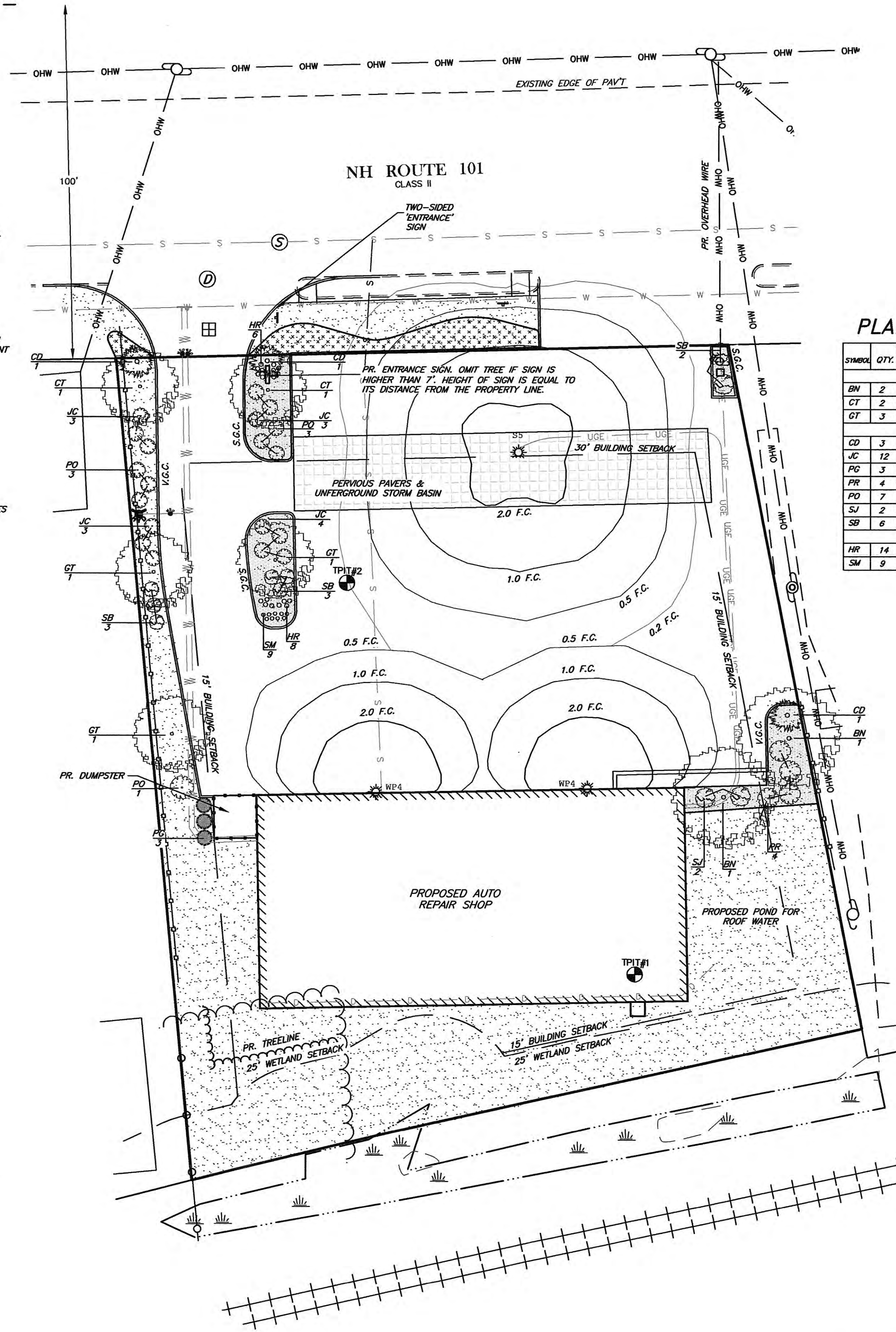
1. ALONG THE PERIPHERY OF THE PROPERTY THERE SHALL BE A 10' LANDSCAPE BUFFER 6' FEET IN HEIGHT TO SCREEN ADJACENT PROPERTIES. WE HAVE REQUESTED A WAIVER FROM THIS REQUIREMENT FOR THE EAST AND NORTH SIDE OF THE PROPERTY.
 - 420' OF PERIPHERY BUFFER STRIP
 - 1 TREE / 30 FEET = 14 TREES REQUIRED
 - = 5 TREES PROPOSED (WAIVER REQUESTED)
2. PERIPHERY LANDSCAPING
 - 1 TREE PER 30' OF LANDSCAPE STRIP WILL BE PROVIDED.
3. LANDSCAPE ALONG BUILDING FRONTS
 - 1 SHRUB FOR EVERY 5' OF BUILDING FRONTAGE
 - 120' BUILDING / 5' = 24 SHRUBS REQUIRED
 - = 24 SHRUBS PROPOSED (LOCATED IN PERIPHERY BUFFER)
4. PARKING LOT LANDSCAPING SHALL BE 5% OF PARKING AREA WITH 1 TREE PER 15 PARKING SPACES REQUIRED.
 - CALCULATIONS: PARKING AREA IS 10,000 S.F.
 - 5% OF PARKING AREA = 500 S.F. REQUIRED
 - = 819 S.F. PROPOSED.
 - 1 TREE / 15 SPACES = 2 TREES REQUIRED
 - = 2 TREES PROPOSED

LIGHTING NOTES

WALL MOUNTED LIGHT HEIGHT IS 15 FEET AND POLE HEIGHT IS 20'. BOTH UNITS ARE I.D.A. DARK SKY COMPLIANT.



Qty	Label	Arrangement	Lumens	Input Watts	LLF	BUG Rating	Description
2	WP4	SINGLE	6076	52	0.850	B1-U0-G2	GARDCO 121-32L-530-WW-G4-4-UNV-FINISH / WALL MOUNTED @ 15FT ARG TO BOF
1	S5	4 @ 90 DEGREES	7284	55.7	0.850	B3-U0-G2	GARDCO ECF-S-32L-530-WW-G2-AR-5W-UNV-FINISH / MOUNTED TO SSS-20-X-X-D4-FINISH



LOCUS MAP
1" = 2000'

PLANT SCHEDULE

SYMBOL	QTY.	LATIN NAME	COMMON NAME	MATURE HEIGHT	PLANT SELECTION SIZE
DECIDUOUS TREES					
BN	2	BETULA NIGRA 'DURA HEAT'	DURA HEAT RIVER BIRCH	40'-60"H x 20'-30"W	2" CALIPER
CT	2	CRATAEGUS CRUGALLI INERMIS 'CRUSADER'	CRUSADER THORNLESS HAWTHORN	10'-15"H x 15'-20"W	2" CALIPER
GT	3	GLEDITSIA T.L. 'STREET KEEPER'	STREET KEEPER HONEYLOCUST	30'-40"H x 15'-20"W	2" CALIPER
DECIDUOUS & EVERGREEN SHRUBS					
CD	3	COTONEASTER DAMMERI 'SALICIFOLIUS 'REPENS'	CREeping WILLOWLEAF COTONEASTER	12"-24"H x 6'-8"W	#2 CONTAINER
JC	12	JUNIPERUS CHINENSIS 'DAUB'S FROSTED'	DAUB'S FROSTED JUNIPER	12"-24"H x 5'-6"W	#3 CONTAINER
PG	3	PICEA GLAUCA 'CONICA'	CONICA SPRUCE	8'-10"H x 4'-5"W	#10 CONTAINER
PR	4	PICEA GLAUCA 'RAINBOW'S END'	RAINBOW'S END SPRUCE	6'-8"H x 3'-4"W	#6 CONTAINER
PD	7	PICEA ORIENTALIS 'BUGMAN'S GEM'	BERGMAN'S GEM ORIENTAL SPRUCE	2'-3"H x 4'-5"W	#3 CONTAINER
SJ	2	SPIRAEA JAPONICA 'SHIBORI'	SHIBORI SPIREA	2'-3"H x 3'-4"W	#3 CONTAINER
SB	6	SYRINGA 'BLOOMERANG DWARF PINK'	BLOOMERANG DARK PINK LILAC	2'-3" H&W	#7 CONTAINER
PERENNIALS					
HR	14	HEMEROCALLIS 'ROSY RETURNS'	ROSY RETURNS DAYLILY	14" H&W	#1 CONTAINER
SM	9	SALVIA 'BLUE MARVEL'	BLUE MARVEL SAGE	12" H&W	#2 CONTAINER

LEGEND

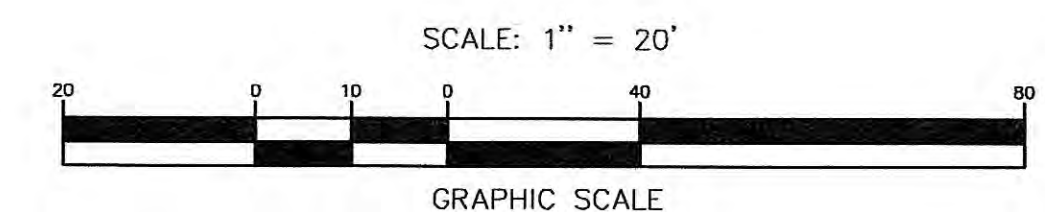
- PR. PERENNIAL GARDEN
- PR. MULCHED AREAS
- PR. GRASSED AREAS



PERENNIAL GARDEN PLANTINGS

QTY.	LATIN NAME	COMMON NAME	MATURE HEIGHT	PLANT SELECTION SIZE
20	HEMEROCALLIS 'ROSY RETURNS'	ROSY RETURNS DAYLILY	14" H&W	#1 CONTAINER
20	SALVIA 'BLUE MARVEL'	BLUE MARVEL SAGE	12" H&W	#2 CONTAINER
20	ECHINACEA 'SUNSEEKERS SALMON'	SUNSEEKERS SALMON CONEFLOWER	18" H&W	#1 CONTAINER
20	MONARDA 'PETITE DELIGHT'	PETITE DELIGHT BEE BALM	18" H&W	#1 CONTAINER
20	SEDUM 'TURQUOISE TAILS'	TURQUOISE TAILS STONECROP	12" H&W	#1 CONTAINER

PERENNIALS ARE TO BE PLANTED IN GROUPS OF 3 TO 5 AND SPACED 12" TO 18" ON CENTER DEPENDING ON THEIR MATURE SIZE IN THE TABLE INDICATED ABOVE.



TAX MAP 12 LOT 11
LIGHTING, & LANDSCAPE PLAN
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055
 OWNER: LAND BARGE, LLC
 754-756 ELM STREET
 MILFORD, NH 03055
 APPLICANT: SOONER TRANSPORTATION LLC
 3 EXECUTIVE PARK DRIVE, SUITE 2016
 BEDFORD, NH 03110

SCALE: 1" = 20' APRIL 25, 2022 SHEET 6 OF 12

DESIGN:	DRAWN:	CHECKED:	FB:	PG:	
KAW	KAW	RJB	580	60	1667-01

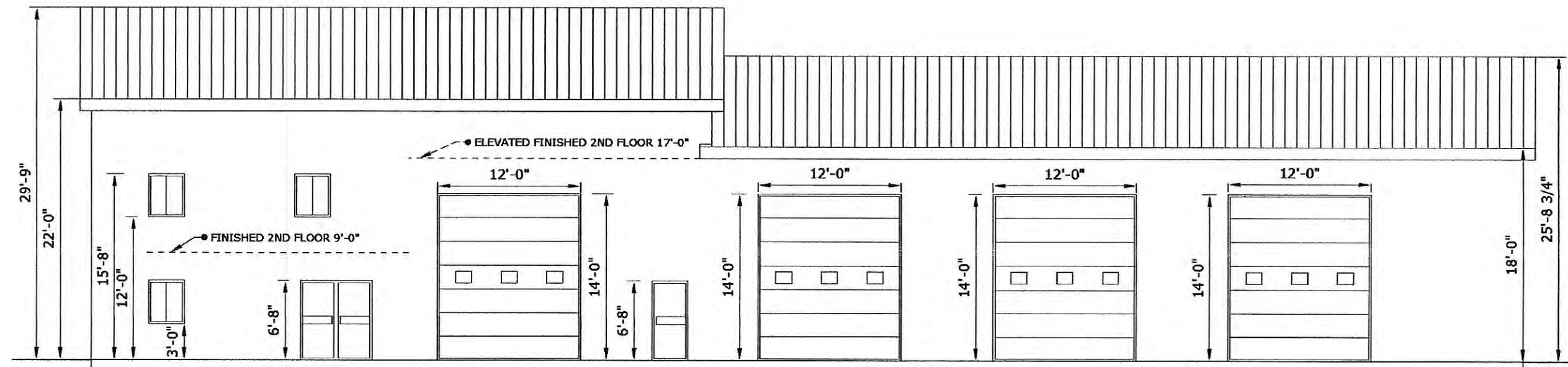
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8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A



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(603) 670-6964
dan.infinitypro@gmail.com

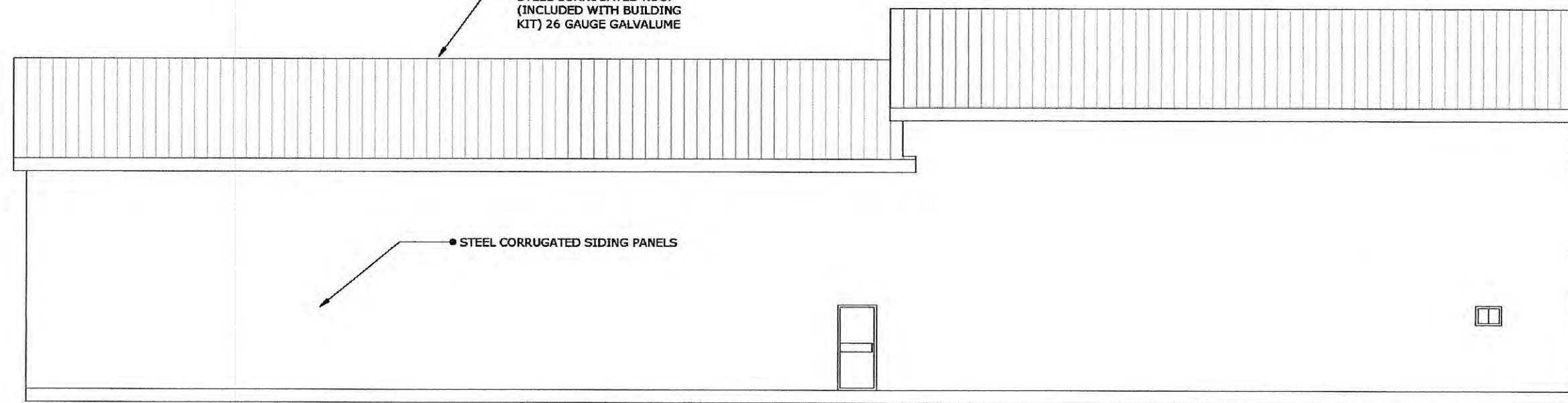
PROJECT: JERRY JOHNSON
HORSELESS CARRIAGE
754 ELM ST.
MILFORD, NH 03055



SLAB ON GRADE. STRUCTURAL SPECIFICATIONS TO BE DETERMINED BY STRUCTURAL ENGINEER.

GENERAL NOTES:
BUILDING TO BE A PREFABRICATED ENGINEERED STEEL STRUCTURE WITH I-BEAM FRAMING.
BUILDING KIT TO INCLUDE INSULATION SYSTEM.
INTERIOR PARTITION WALLS TO BE OF STEEL STUD AND FIRE CODE FINISHED DRYWALL.
OFFICE CEILING AND FLOOR FINISHES TO BE DETERMINED.
WINDOWS AND DOORS TO BE ORDERED SEPARATE FROM BUILDING KIT.

STEEL CORRUGATED ROOF (INCLUDED WITH BUILDING KIT) 26 GAUGE GALVALUME



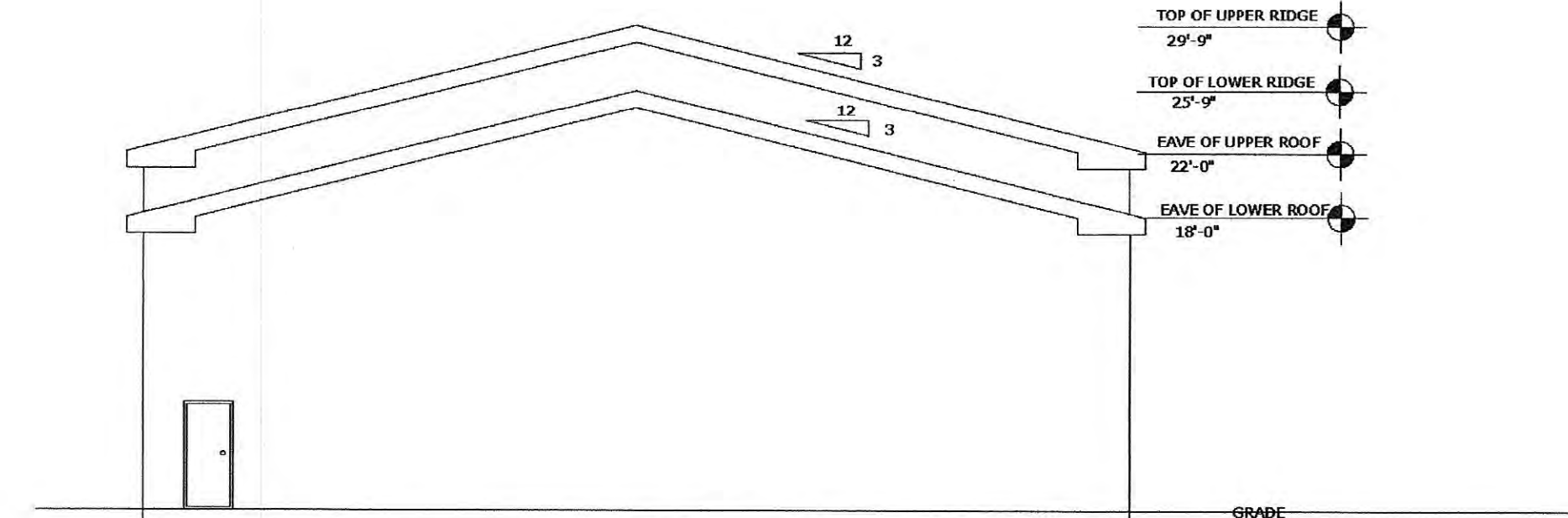
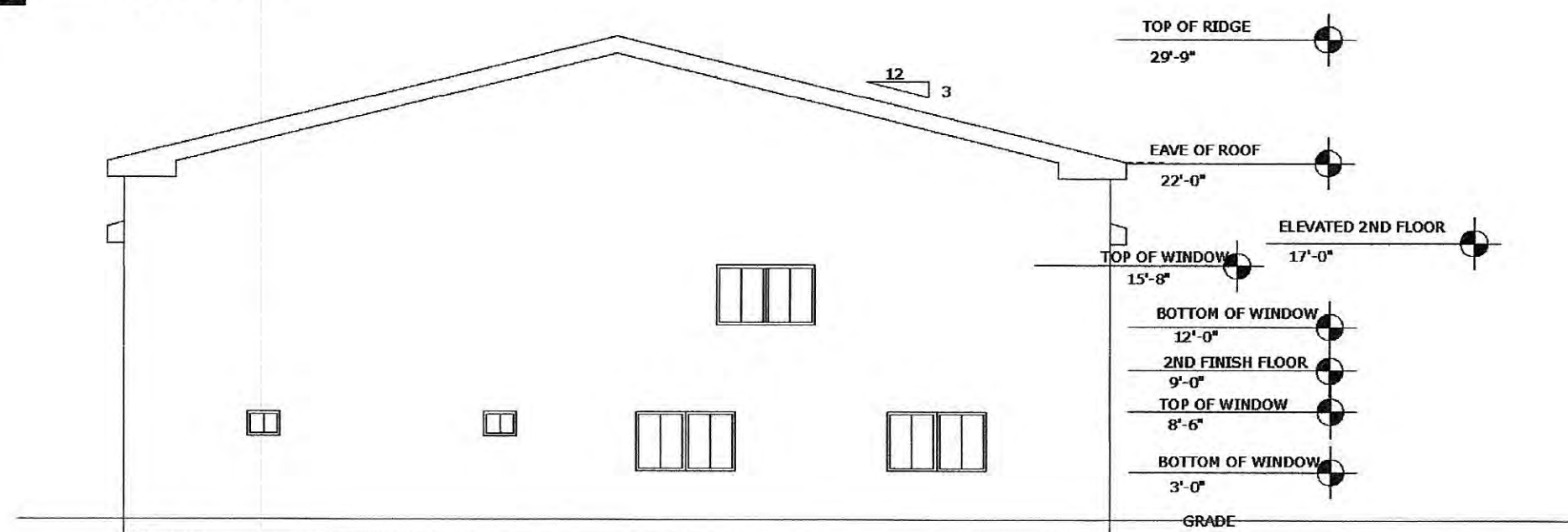
BUILDING ELEVATIONS

SCALE = 1" = 10'



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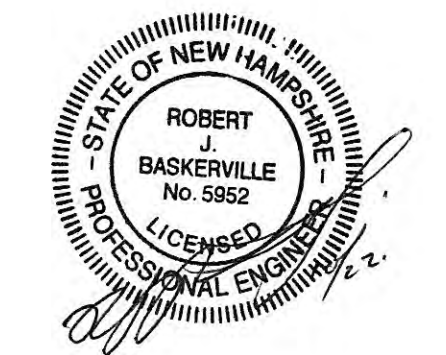
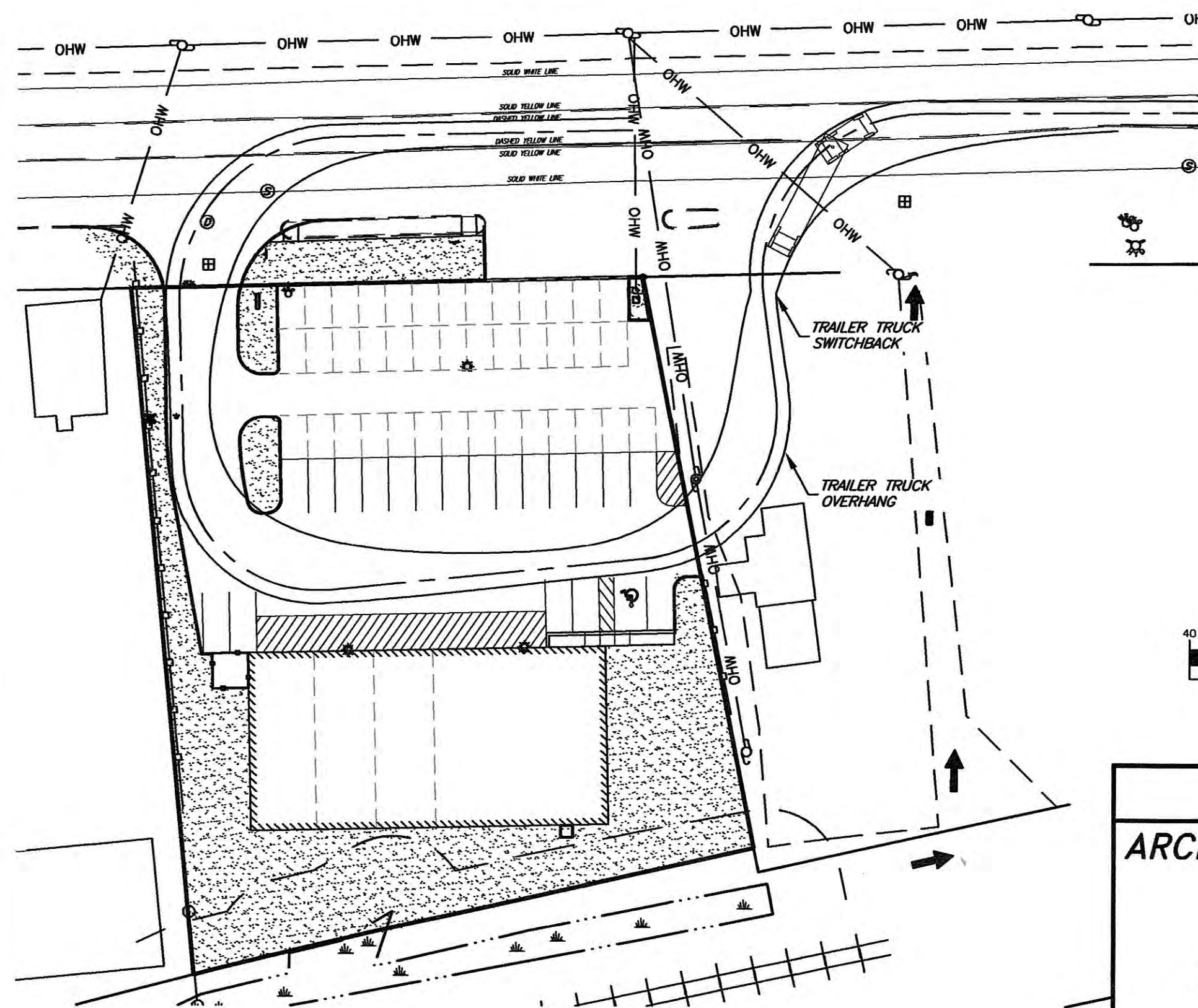
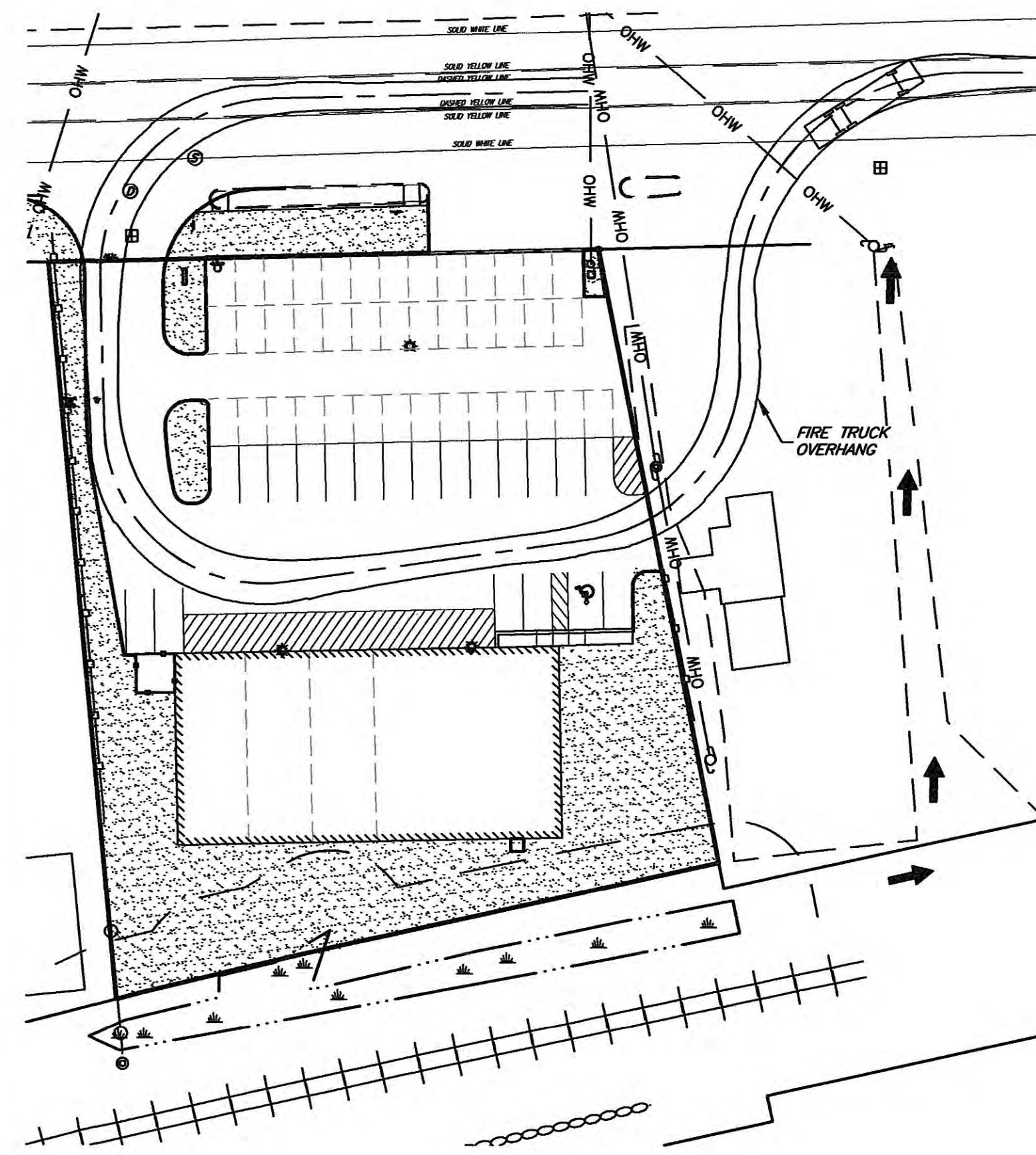
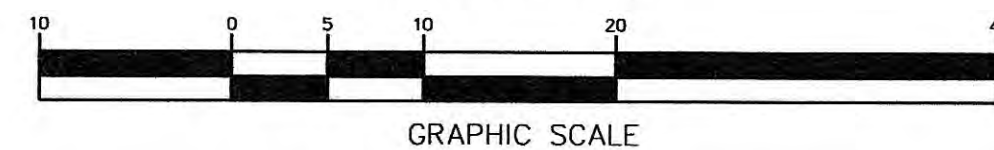
PROJECT: JERRY JOHNSON
HORSELESS CARRIAGE
754 ELM ST.
MILFORD, NH 03055



NOTES:

1. ARCHITECTURAL PLANS ARE PROVIDED BY INFINITY CONSTRUCTION.

SCALE: 1" = 10'



SCALE: 1" = 40'



TAX MAP 12 LOT 11
ARCHITECTURAL & TRUCK TURNING PLAN
SOONER-MILFORD
LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055
OWNER: LAND BARGE, LLC
754-756 ELM STREET
MILFORD, NH 03055
APPLICANT: SOONER TRANSPORTATION LLC
3 EXECUTIVE PARK DRIVE, SUITE 201G
BEDFORD, NH 03110

SCALE: AS NOTED APRIL 25, 2022 SHEET 7 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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GENERAL CONSTRUCTION NOTES:

- BOTH THE CONTRACTOR AND OWNER NEED TO SUBMIT A SEPARATE "NOTICE OF INTENT" TO BE COVERED BY THE N.H.D.E.S. GENERAL PERMIT FOR STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES.
- A MANDATORY PRECONSTRUCTION MEETING SHALL BE HELD WITH THE TOWN, CONTRACTOR, OWNER, AND ALL UTILITY REPRESENTATIVES PRIOR TO CONSTRUCTION. NO WORK SHALL BEGIN UNTIL APPROVAL BY THE HIGHWAY DEPARTMENT HAS BEEN OBTAINED.
- ALL CONSTRUCTION MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPROPRIATE SECTION OF THE STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (LATEST EDITION) AND LOCAL REGULATIONS.
- ANY SUBSTITUTIONS OF MATERIALS SHALL BE APPROVED BY THE ENGINEER IN WRITING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED LOCAL AND STATE CONSTRUCTION PERMITS PRIOR TO BEGINNING WORK.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN THESE DRAWINGS AND ACTUAL FIELD CONDITIONS PRIOR TO BEGINNING CONSTRUCTION.
- SHOULD GROUND WATER OR UNSUITABLE MATERIALS BE ENCOUNTERED DURING CONSTRUCTION, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY FOR DETERMINATION OF POSSIBLE CONSTRUCTION DESIGN CHANGES SUCH AS (BUT NOT LIMITED TO) UNDERDRAINS OR ALIGNMENT AND GRADE CHANGES.
- CLEARING THE SITE SHALL INCLUDE THE REMOVAL AND DISPOSAL OF DOWN TIMBER, RUBBISH AND DEBRIS FOUND EXISTING WITHIN THE AREAS TO BE CLEARED. CLEARING SHALL NOT TAKE PLACE UNTIL THE CONTRACTOR HAS DETERMINED FROM THE OWNER WHICH TREES ARE TO BE SAVED WITHIN THE CLEARING LIMITS.
- PAVEMENT OF THE ROADWAY SHALL CONSIST OF A HOT BITUMINOUS LAYER, A CRUSHED GRAVEL LAYER AND A GRAVEL SUBBASE LAYER.
 - BITUMINOUS TYPE F WEARING AND TYPE B BASE COURSES SHALL BE CONSTRUCTED PER N.H.D.O.T. SPECIFICATION 401 CONSTRUCTION REQUIREMENTS.
 - GRAVEL SHALL MEET THE REQUIREMENTS OF N.H.D.O.T. 304.2.
 - THE CRUSHED GRAVEL SHALL MEET THE REQUIREMENTS OF N.H.D.O.T. 304.3.
 - REFER TO THE TYPICAL ROAD CROSS SECTION DETAIL FOR DIMENSIONS.
- COMPACTION OF BACKFILL:
 - GRASSED AREAS: EMBANKMENT FILL AREAS SHALL CONSIST OF COMMON FILL PLACED IN 12 INCH LIFTS AND COMPACTED TO 90%.
 - ROADWAYS: THE COMPACTION REQUIREMENTS FOR MATERIALS PLACED AS BACKFILL, SUBGRADE, BASE COURSE AND PAVEMENT SHALL BE AS SPECIFIED FOR EACH SEPARATE ITEM IN THE N.H.D.O.T. "STANDARD SPECIFICATIONS" FOR ROAD AND BRIDGE CONSTRUCTION.
- CATCH BASINS AND MANHOLES SHALL BE PRE-CAST REINFORCED CONCRETE DESIGNED BY AN ENGINEER REGISTERED IN THE STATE OF NEW HAMPSHIRE AND ABLE TO WITHSTAND LOADINGS OF 8 TONS (H-20 LOADING).
- TRENCH CONSTRUCTION WILL CONFORM WITH SECTION 603.3.1. OF THE N.H.D.O.T. STANDARD SPECIFICATIONS (LATEST EDITION).
- WOOD SHEETING OR A SUITABLE TRENCH BOX SHALL BE USED TO SUPPORT THE TRENCH AS NECESSARY. IF WOOD SHEETING IS USED, IT SHALL BE DRIVEN AT A DISTANCE OF ONE FOOT FROM THE OUTSIDE DIAMETER OF THE PIPE TO A DEPTH SIX INCHES BELOW THE INVERT OF THE PIPE. WOOD SHEETING SHALL BE CUT OFF AND LEFT IN PLACE TO AN ELEVATION NOT LESS THAN ONE FOOT ABOVE THE TOP OF THE PIPE, BUT NOT GREATER THAN THREE FEET BELOW THE FINISHED GRADE.
- TRENCH BEDDING SHALL CONFORM WITH SECTION 603.3.2. OF THE STANDARD SPECIFICATIONS (LATEST EDITION). FIRST CLASS BEDDING WILL BE REQUIRED FOR ALL PIPES 48" OR MORE IN DIAMETER OR SPAN.
- BACKFILL MATERIAL FOR TRENCHES WILL CONFORM WITH SECTION 603.3.5. OF THE STANDARD SPECIFICATIONS (LATEST EDITION) AND IN ADDITION, SHALL EXCLUDE DEBRIS, PIECES OF PAVEMENT, ORGANIC MATTE, TOP SOIL, ALL WET OR SOFT MUCK, PEAT OR CLAY, ALL EXCAVATED LEDGE MATERIAL, ALL ROCKS OVER SIX INCHES IN LARGEST DIMENSION, OR ANY MATERIAL WHICH AS DETERMINED BY THE ENGINEER, WILL NOT PROVIDE SUFFICIENT SUPPORT OR MAINTAIN THE COMPLETED CONSTRUCTION IN A STABLE CONDITION. BACKFILL SHALL NOT BE PLACED ON FROZEN MATERIAL.
- COMPACTION OF TRENCH BACKFILL AND PIPE BEDDING SHALL BE SIX INCH LIFTS FOR BEDDING AND BACKFILL TO A PLANE ONE FOOT ABOVE THE PIPE AND IN 12 INCH LIFTS THEREAFTER BY AN APPROVED MECHANICAL COMPACTOR.
- SHOULD FROZEN MATERIAL BE ENCOUNTERED, IT SHALL NOT BE PLACED IN THE BACKFILL NOR SHALL BACKFILL BE PLACED UPON FROZEN MATERIAL.
- THE DISTURBED AREA SHALL BE KEPT TO A MINIMUM. DISTURBED AREAS REMAINING IDLE FOR MORE THAN 30 DAYS SHALL BE STABILIZED.
- ALL SEEDED AREAS SHALL BE MULCHED WITHIN 24 HOURS AFTER SEEDING. A GOOD QUALITY OF STRAW MULCH SHOULD BE USED AND APPLIED AT THE RATE OF 2 TONS PER ACRE.
- BASIN FLOORS IN THE INFILTRATION BASINS ARE TO BE DEEPLY TILLED TO RESTORE INFILTRATION RATES, FOLLOWED BY A PASS WITH A LEVELING DRAG PRIOR TO FINAL SEEDING. STORMWATER FLOWS SHALL NOT BE DIRECTED TO THE INFILTRATION BASINS, SWALES, OR DITCHES UNTIL ALL CONTRIBUTING AREAS HAVE BEEN FULLY STABILIZED.
- ALL SLOPES GREATER THAN 3:1 MUST BE MATTED WITH NORTH AMERICAN GREEN ST100BN EROSION CONTROL BLANKETING.
- THE PROJECT SHALL BE MANAGED TO MEET THE REQUIREMENTS OF AND INTENT OF RSA 430:51-57 AND Agr 3800 RELATIVE TO INVASIVE SPECIES: AND FUGITIVE DUST IS TO BE CONTROLLED IN ACCORDANCE WITH Env-A 1000.
- THE TOWN OF AUBURN RESERVES THE RIGHT TO REQUIRE THAT ADDITIONAL EROSION CONTROL MEASURES BE INSTALLED DURING CONSTRUCTION BASED ON FIELD OBSERVATIONS/INSPECTIONS.

FUELING AND MAINTENANCE OF CONSTRUCTION EQUIPMENT

- IF ANY CONSTRUCTION EQUIPMENT, INCLUDING BUT NOT LIMITED TO EARTHMOVING, EXCAVATION, AND BORING EQUIPMENT, WILL BE FUELED FROM A TANK TRUCK OR OTHER CONTAINER THAT IS MOVED AROUND THE SITE, THE FOLLOWING SHALL APPLY:
 - PORTABLE CONTAINMENT EQUIPMENT THAT IS SIZED TO CONTAIN THE MOST LIKELY VOLUME OF FUEL TO BE SPILLED DURING A FUEL TRANSFER SHALL BE USED, WHERE THE MOST LIKELY VOLUME TO BE SPILLED IS DETERMINED BASED ON THE FUEL TRANSFER RATE, THE AMOUNT OF FUEL BEING TRANSFERRED, THE DISTANCE BETWEEN THE HOSE NOZZLE AND PUMP SHUT OFF SWITCH, AND THE RESPONSE TIME OF PERSONNEL AND EQUIPMENT AVAILABLE AT THE FACILITY;
 - THE CONTAINMENT EQUIPMENT SHALL BE POSITIONED TO CATCH ANY FUEL SPILLS DUE TO OVERTILLING THE EQUIPMENT AND ANY OTHER SPILLS THAT MIGHT OCCUR AT OR NEAR THE FUEL FILLER PORT TO THAT EQUIPMENT;
 - THE TYPE OF CONTAINMENT EQUIPMENT USED AND ITS POSITIONING AND USE SHALL ACCOUNT FOR ALL OF THE DRIP POINTS ASSOCIATED WITH THE FUEL FILLING PORT AND THE HOSE FROM THE FUEL DELIVERY TRUCK; AND
 - PERSONNEL SHALL NOT LEAVE THE IMMEDIATE AREA WHILE FUEL IS BEING TRANSFERRED, TO ENSURE THAT ANY SPILLS WILL BE OF LIMITED VOLUME.
- IF THE SITE WILL HAVE A FIXED LOCATION FOR FUELING CONSTRUCTION EQUIPMENT, THE FOLLOWING SHALL APPLY:
 - ALL FUEL CONTAINERS, INCLUDING BUT NOT LIMITED TO SKID-MOUNTED TANKS, DRUMS, AND FIVE GALLON CANS, SHALL HAVE SECONDARY CONTAINMENT THAT:
 - IS CAPABLE OF CONTAINING 110% OF THE VOLUME OF THE LARGEST FUEL STORAGE CONTAINER; AND
 - HAS AN IMPERVIOUS FLOOR;
 - SECONDARY CONTAINMENT FOR TANKS MAY COMPRISE A METAL, PLASTIC, POLYMER OR PRECAST CONCRETE VAULT PROVIDING 110 PERCENT OF THE VOLUME OF THE LARGEST FUEL STORAGE CONTAINER;
 - FOR FUEL CONTAINERS, SECONDARY CONTAINMENT MAY COMPRISE CONTAINMENT PALLETS;
 - THE AREA WHERE FUEL IS TRANSFERRED SHALL BE A FLAT, IMPERVIOUS AREA THAT:
 - IS ADJACENT TO THE FUEL CONTAINER(S); AND
 - EXTENDS BEYOND THE FULL REACH, OR LENGTH, OF THE FUEL HOSE; AND
 - SECONDARY CONTAINMENT AREAS MAY BE IN THE FORM OF A BASIN THAT IS:
 - SLOPED DOWN TO A CENTRAL LOW POINT OR BERMED ALONG THE PERIMETER;
 - LINED WITH A CONTINUOUS SHEET OF 20 MIL OR THICKER POLYMER MATERIAL OR APPROPRIATE GEOMEMBRANE LINER; AND
 - BACKFILLED WITH AT LEAST 6 INCHES OF SAND

CONSTRUCTION SEQUENCE:

- A MANDATORY PRECONSTRUCTION MEETING SHALL BE HELD WITH THE TOWN, CONTRACTOR, OWNER, AND ALL UTILITY REPRESENTATIVES PRIOR TO CONSTRUCTION. NO WORK SHALL BEGIN UNTIL APPROVAL BY THE HIGHWAY DEPARTMENT HAS BEEN OBTAINED.
- CLEAR AREA FOR CONSTRUCTION ENTRANCE AND INSTALL STABILIZED CONSTRUCTION ENTRANCES AS SHOWN ON THESE PLANS.
- CUT AND CLEAR TREES IN CONSTRUCTION AREAS ONLY.
- INSTALL SILT FENCE
- REMOVE STUMPS FROM SITE FOR SITE GRADING (CUT AND/OR FILL) TO SUBGRADE. STABILIZE AREAS WITH BASE GRAVEL WITHIN SIX WEEKS OF REMOVING STUMPS.
- THE MAXIMUM UNSTABILIZED AREA SHALL BE LIMITED TO THE MINIMUM AREA PRACTICABLE FOR SITE CONSTRUCTION (NOT TO EXCEED 5 ACRES). NO AREA SHALL BE LEFT UNSTABILIZED MORE THAN 6 WEEKS. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS HAPPENED:
 - BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
 - A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
 - A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIP-RAP HAS BEEN INSTALLED; OR EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.
- CONSTRUCT TEMPORARY AND PERMANENT SEDIMENT, EROSION AND DETENTION CONTROL FACILITIES AS PER THE NOTES IN THESE DRAWINGS. EROSION, SEDIMENT, AND DETENTION MEASURES SHALL BE INSTALLED PRIOR TO ANY EARTH MOVING OPERATION.
 - SILT FENCE
 - RIP RAP LINED SWALES
 - RIP RAP APRONS AT CULVERT OUTLETS
 - TREATMENT SWALES
 - DETENTION PONDS
- ALL DITCHES/SWALES/BASINS SHALL BE STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM.
- IF, DURING CONSTRUCTION, IT BECOMES APPARENT THAT ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES ARE REQUIRED TO PROTECT THE CONSTRUCTION SITE DUE TO ACTUAL SITE CONDITIONS, THE OWNER SHALL BE REQUIRED TO INSTALL THE NECESSARY EROSION AND SEDIMENT CONTROL MEASURES.
- BASIN FLOORS IN THE INFILTRATION BASINS ARE TO BE DEEPLY TILLED TO RESTORE INFILTRATION RATES, FOLLOWED BY A PASS WITH A LEVELING DRAG PRIOR TO FINAL SEEDING. STORMWATER FLOWS SHALL NOT BE DIRECTED TO THE INFILTRATION BASINS, SWALES, OR DITCHES UNTIL ALL CONTRIBUTING AREAS HAVE BEEN FULLY STABILIZED.
- FINISH CLEARING AND GRUBBING.
- CONSTRUCT TEMPORARY CULVERTS AND DIVERSION CHANNELS, AS NECESSARY.
- CONSTRUCT CONSTRUCTION ENTRANCES TO ACCESS DESIRED CONSTRUCTION AREAS.
- BEGIN CONSTRUCTION OF UTILITIES AND STORM DRAINAGE AS NECESSARY.
- MODIFY EROSION CONTROL MEASURES.
- BEGIN PERMANENT AND TEMPORARY INSTALLATION OF SEED AND MULCH. ALL CUT AND FILL SLOPES SHALL BE STABILIZED.
- DRAIN OR AS REQUIRED, CONSTRUCT TEMPORARY BERMS, DRAIN DITCHES, SILT FENCES, SEDIMENT TRAPS, ETC., MULCH AND SEED AS NECESSARY.
- PAVE ALL PARKING AREAS AS SPECIFIED ON THE PLAN.
- INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES DURING CONSTRUCTION. ALL EROSION AND SEDIMENT CONTROLS NEED TO BE INSPECTED WEEKLY AND AFTER EVERY 0.5" OF RAINFALL.
- COMPLETE PERMANENT SEEDING AND LANDSCAPING.
- ALL DISTURBED AREAS SHALL BE PERMANENTLY STABILIZED WITHIN 72 HOURS OF FINISH GRADING. MAXIMUM EXPOSURE LENGTH FOR ALL DISTURBED AREAS IS 30 DAYS.
- REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER SEEDED AREAS HAVE ESTABLISHED THEMSELVES AND SITE IMPROVEMENTS ARE COMPLETED.

SITE MAINTENANCE AND INSPECTION PROGRAM

- INSPECTIONS: THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT. MAINTENANCE PRACTICES SHALL INCLUDE, BUT ARE NOT LIMITED TO

- CLEANING OF CATCH BASINS TWICE PER YEAR OR MORE FREQUENTLY AS DICTATED BY WEEKLY INSPECTIONS AND/OR AFTER 0.5" RAINFALL EVENTS.
 - CLEANING OF SEDIMENT OR DEBRIS FROM STORM WATER MANAGEMENT AREA INLETS TWICE PER YEAR OR MORE FREQUENTLY AS DICTATED BY WEEKLY INSPECTIONS AND/OR AFTER 0.5" RAINFALL EVENTS.
 - WEEKLY SITE INSPECTIONS TO DETERMINE IMPLEMENT NECESSARY REPAIR AND MAINTENANCE ACTIVITIES.
 - REMOVAL OF SEDIMENT BUILDUP ALONG SILT FENCES, STRAW BALE BARRIERS, GRASS SWALES, AND TREATMENT BASIN INLETS. REMOVE SEDIMENT BUILDUP IN BOTTOM OF TREATMENT BASINS SUCH THAT ALL OUTLETS ARE KEPT FREE FROM SEDIMENT AND DEBRIS.
 - INSPECTION/RECONSTRUCTION OF STABILIZED CONSTRUCTION ENTRANCE.
 - TREATMENT OF NON-STORMWATER RELATED DISCHARGES SUCH AS WATER LINE INSTALLATION FLUSH WATER OR GROUNDWATER FROM DEWATERING ACTIVITIES. THESE FLOWS SHOULD BE DIRECTED TO A TEMPORARY SEDIMENTATION BASIN OR CONSTRUCTED STORM WATER MANAGEMENT AREA WITH WATER QUALITY SKIMMER OUTLETS.
 - SWEEP PAVED PARKING LOTS AND DRIVES REGULARLY TO MINIMIZE SEDIMENT ACCUMULATION.
- GOOD HOUSEKEEPING PRACTICES

THE CONTRACTOR SHALL EMPLOY MEASURES AND PRACTICES TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS TO STORM WATER RUNOFF. THE CONTRACTOR SHALL USE CARE IN THE HANDLING, USE AND DISPOSAL OF MATERIALS SUCH AS PETROLEUM PRODUCTS, FERTILIZERS AND PAINTS TO ENSURE THAT THE RISK ASSOCIATED WITH THE USE OF THESE PRODUCTS IS MINIMIZED. THE FOLLOWING PRACTICES SHALL BE FOLLOWED DURING THE CONSTRUCTION OF THIS PROJECT:

- AN EFFORT SHALL BE MADE TO STORE ONLY ENOUGH PRODUCT REQUIRED FOR THIS SPECIFIC SITE.
 - ALL MATERIALS STORED ON SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER IN THEIR APPROPRIATE CONTAINERS AND, IF POSSIBLE, UNDER A ROOF OR OTHER SUITABLE ENCLOSURE.
 - PRODUCTS SHALL BE KEPT IN THEIR ORIGINAL CONTAINERS WITH THEIR ORIGINAL LABELS.
 - WHENEVER POSSIBLE, ALL OF THE PRODUCT SHALL BE USED BEFORE DISPOSING OF THE CONTAINER.
 - THE MANUFACTURERS RECOMMENDATIONS SHALL BE FOLLOWED IN REGARD TO THE PROPER USE AND DISPOSAL OF ALL PRODUCTS.
 - THE CONTRACTOR SHALL INSPECT DAILY TO ENSURE THE PROPER USE AND DISPOSAL OF ALL MATERIALS ON SITE.
- SPILL PREVENTION AND CLEANUP PRACTICES

THE CONTRACTOR/OPERATOR SHALL BE RESPONSIBLE FOR THE SAFE HANDLING, USE AND DISPOSAL PROGRAM OF ALL HAZARDOUS MATERIALS FOR THE DURATION OF THIS PROJECT AND SHALL HAVE A SPECIFIC SPILL PREVENTION AND CLEANUP PROTOCOL FOR ALL HAZARDOUS MATERIALS, INCLUDING, BUT NOT LIMITED TO:

- MANUFACTURERS RECOMMENDED METHODS FOR SPILL CLEANUP WILL BE CLEARLY POSTED AND SITE PERSONNEL WILL BE MADE AWARE OF THESE PROCEDURES AND THE LOCATION OF THE CLEANUP SUPPLIES.
- MATERIALS AND EQUIPMENT NECESSARY FOR SPILL CLEANUP WILL BE KEPT IN THE MATERIAL STORAGE AREA ON SITE. EQUIPMENT AND MATERIAL WILL INCLUDE, BUT NOT BE LIMITED TO, BROOMS, DUSTPANS, MOPS, RAGS, GLOVES, GOGGLES, KITTY LITTER, SAND, SANDUST, AND PLASTIC/METAL TRASH CONTAINERS SPECIFICALLY FOR THIS PURPOSE.
- ALL SPILLS SHALL BE CLEANED UP IMMEDIATELY AFTER DISCOVERY.
- THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- SPILLS OF TOXIC OR HAZARDOUS MATERIAL WILL BE REPORTED TO THE APPROPRIATE STATE OR LOCAL GOVERNMENT AGENCY, REGARDLESS OF THE SIZE.
- THE SPILL PREVENTION PLAN WILL BE ADJUSTED TO INCLUDE MEASURES TO PREVENT THIS TYPE OF SPILL FROM REOCCURRING, AND HOW TO CLEAN UP THE SPILL IF THERE IS ANOTHER ONE. A DESCRIPTION OF THE SPILL, WHAT CAUSED IT, AND THE CLEANUP MEASURES WILL ALSO BE INCLUDED.

COLD WEATHER STABILIZATION

- TO ADEQUATELY PROTECT WATER QUALITY DURING COLD WEATHER AND DURING SPRING RUNOFF, THE ADDITIONAL STABILIZATION TECHNIQUES SPECIFIED IN THIS SECTION SHALL BE EMPLOYED DURING THE PERIOD FROM OCTOBER 15 THROUGH MAY 1.
- SUBJECT TO (C), BELOW THE AREA OF EXPOSED, UNSTABILIZED SOIL SHALL BE LIMITED TO ONE ACRE, AND PROTECTED AGAINST EROSION BY THE METHODS DESCRIBED IN THIS SECTION PRIOR TO ANY THAW OR SPRING MELT EVENT.
- THE ALLOWABLE AREA OF EXPOSED SOIL MAY BE INCREASED IF A WINTER CONSTRUCTION PLAN IS DEVELOPED BY A QUALIFIED ENGINEER OR A GEOTECH SPECIALIST AND BY SUBMITTED TO THE DEPARTMENT FOR APPROVAL AS A REQUEST TO WAIVE THE ONE-ACRE LIMIT.
- SUBJECT TO (F) AND (G), BELOW, ALL PROPOSED VEGETATED AREAS HAVING A SLOPE OF LESS THAN 15% THAT DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15, OR THAT ARE DISTURBED AFTER OCTOBER 15, SHALL BE SEEDED AND COVERED WITH 3 TO 4 TONS OF HAY OR STRAW MULCH OR ANCHORED NETTING OR TACKIFIER OR WITH AT LEAST 2 INCHES OF EROSION CONTROL MIX MEETING THE CRITERIA OF ENV-WO 1506.05(B).
- ANCHORED HAY MULCH OR EROSION CONTROL MIX THAT MEETS THE CRITERIA OF ENV-WO 1506.05(B), SHALL NOT BE INSTALLED OVER SNOW GREATER THAN ONE INCH IN DEPTH.
- EROSION CONTROL BLANKETS SHALL NOT BE INSTALLED OVER SNOW GREATER THAN ONE INCH IN DEPTH OR ON FROZEN GROUND.
- ALL PROPOSED STABILIZATION IN ACCORDANCE WITH (D) OR (E), ABOVE, SHALL BE COMPLETED WITHIN A DAY OF ESTABLISHING THE GRADE THAT IS FINAL OR THAT OTHERWISE WILL EXIST FOR MORE THAN 5 DAYS.
- ALL DITCHES OR SWALES THAT DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15, OR THAT ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS, AS DETERMINED BY THE OWNER'S ENGINEERING CONSULTANT.
- AFTER OCTOBER 15, INCOMPLETE ROAD OR PARKING AREAS WHERE ACTIVE CONSTRUCTION OF THE ROAD OR PARKING AREA HAS STOPPED FOR THE WINTER SEASON SHALL BE PROTECTED WITH A MINIMUM 3-INCH LAYER OF BASE COURSE GRAVELS MEETING THE GRADATION REQUIREMENTS OF NHDOT STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION, 2016, TABLE 304-1, ITEM NO. 304.1, 304.2, OR 304.3.
- ALL TOPSOIL STOCKPILED SHALL BE MULCHED PRIOR TO ANY SNOW EVENT. STANDARD PROTECTION OF STOCKPILES AS DESCRIBED IN OTHER SECTIONS APPLIES.
- IN THE EVENT OF SNOWFALL (FRESH OR CUMULATIVE) GREATER THAN 1 INCH DURING WINTER CONSTRUCTION PERIOD ALL SNOW SHALL BE REMOVED FROM AREAS OF SEEDING AND MULCHING PRIOR TO PLACEMENT.

GENERAL EROSION CONTROL NOTES:

- PERIMETER CONTROLS MUST BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS;
- STORMWATER TREATMENT PONDS AND DRAINAGE SWALES MUST BE INSTALLED BEFORE ROUGH GRADING THE SITE;
- RUNOFF MUST BE DIRECTED TO TEMPORARY PRACTICES UNTIL STORMWATER BMPs ARE STABILIZED;
- BASINS, DITCHES AND SWALES MUST BE STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM;
- ROADWAYS AND PARKING AREAS MUST BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE;
- CUT AND FILL SLOPES MUST BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE;
- ALL AREAS OF UNSTABILIZED SOIL MUST BE STABILIZED AS SOON AS PRACTICABLE BUT NO LATER THAN 45 DAYS OF INITIAL DISTURBANCE;
- EROSION CONTROL PRACTICES MUST BE INSPECTED AT LEAST WEEKLY AND AFTER EVERY RAIN EVENT OF 0.5 INCH OR MORE;
- THE AREA OF DISTURBANCE MUST BE LIMITED TO 5 ACRES UNLESS ENV-WO 1505.05 RELATIVE TO COLD WEATHER STABILIZATION APPLIES;
- IN AREAS THAT WILL NOT BE PAVED, STABLE MEANS THAT:
 - A MINIMUM OF 85% VEGETATIVE COVER HAS BEEN ESTABLISHED;
 - A MINIMUM OF 3 INCHES OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED; OR
 - EROSION CONTROL BLANKETS HAVE BEEN INSTALLED IN ACCORDANCE WITH ENV-WO 1506.03; AND
- IN AREAS TO BE PAVED, STABLE MEANS THAT BASE COURSE GRAVELS MEETING THE REQUIREMENTS OF NHDOT STANDARD FOR ROAD AND BRIDGE CONSTRUCTION, 2016, ITEM 304.2 HAVE BEEN INSTALLED.

- TEMPORARY SEDIMENT TRAP.** TEMPORARY SEDIMENT TRAPS SHALL COMPLY WITH THE FOLLOWING:
- THE TRAP SHALL BE INSTALLED AS CLOSE TO THE DISTURBED AREA OR SOURCE OF SEDIMENT AS POSSIBLE;
 - THE MAXIMUM CONTRIBUTING DRAINAGE AREA TO THE TRAP SHALL BE LESS THAN 5 ACRES;
 - THE MINIMUM VOLUME OF THE TRAP SHALL BE 3,600 CUBIC FEET OF STORAGE FOR EACH ACRE OF DRAINAGE AREA;
 - THE SIDE SLOPES OF THE TRAP SHALL BE 3:1 OR FLATTER, AND SHALL BE STABILIZED IMMEDIATELY AFTER THEIR CONSTRUCTION;
 - THE OUTLET OF THE TRAP SHALL BE A MINIMUM OF ONE FOOT BELOW THE CREST OF THE TRAP AND SHALL DISCHARGE TO A STABILIZED AREA;
 - THE TRAP SHALL BE CLEANED WHEN 50 PERCENT OF THE ORIGINAL VOLUME IS FILLED; AND
 - THE MATERIALS REMOVED FROM THE TRAP SHALL BE PROPERLY DISPOSED OF AND STABILIZED.

- CONSTRUCTION DEWATERING.** DEWATERING SHALL COMPLY WITH THE FOLLOWING:
- THE DISCHARGE SHALL BE STOPPED IMMEDIATELY IF THE RECEIVING AREA SHOWS ANY SIGN OF INSTABILITY OR EROSION;
 - ALL CHANNELS, SWALES, AND DITCHES DUG FOR DISCHARGING WATER FROM THE EXCAVATED AREA SHALL BE STABILIZED PRIOR TO DIRECTING DISCHARGE TO THEM;
 - IF A CONSTRUCTION EQUIPMENT BUCKET IS USED, IT SHALL EMPTY THE MATERIAL TO A STABLE AREA;
 - NO DEWATERING SHALL OCCUR DURING PERIODS OF INTENSE, HEAVY RAIN;
 - FLOW TO THE SEDIMENT REMOVAL STRUCTURE SHALL NOT EXCEED THE STRUCTURE'S CAPACITY TO SETTLE AND FILTER FLOW OR ITS VOLUME CAPACITY; AND
 - WHEREVER POSSIBLE, THE DISCHARGE FROM THE SEDIMENT REMOVAL STRUCTURE SHALL DRAIN TO A WELL-VEGETATED BUFFER BY SHEET FLOW WHILE MAXIMIZING THE DISTANCE TO THE NEAREST WATER RESOURCE AND MINIMIZING THE SLOPE OF THE BUFFER AREA

- TEMPORARY STORMWATER DIVERSION.** TEMPORARY STORMWATER DIVERSION SHALL COMPLY WITH THE FOLLOWING:
- WHEN NECESSARY TO MINIMIZE RELEASE OF SEDIMENT-LADEN RUNOFF PRIOR TO STABILIZATION OF THE SITE THE PERMANENT STORMWATER MANAGEMENT SYSTEM COMPONENTS, SEDIMENT-LADEN WATER SHALL BE DIVERTED AND STORED IN TEMPORARY DIVERSION PRACTICES SUCH AS SEDIMENT BASINS OR TRENCHES;
 - SUBJECT TO (C), BELOW, TEMPORARY DIVERSION PRACTICES SHALL BE STABILIZED PRIOR TO RECEIVING RUNOFF;
 - TEMPORARY DIVERSION CHANNELS WITH A GRADIENT OF 2 PERCENT OR GREATER SHALL BE STABILIZED, HOWEVER CHANNELS WITH A SLOPE OF LESS THAN 2% SHALL BE STABILIZED ONLY IF EROSION IS OBSERVED;
 - THE AREA DRAINING TO EACH TEMPORARY DIVERSION PRACTICE SHALL BE LESS THAN 5 ACRES;
 - TEMPORARY DIVERSION CHANNELS SHALL CONVEY, AND TEMPORARY BASINS AND TRENCHES SHALL CONTAIN, THE 2-YEAR, 24 HOUR DESIGN STORM WITHOUT OVERTOPPING THE BANKS;
 - THE BED SLOPE OF DIVERSION CHANNELS SHALL HAVE A POSITIVE GRADE TO ASSURE DRAINAGE;
 - WHERE DIVERSIONS CARRY CONCENTRATED FLOWS, ENERGY DISSIPATION MEASURES SHALL BE IMPLEMENTED TO DISPERSE FLOW INTO AREAS DOWNSTREAM OF THE DISTURBED AREA;
 - IF EROSION OF DIVERSION PRACTICES OCCURS DURING CONSTRUCTION, CORRECTIVE ACTION SHALL BE TAKEN TO STABILIZE THE BASIN, CHANNEL, AND BERM; AND
 - DIVERSION BASINS AND TRENCHES SHALL BE CLEARED OF SEDIMENT WHENEVER SEDIMENT ACCUMULATES.

SEEDING/MULCHING OF DISTURBED AREAS

- TEMPORARY AND PERMANENT MULCHING.** MULCHING SHALL COMPLY WITH THE FOLLOWING:
- HAY AND STRAW MULCHES SHALL BE ANCHORED WITH MULCH NETTING OR TACKIFIER SO THAT THEY ARE NOT BLOWN AWAY BY WIND OR WASHED AWAY BY FLOWING WATER;
 - MULCH MATERIALS SHALL BE SELECTED BASED UPON SOILS, SLOPE, FLOW CONDITIONS, AND TIME OF YEAR;
 - HAY OR STRAW MULCH SHALL BE APPLIED AT A RATE OF 1.5 TO 2 TONS PER ACRE, EQUIVALENT TO 70 TO 90 POUNDS PER 1,000 SQUARE FEET;
 - WOOD CHIPS OR GROUND BARK SHALL BE APPLIED AT 2 TO 6 INCHES DEEP AT A RATE OF 10 TO 20 TONS PER ACRE, EQUIVALENT TO 460 TO 920 POUNDS PER 1,000 SQUARE FEET;
 - JUTE AND FIBROUS MATS AND WOOD EXPOSURE SHALL BE INSTALLED ACCORDING TO THE APPLICABLE MANUFACTURER'S INSTRUCTIONS; AND
 - EROSION CONTROL MIX SHALL:
 - MEET THE CRITERIA OF ENV-WO 1506.05(B); AND
 - BE PLACED AT A THICKNESS OF 2 INCHES OR MORE.

VEGETATION. VEGETATING DISTURBED AREAS SHALL BE COMPLETED ONLY AS SPECIFIED BELOW:

- ALL ESSENTIAL GRADING AND TEMPORARY STRUCTURES, SUCH AS DIVERSIONS, DAMS, DITCHES, AND DRAINS NEEDED TO PREVENT GULLYING AND REDUCE SILTATION, SHOULD BE COMPLETED PRIOR TO SEEDING.
- STONES AND TRASH SHALL BE REMOVED FROM THE AREA TO BE SEEDDED SO AS NOT TO INTERFERE WITH THE SEEDING;
- TILL THE SOIL TO A DEPTH OF ABOUT FOUR (4) INCHES TO PREPARE A SEEDBED AND MIX FERTILIZER INTO THE SOIL. THE SEEDBED SHOULD BE LEFT IN A FIRM AND SMOOTH CONDITION. THE LAST TILLAGE OPERATION SHOULD BE PERFORMED ACROSS THE SLOPE WHEREVER PRACTICAL.
- ON SLOPES 4:1 OR STEEPER, FINAL PREPARATION OF THE AREA TO BE SEEDDED SHALL INCLUDE CREATING GROOVES IN THE SOIL PERPENDICULAR TO THE DIRECTION OF THE SLOPE TO CATCH SEED AND REDUCE RUNOFF;
- IF NEEDED TO ENSURE GROWTH, FERTILIZER OR OTHER ORGANIC SOIL AMENDMENTS SHALL BE APPLIED DURING THE GROWING SEASON;
- FERTILIZER APPLIED TO ANY AREA WITHIN 100 FEET OF ANY RIVER, STREAM, POND, OR LAKE SHALL BE LOW PHOSPHATE, SLOW RELEASE NITROGEN FERTILIZER ONLY;
- FERTILIZER APPLIED TO ANY AREA THAT IS SUBJECT TO RSA 483-B, THE COMPREHENSIVE WATER QUALITY PROTECTION ACT (ACT), SHALL MEET OR BE MORE PROTECTIVE OF WATER QUALITY THAN THE MINIMUM STANDARDS OF THE ACT;
- ALL SEEDED AREAS SHALL BE FERTILIZED, FERTILIZATION SHALL BE AT THE EQUIVALENT OF 500 LBS. PER ACRE OF 10-20-20 FERTILIZER.
- ALL GRADED AREAS SHALL BE SEEDDED WITH:
 - TALL FESCUE: 20 POUNDS PER ACRE
 - CRESPER REED FESCUE: 20 POUNDS PER ACRE
 - BIRDFOOT TREFLOL: 8 POUNDS PER ACRE
 - TOTAL 48 POUNDS PER ACRE LIVE SEED

- SEED SHOULD BE SPREAD UNIFORMLY BY THE METHOD MOST APPROPRIATE FOR THE SITE. METHODS INCLUDE BROADCASTING, DRILLING, AND HYDROSEEDING. WHERE BROADCASTING IS USED, COVER SEED WITH 0.25 IN. OF SOIL OR LESS, BY CULTIPACHING OR RAKING.
- RUNOFF SHALL BE DIVERTED FROM THE SEEDED AREA;
- SUBJECT TO (N) BELOW, SEEDING SHALL OCCUR PRIOR TO SEPTEMBER 15TH OF THE YEAR IN WHICH THE AREA BEING SEEDDED WAS DISTURBED;
- AREAS SEEDDED BETWEEN MAY 15TH TO AUGUST 15TH SHALL BE COVERED WITH HAY OR STRAW MULCH MEETING THE CRITERIA OF ENV-WO 1506.01(A) THROUGH (C); AND
- IF VEGETATED GROWTH COVERING AT LEAST 85% OF THE DISTURBED AREA IS NOT ACHIEVED PRIOR TO OCTOBER 15TH, ONE OR MORE ADDITIONAL EROSION CONTROL METHODS SHALL BE IMPLEMENTED.

BEST MANAGEMENT PRACTICES FOR BLASTING

ALL ACTIVITIES RELATING TO BLASTING SHALL FOLLOW BEST MANAGEMENT PRACTICES (BMP'S) TO PREVENT CONTAMINATION OF GROUNDWATER INCLUDING PREPARING, REVIEWING AND FOLLOWING AN APPROVED BLASTING PLAN; PROPER DRILLING, EXPLOSIVE HANDLING AND LOADING PROCEDURES; OBSERVING THE ENTIRE BLASTING PROCEDURE; EVALUATING BLASTING PERFORMANCE; AND HANDLING AND STORAGE OF BLASTED ROCK.

- LOADING PRACTICES.** THE FOLLOWING BLAST HOLE LOADING PRACTICES SHALL BE IMPLEMENTED:
- THE DRILLER SHALL MAINTAIN DRILLING LOGS TO DOCUMENT:
 - THE DEPTHS AND LENGTHS OF VOIDS, CAVITIES, AND FAULT ZONES OR OTHER WEAK ZONES ENCOUNTERED;
 - GROUNDWATER CONDITIONS;
 - THE DRILLER SHALL COMMUNICATE THE CONTENTS OF THE DRILLING LOGS DIRECTLY TO THE BLASTER;
 - EXPLOSIVE PRODUCTS SHALL BE MANAGED ON-SITE SUCH THAT THEY ARE:
 - USED IN THE BOREHOLE;
 - RETURNED TO THE DELIVERY VEHICLE; OR
 - PLACED IN SECURE CONTAINERS FOR OFF-SITE DISPOSAL;
 - SPILLAGE AROUND THE BOREHOLE SHALL BE:
 - PLACED IN THE BOREHOLE; OR
 - CLEANED UP AND RETURNED TO AN APPROPRIATE VEHICLE FOR HANDLING OR PLACEMENT IN SECURED CONTAINERS FOR OFF-SITE DISPOSAL;
 - LOADED EXPLOSIVES SHALL BE DETONATED AS SOON AS POSSIBLE AND NOT LEFT IN THE BLAST HOLES OVERNIGHT, UNLESS WEATHER OR OTHER SAFETY CONCERNS REASONABLY DICTATE THAT DETONATION SHOULD BE POSTPONED;
 - LOADING OPERATIONS SHALL BE CLEANED IN AN AREA WHERE WASTEWATER CAN BE PROPERLY CONTAINED AND HANDLED IN A MANNER THAT PREVENTS RELEASE OF CONTAMINANTS TO THE ENVIRONMENT; AND
 - EXPLOSIVES SHALL BE LOADED IN ACCORDANCE WITH INDUSTRY STANDARD PRACTICES FOR PRIMING, STEMMING, DECKING AND COLUMN RISE NEED TO BE ATTENDED TO MAINTAIN GOOD CONTINUITY IN THE COLUMN LOAD TO PROMOTE COMPLETE DETONATION.

EXPLOSIVE SELECTION: THE FOLLOWING BMP'S SHALL BE FOLLOWED TO REDUCE THE POTENTIAL FOR GROUNDWATER CONTAMINATION WHEN EXPLOSIVES ARE USED:

- EXPLOSIVE PRODUCTS SHALL BE SELECTED THAT ARE APPROPRIATE FOR SITE CONDITIONS AND SAFE BLAST EXECUTION.
- EXPLOSIVE PRODUCTS SHALL BE SELECTED THAT HAVE THE APPROPRIATE WATER RESISTANCE FOR THE SITE CONDITIONS PRESENT TO MINIMIZE THE POTENTIAL FOR HAZARDOUS EFFECT OF THE PRODUCT UPON GROUNDWATER.

PREVENTION OF MISFIRE: INDUSTRY-STANDARD PRACTICES SHALL BE IMPLEMENTED TO PREVENT MISFIRES.

MUCK AND ROCK MANAGEMENT. MUCK PILES (THE BLASTED PIECES OF ROCK) AND ROCK PILES SHALL BE MANAGED IN A MANNER TO REDUCE THE POTENTIAL FOR CONTAMINATION BY IMPLEMENTING THE FOLLOWING MEASURES:

- FOR PURPOSES OF THIS PART, THE FOLLOWING DEFINITIONS APPLY:
 - "BLASTED MATERIAL" MEANS ALL OF THE EARTH MATERIAL LOOSEENED AS A RESULT OF THE BLASTING;
 - "MUCK" MEANS THE BLASTED MATERIAL REMAINING AFTER THE ROCKS HAVE BEEN REMOVED; AND
 - "ROCKS" MEANS THE LARGER PIECES OF BLASTED MATERIAL THAT ARE SEPARATED FROM THE MUCK FOR USE ELSEWHERE, INCLUDING FOR FEEDSTOCK OF A ROCK CRUSHING OPERATION.
- MUCK SHALL BE REMOVED FROM THE BLAST AREA AS SOON AS REASONABLY POSSIBLE.
- ROCKS SHALL BE MANAGED SO AS TO PREVENT WATER SUPPLY WELLS OR SURFACE WATERS FROM BEING CONTAMINATED BY RUNOFF.

SPILL PREVENTION MEASURES AND SPILL MANAGEMENT. SPILL PREVENTION AND SPILL MITIGATION MEASURES SHALL BE IMPLEMENTED TO PREVENT THE RELEASE OF FUEL AND OTHER RELATED SUBSTANCES TO THE ENVIRONMENT. THE MEASURES SHALL INCLUDE AT A MINIMUM:

- FUEL AND OTHER REGULATED SUBSTANCES SHALL BE MANAGED AS REQUIRED BY ENV-WO 401.04.
- PERSONNEL WORKING AT THE BLAST SITE SHALL BE TRAINED IN HOW TO RESPOND TO A SPILL OF THE REGULATED SUBSTANCES BEING USED AT THE SITE.
- THE FUEL STORAGE REQUIREMENTS SHALL INCLUDE:
 - STORAGE OF REGULATED SUBSTANCES ON AN IMPERVIOUS SURFACE.
 - SECURE STORAGE AREAS AGAINST UNAUTHORIZED ENTRY.
 - LABEL REGULATED CONTAINERS CLEARLY AND VISIBLY.
 - INSPECT STORAGE AREAS WEEKLY.
 - COVER REGULATED CONTAINERS IN OUTSIDE STORAGE AREAS.
 - WHEREVER POSSIBLE, KEEP REGULATED CONTAINERS THAT ARE STORED OUTSIDE MORE THAN 50 FEET FROM SURFACE WATER AND STORM DRAINS, 75 FEET FROM PRIVATE WELLS, AND 400 FEET FROM PUBLIC WELLS.
 - SECONDARY CONTAINMENT IS REQUIRED FOR CONTAINERS CONTAINING REGULATED SUBSTANCES STORED OUTSIDE, EXCEPT FOR ON PREMISE USE HEATING FUEL TANKS, OR ABOVEGROUND OR UNDERGROUND STORAGE TANKS OTHERWISE REGULATED.
- THE FUEL HANDLING REQUIREMENTS SHALL INCLUDE:
 - EXCEPT WHEN IN USE, KEEP CONTAINERS CONTAINING REGULATED SUBSTANCES CLOSED AND SEALED.
 - PLACE DRIP PANS UNDER SPIGOTS, VALVES, AND PUMPS.
 - HAVE SPILL CONTROL AND CONTAINMENT EQUIPMENT READILY AVAILABLE IN ALL WORK AREAS.
 - USE FUNNELS AND DRIP PANS WHEN TRANSFERRING THE REGULATED SUBSTANCES.
 - PERFORM TRANSFERS OF REGULATED SUBSTANCES OVER AN IMPERVIOUS SURFACE.
 - THE TRAINING OF ON-SITE EMPLOYEES AND THE ON-SITE POSTING OF RELEASE RESPONSE INFORMATION DESCRIBING WHAT TO DO IN THE EVENT OF A SPILL OF REGULATED SUBSTANCES.
 - FUELING AND MAINTENANCE OF EXCAVATION, EARTHMOVING AND OTHER CONSTRUCTION RELATED EQUIPMENT WILL COMPLY WITH REGULATIONS OF THE STATE OF NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES (AS SUMMARIZED IN MD-DWBG-22-6 "BEST MANAGEMENT PRACTICES FOR FUELING AND MAINTENANCE OF EXCAVATION AND EARTHMOVING EQUIPMENT" OR ITS SUCCESSOR DOCUMENT.



TAX MAP 12 LOT 11

DETAIL SHEET 1
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055

OWNER: LAND BARGE, LLC
 754-756 ELM STREET
 MILFORD, NH 03055

APPLICANT: SOONER TRANSPORTATION LLC
 3 EXECUTIVE PARK DRIVE, SUITE 2106
 BEDFORD, NH 03110

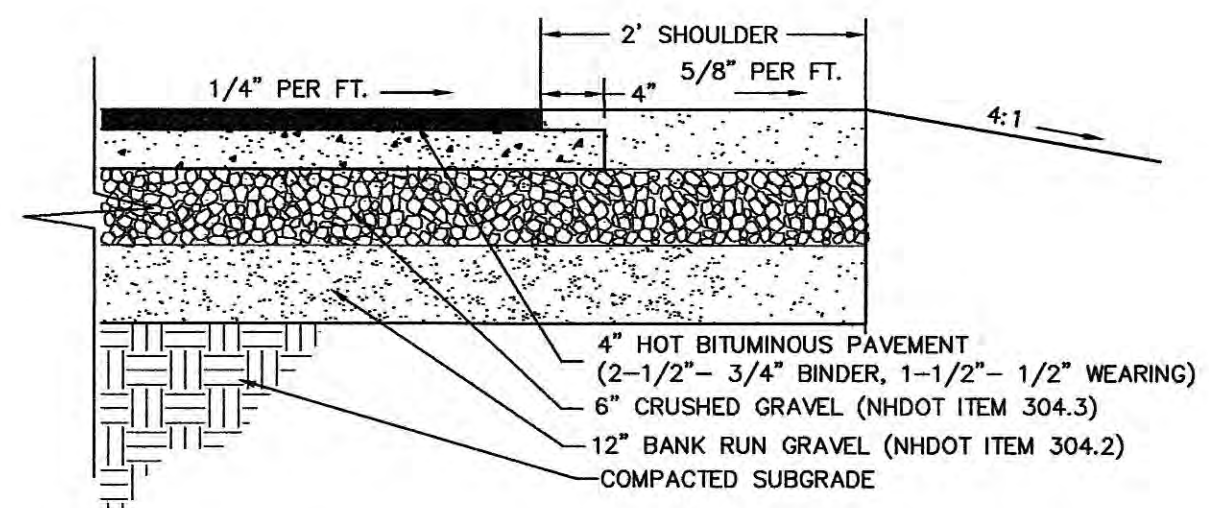
SCALE: 1" = 20' APRIL 25, 2022 SHEET 9 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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ENGINEERS AND SURVEYORS
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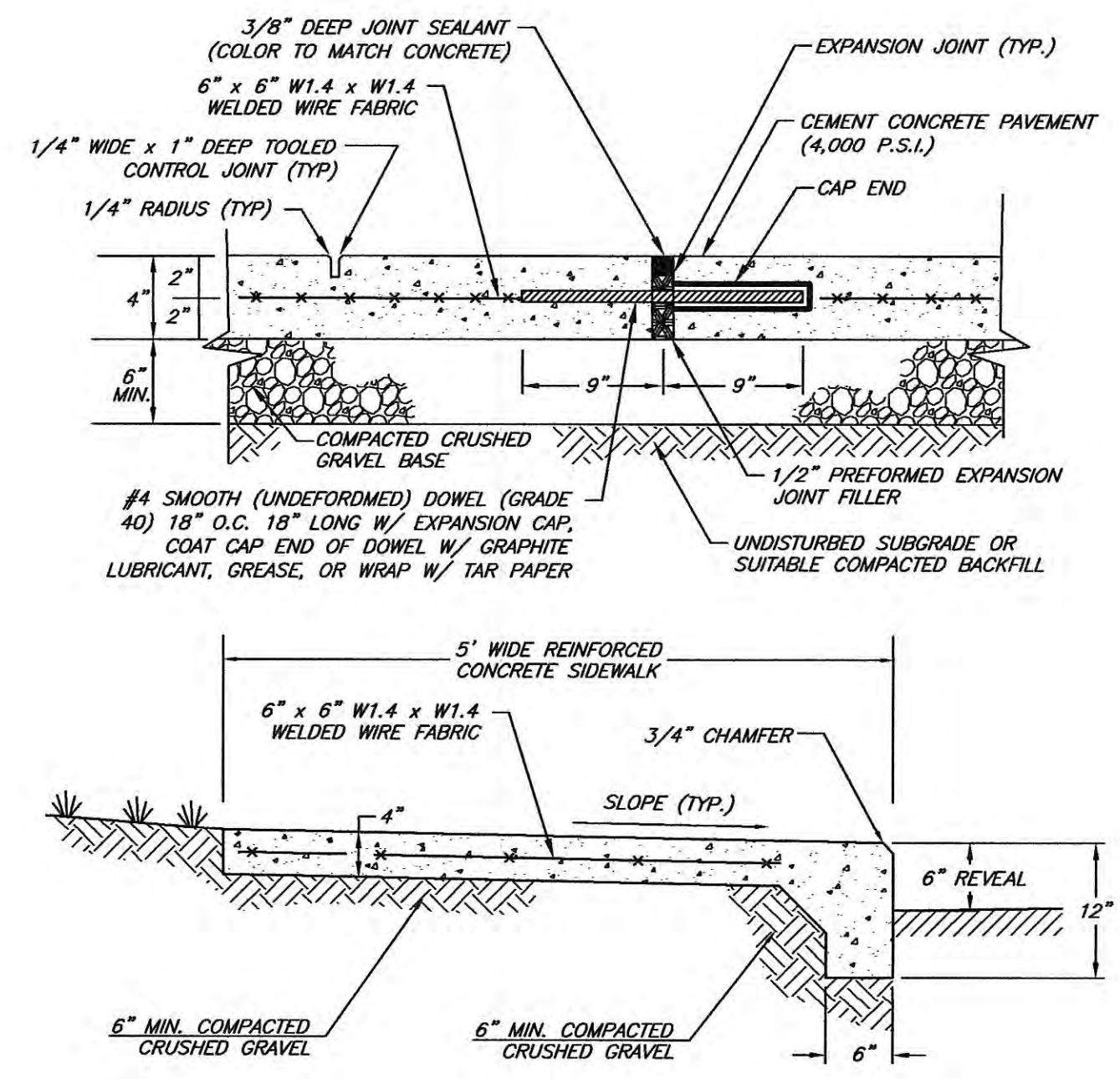
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B		
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A		
DATE	DESCRIPTION	BY	REV.		



NOTES:
 1. SECTION NUMBERS REFER TO APPROPRIATE SECTIONS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION.
 2. SECTION 410 (TACK COAT) WILL APPLY IF MORE THAN 180 CALENDAR DAYS ELAPSE BETWEEN PLACEMENT OF BINDER COURSE AND WEARING COURSE.
 3. PAVEMENT SECTION SHALL CONFORM TO LATEST NHDOT MANUAL AND TOWN OF AUBURN SITE PLAN REGULATIONS.

PAVEMENT DETAIL

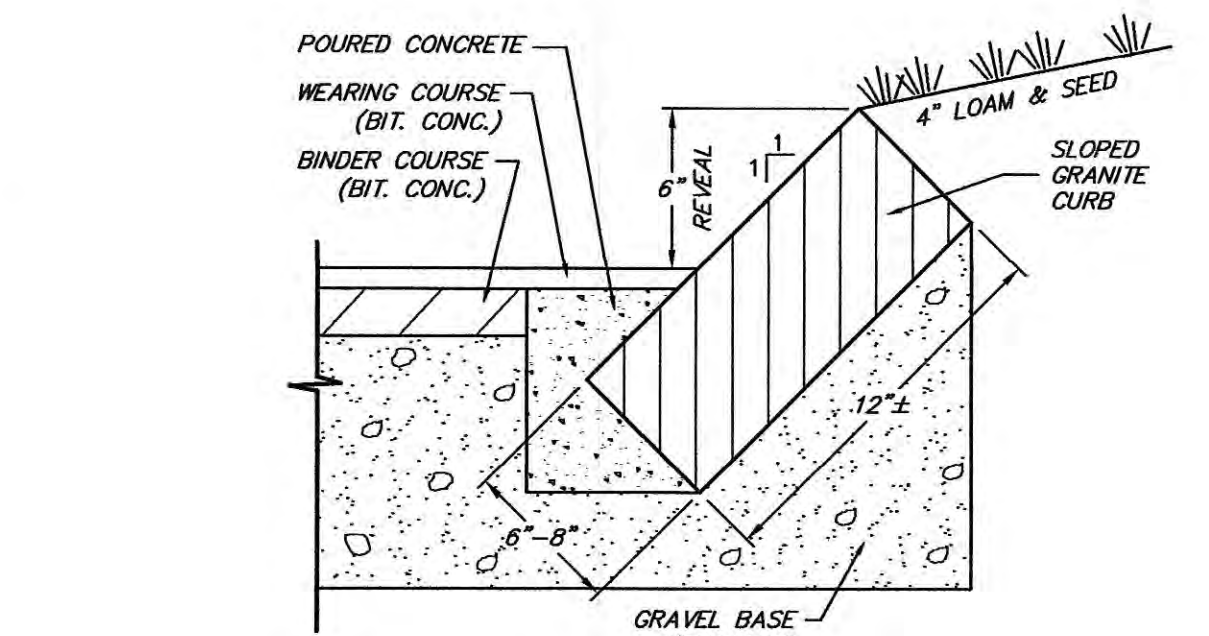
NOT TO SCALE



NOTES:
 1. CROSS SLOPE OF SIDEWALK TO BE AS SPECIFIED ON THE PLAN.
 2. MAINTAIN 2" CLEARANCE (TYP) BETWEEN ALL CONCRETE EDGES AND WIRE FABRIC OR DOWEL.
 3. CONTROL JOINTS TO BE LOCATED 5- FEET ON CENTER.
 4. EXPANSION JOINTS TO BE LOCATED 25- FEET ON CENTER.
 5. ALL CONCRETE TO BE 4,000 PSI NHDOT CLASS AA.

REINFORCED CONCRETE SIDEWALK DETAIL

NOT TO SCALE

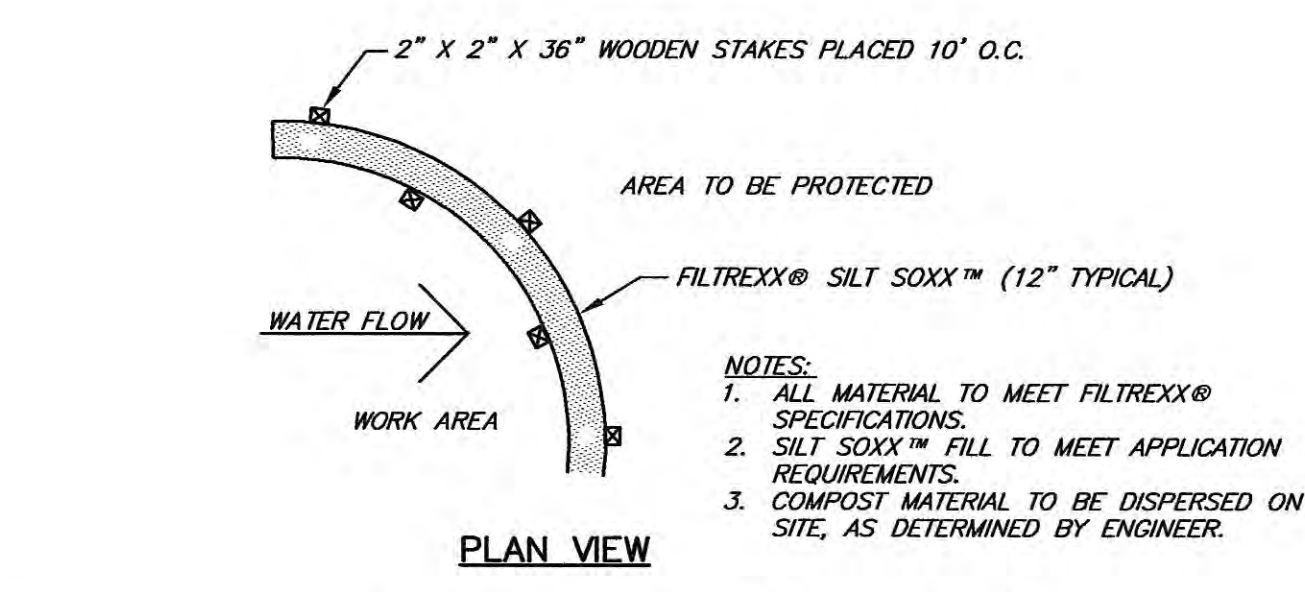
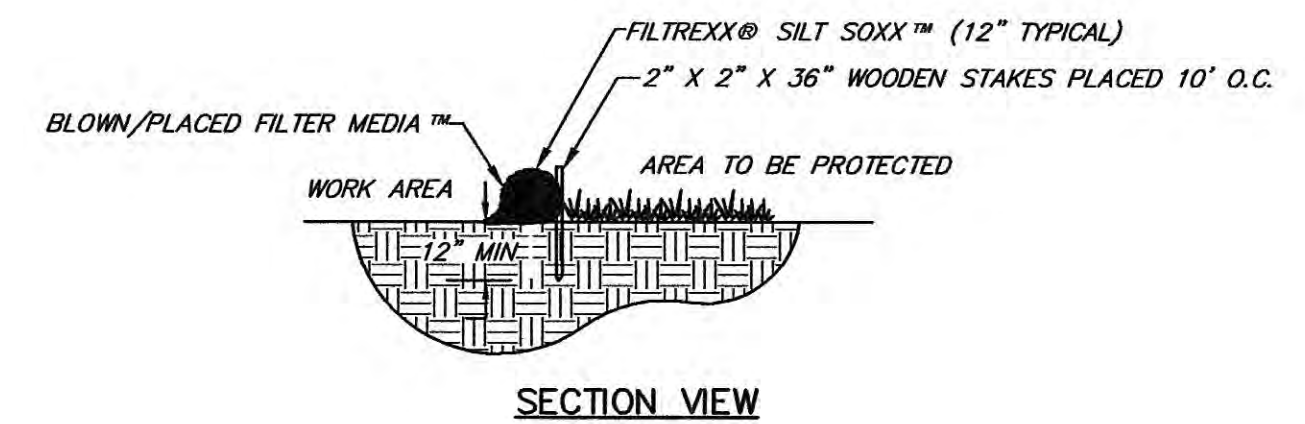


NOTES:
 MINIMUM LENGTH OF STRAIGHT CURB STONES = 1'
 MAXIMUM LENGTH OF STRAIGHT CURB STONES = 8'
 MINIMUM LENGTH OF STRAIGHT CURB STONES LAID ON CURVE = SEE CHART
 ADJOINING STONES SHALL HAVE THE SAME OR APPROXIMATELY THE SAME LENGTH

RADIUS FOR STONES WITH SQUARE JOINTS	MAXIMUM LENGTH
< 2'	USE CURVED CURB
2' - 15'	USE RADIAL JOINTS
16' - 28'	1'-6"
29' - 41'	2'
42' - 55'	3'
56' - 68'	4'
69' - 82'	5'
83' - 96'	6'
97' - 110'	7'
OVER 110'	8'

SLOPED GRANITE CURB DETAIL

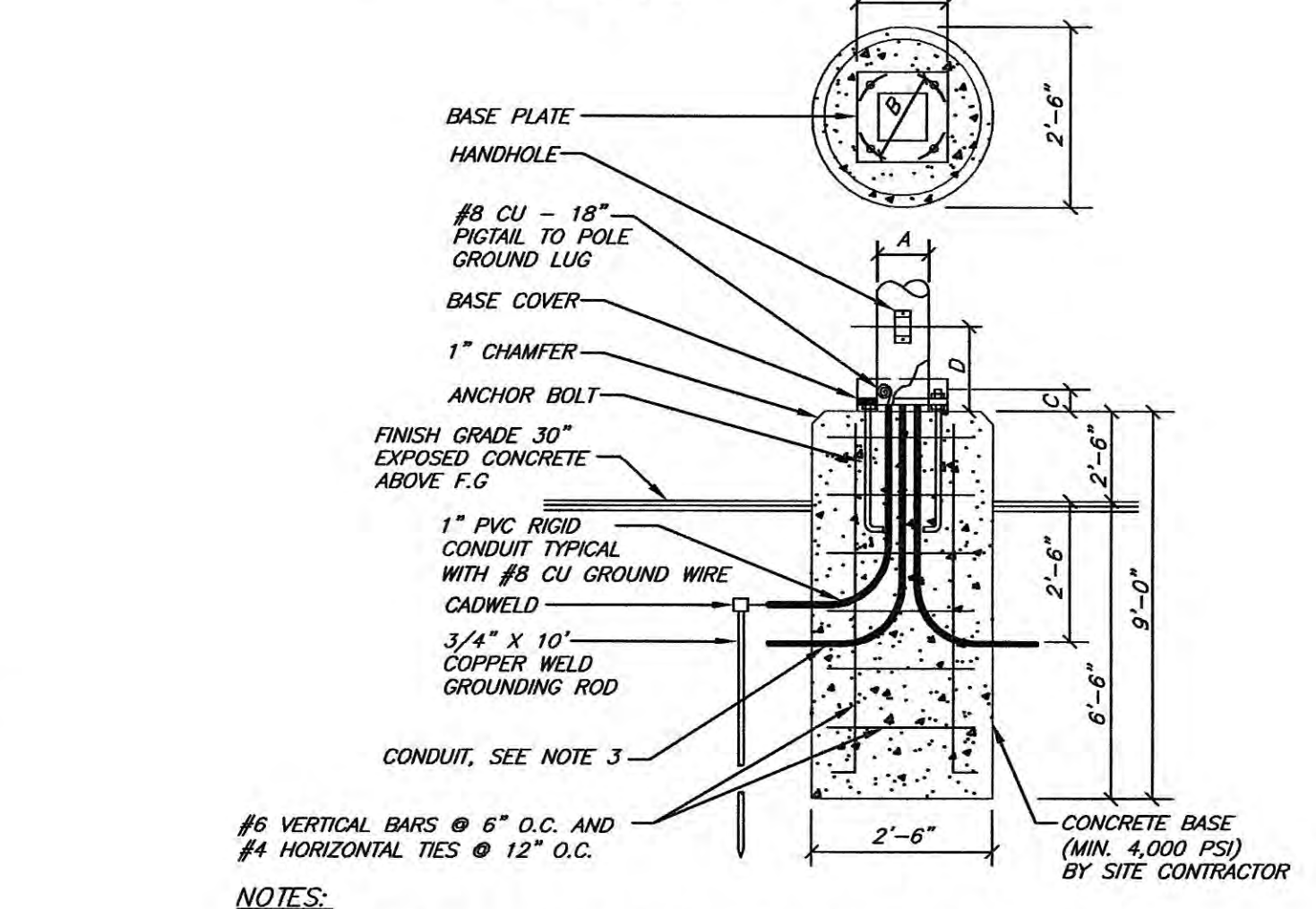
NOT TO SCALE



NOTES:
 1. ALL MATERIAL TO MEET FILTREXX® SPECIFICATIONS.
 2. SILT SOXX™ FILL TO MEET APPLICATION REQUIREMENTS.
 3. COMPOST MATERIAL TO BE DISPersed ON SITE, AS DETERMINED BY ENGINEER.

FILTREXX SILT SOXX DETAIL

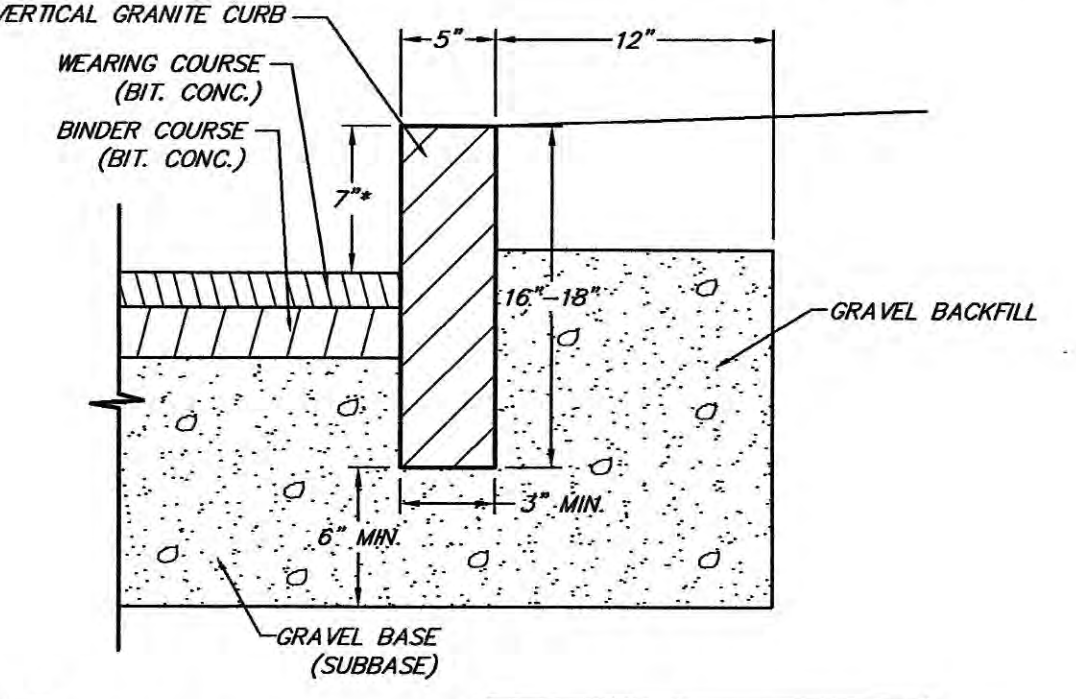
NOT TO SCALE



NOTES:
 1. REFER TO LIGHT POLE MANUFACTURER FOR A, B, C AND D DIMENSIONS AND ANCHOR BOLT SIZE.
 2. SEE ELECTRICAL SITE PLAN FOR CONDUITS AT EACH LOCATION.
 3. ALL CONDUIT INSIDE OF CONCRETE POLE BASES SHALL BE GALVANIZED RIGID STEEL AND ALL ELBOWS SHALL BE A MIN. 36" RADIUS.
 4. THE 2'-6" POLE BASE EXPOSURE IS INTENDED TO PREVENT DAMAGE TO LIGHT POLE FROM LOW SPEED VEHICLE IMPACT AND IS INTENDED FOR PARKING LOT USE ONLY. CONTRACTOR SHALL REVIEW AND VERIFY THIS FEATURE WITH THE OWNER PRIOR TO CONSTRUCTION.

LIGHT POLE BASE DETAIL

NOT TO SCALE

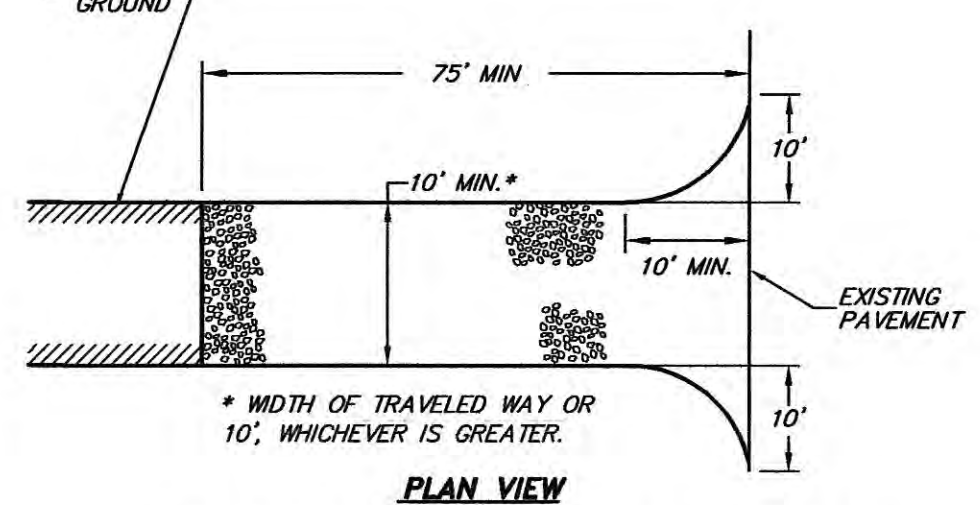
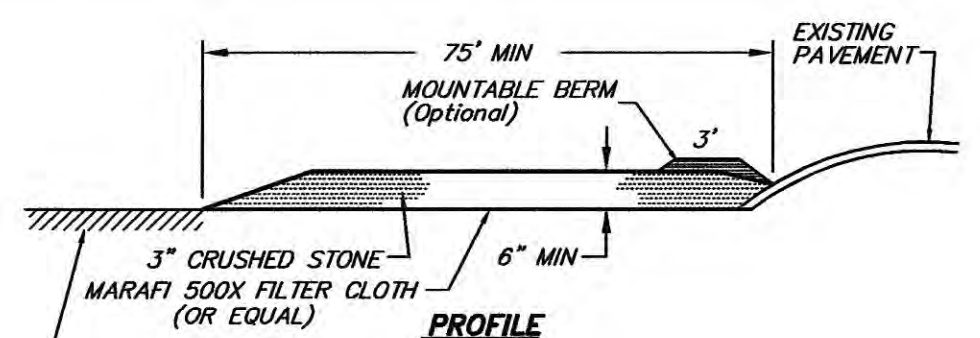


NOTES:
 MINIMUM LENGTH OF STRAIGHT CURB STONES = 3'
 MAXIMUM LENGTH OF STRAIGHT CURB STONES = 10'
 MINIMUM LENGTH OF STRAIGHT CURB STONES LAID ON CURVE = SEE CHART
 ADJOINING STONES SHALL HAVE THE SAME OR APPROXIMATELY THE SAME LENGTH
 * 2" AT DRIVEWAYS AND 0" AT SIDEWALK RAMPS

RADIUS	MAXIMUM LENGTH
< 21'	USE CURVED CURB
22' - 28'	3'
29' - 35'	4'
36' - 42'	5'
43' - 49'	6'
50' - 56'	7'
57' - 60'	8'
OVER 60'	10'

VERTICAL GRANITE CURB DETAIL

NOT TO SCALE

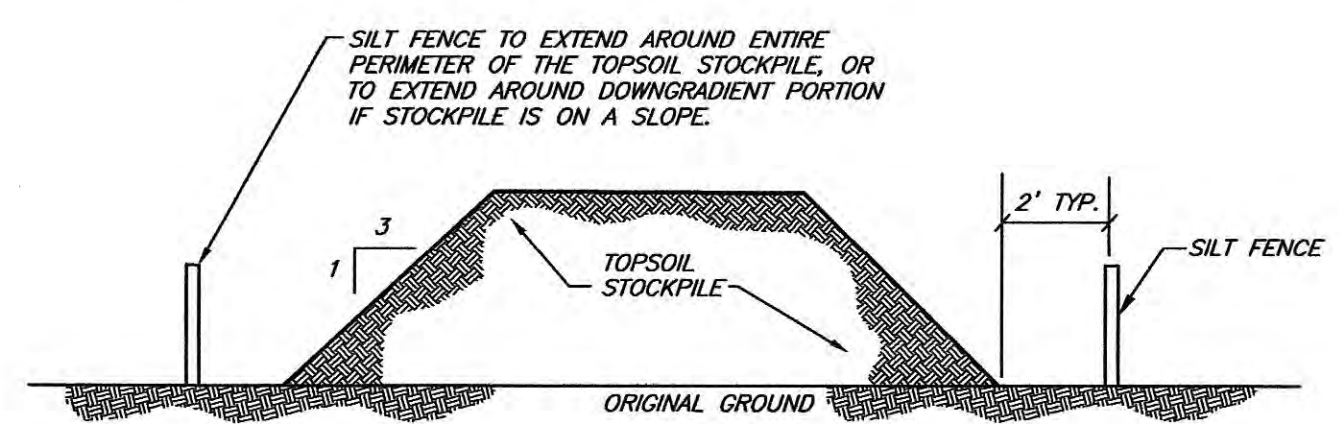


STABILIZED CONSTRUCTION EXIT:

NOT TO SCALE

MAINTENANCE NOTES:
 MUD AND SOIL PARTICLES WILL EVENTUALLY CLOG THE VOIDS IN THE GRAVEL AND THE EFFECTIVENESS OF THE GRAVEL PAD WILL NOT BE SATISFACTORY. WHEN THIS OCCURS, THE PAD SHOULD BE TOPDRESSED WITH NEW STONE. COMPLETE REPLACEMENT OF THE PAD MAY BE NECESSARY WHEN THE PAD BECOMES COMPLETELY CLOGGED.
 IF WASHING FACILITIES ARE USED, THE SEDIMENT TRAPS SHOULD BE CLEANED OUT AS OFTEN AS NECESSARY TO ASSURE THAT ADEQUATE TRAPPING EFFICIENCY AND STORAGE VOLUME IS AVAILABLE. VEGETATIVE FILTER STRIPS SHOULD BE MAINTAINED TO INSURE A VIGOROUS STAND OF VEGETATION AT ALL TIMES.

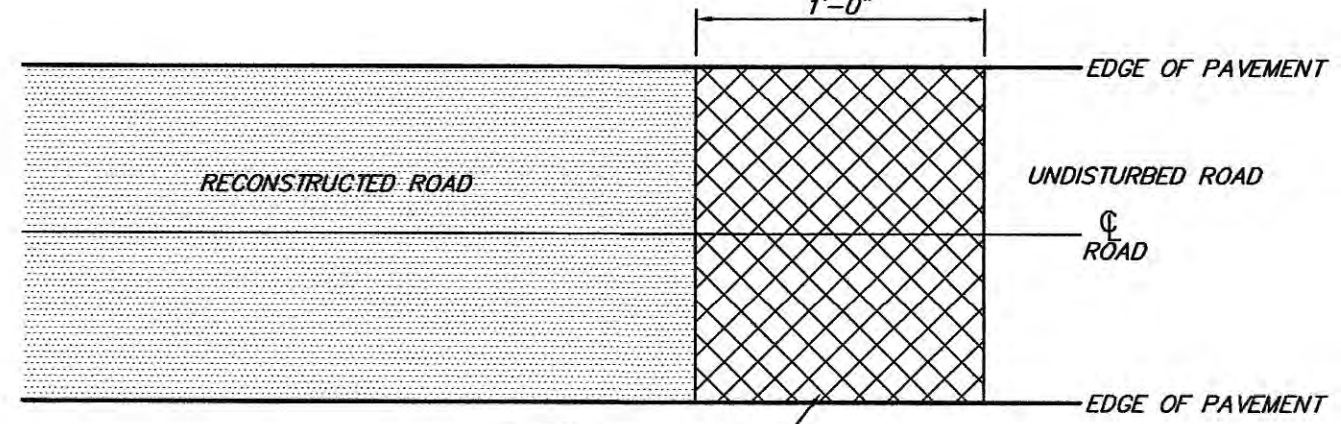
CONSTRUCTION SPECIFICATION
 (A) THE MINIMUM STONE USED SHALL BE 3-INCH CRUSHED STONE;
 (B) THE MINIMUM LENGTH OF THE PAD SHALL BE 75 FEET, EXCEPT THAT THE MINIMUM LENGTH MAY BE REDUCED TO 50 FEET IF A 3-INCH TO 6-INCH HIGH BERM IS INSTALLED AT THE ENTRANCE OF THE PROJECT SITE;
 (C) THE PAD SHALL EXTEND THE FULL WIDTH OF THE CONSTRUCTION ACCESS ROAD OR 10 FEET, WHICHEVER IS GREATER;
 (D) THE PAD SHALL SLOPE AWAY FROM THE EXISTING ROADWAY;
 (E) THE PAD SHALL BE AT LEAST 6 INCHES THICK;
 (F) A GEOTEXTILE FILTER FABRIC SHALL BE PLACED BETWEEN THE STONE PAD AND THE EARTH SURFACE BELOW THE PAD; AND
 (G) THE PAD SHALL BE MAINTAINED OR REPLACED WHEN MUD AND SOIL PARTICLES CLOG THE VOIDS IN THE STONE SUCH THAT MUD AND SOIL PARTICLES ARE TRACKED OFF-SITE. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT INTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED PROMPTLY.
 (H) ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARD THE CONSTRUCTION ENTRANCE SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A BERM WITH 3:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.



NOTES:
 1. AN ON-SITE DRAINAGE SWALE SHALL BE LOCATED BETWEEN THE TOPSOIL STOCKPILE AND OFF-SITE PROPERTY.
 2. REFERENCE IS MADE TO SILT FENCE DETAIL FOR MATERIALS AND INSTALLATION METHODS.
 3. IF THE STOCKPILE IS TO REMAIN FOR MORE THAN 14 DAYS, IT SHALL BE STABILIZED WITH EROSION CONTROL MATTING OR SEEDED WITHIN 7 DAYS OF COMPLETION TO MINIMIZE EROSION.
 4. INSPECTION OF SILT FENCES SHALL BE AT LEAST ONCE PER WEEK AND AFTER RAINFALL EVENTS IN EXCESS OF 0.5 INCHES. REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 5. SEDIMENT TRAPPED BY THE FENCES SHALL BE REMOVED AND PROPERLY DISPOSED OF WHENEVER SIGNIFICANT ACCUMULATION OCCURS.
 6. SILT FENCES SHALL BE MAINTAINED IN PLACE UNTIL TOPSOIL STOCKPILE HAS BEEN ELIMINATED AND SHALL BE REMOVED ONLY WHEN DIRECTED BY THE TOWN.

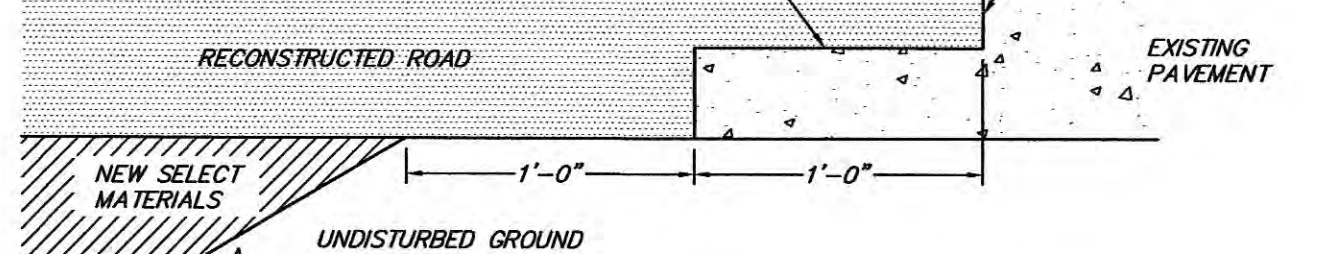
TEMPORARY STOCKPILE DETAIL

NOT TO SCALE



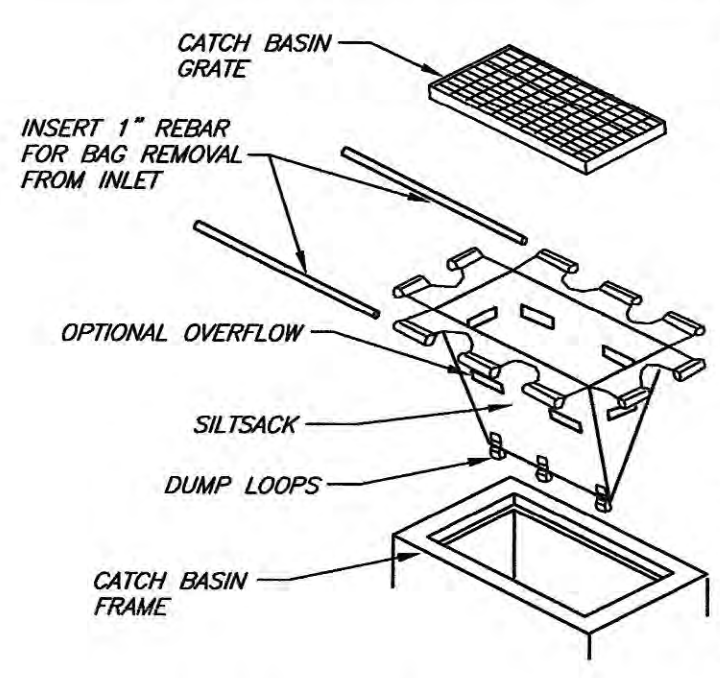
PAVEMENT CONNECTION

NOT TO SCALE



DETAIL OF END SECTION

NOT TO SCALE

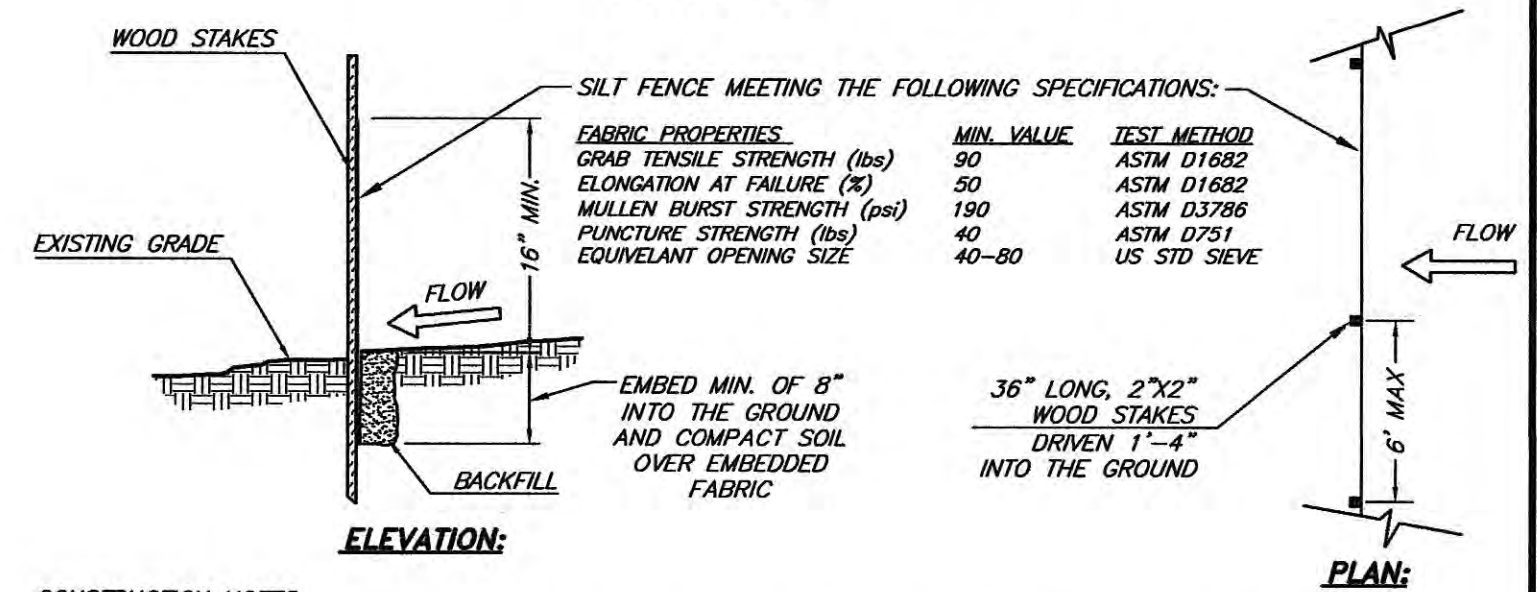


NOTES:
 1. INLET BASKETS SHALL BE USED ON ALL CATCH BASINS DURING CONSTRUCTION. INLET BASKETS SHALL BE "SILTSACK" OR APPROVED EQUAL.
 2. THE FILTER FABRIC SHALL BE A WOVEN POLYPROPYLENE GEO-TEXTILE FABRIC MEETING THE FOLLOWING SPECIFICATIONS:
 GRAB TENSILE STRENGTH: 300 LBS. MINIMUM (ASTM D-4632)
 GRAB ELONGATION: 20% MINIMUM (ASTM D-4632)
 PUNCTURE: 120 LBS. MINIMUM (ASTM D-4633)
 TRAPEZOID TEAR STRENGTH: 120 LBS. MINIMUM (ASTM D-4533)
 MULLEN BURST STRENGTH: MINIMUM 800 PSI (ASTM D-3786)
 3. THE FABRIC SHALL HAVE AN OPENING NO GREATER THAN A NUMBER 40 U.S. STANDARD SIEVE AND MINIMUM PERMEABILITY OF 40 GPM/SQ. FT.
 4. THE INLET BASKET SHALL BE INSPECTED WITHIN 24 HOURS AFTER EACH RAINFALL OR DAILY DURING EXTENDED PERIODS OF PRECIPITATION. REPAIRS SHALL BE MADE IMMEDIATELY, AS NECESSARY, TO PREVENT PARTICLES FROM ENTERING THE DRAINAGE PIPING SYSTEM AND/OR CAUSING SURFACE FLOODING.
 5. INLET BASKET SHALL BE MAINTAINED IN PLACE UNTIL ALL PAVING IS COMPLETED AND ALL UNPAVED AREAS HAVE BEEN STABILIZED WITH VEGETATION.

INSTALLATION AND MAINTENANCE NOTES:
 TO INSTALL THE SILTSACK® IN THE CATCH BASIN, REMOVE THE GRATE AND PLACE THE SACK IN THE OPENING. HOLD APPROXIMATELY SIX INCHES OF THE SACK OUTSIDE THE FRAME. THIS IS THE AREA OF THE LIFTING STRAPS. REPLACE THE GRATE TO HOLD THE SACK IN PLACE.
 THE SILTSACK® IS FULL AND SHOULD BE EMPTIED WHEN THE YELLOW RESTRAINT CORD IS NO LONGER VISIBLE.
 TO REMOVE THE SILTSACK®, TAKE TWO PIECES OF 1" DIAMETER REBAR AND PLACE THROUGH THE LIFTING LOOPS ON EACH SIDE OF THE SACK TO FACILITATE THE LIFTING OF THE SILTSACK®.
 TO EMPTY THE SILTSACK®, PLACE IT WHERE THE CONTENTS WILL BE COLLECTED. PLACE THE REBAR THROUGH THE LIFT STRAPS (CONNECTED TO THE BOTTOM OF THE SACK) AND LIFT. THIS WILL TURN THE SILTSACK® INSIDE OUT AND EMPTY THE CONTENTS. CLEAN OUT AND RINSE. RETURN THE SILTSACK® TO ITS ORIGINAL SHAPE AND PLACE BACK IN THE BASIN.
 THE SILTSACK® IS REUSABLE. ONCE THE CONSTRUCTION CYCLE IS COMPLETE, REMOVE THE SILTSACK® FROM THE BASIN AND CLEAN. THE SILTSACK® SHOULD BE STORED OUT OF THE SUNLIGHT WHEN NOT IN USE.

INLET SEDIMENT FILTER BASKET

NOT TO SCALE



CONSTRUCTION NOTES:
 A. FENCES SHALL BE USED IN AREAS WHERE EROSION WILL OCCUR ONLY IN THE FORM OF SHEET EROSION AND THERE IS NO CONCENTRATION OF WATER IN A CHANNEL OR OTHER DRAINAGE WAY ABOVE THE FENCE;
 B. THE MAXIMUM CONTRIBUTING DRAINAGE AREA ABOVE THE FENCE SHALL BE LESS THAN 1/4-ACRE PER 100 LINEAR FEET OF FENCE;
 C. THE MAXIMUM LENGTH OF THE SLOPE ABOVE THE FENCE SHALL BE 100 FEET;
 D. THE MAXIMUM SLOPE OF THE AREA ABOVE THE FENCE SHALL BE 2:1;
 E. FENCES SHALL BE INSTALLED AS FOLLOWS:
 1. FENCES SHALL FOLLOW THE CONTOUR OF THE LAND AS CLOSELY AS POSSIBLE;
 2. THE ENDS OF THE FENCE SHALL BE FLARED UP-SLOPE;
 3. THE BASE OF THE FENCE SHALL BE:
 a. FOLDED SUCH THAT NOT LESS THAN 4 INCHES OF THE FENCE IS PLACED ALONG THE BOTTOM OF A TRENCH THAT IS EXCAVATED AT LEAST 4 INCHES DEEP INTO THE GROUND, WITH THE SOIL COMPACTED OVER THE EMBEDDED FABRIC; OR
 b. IF SITE CONDITIONS INCLUDE FROZEN GROUND, LEDGE, OR THE PRESENCE OF HEAVY ROOTS, EMBEDDED IN A MINIMUM THICKNESS OF 8 INCHES OF 3/4-INCH STONE;
 4. SUPPORT POSTS SHALL BE SIZED AND ANCHORED ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS; AND
 5. ADJOINING SECTIONS OF THE FENCE SHALL BE OVERLAPPED BY 6 INCHES, FOLDED AND STAPLED TO A SUPPORT POST;
 F. FENCES SHALL BE INSPECTED AND MAINTAINED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REPAIRS THAT ARE REQUIRED SHALL BE MADE IMMEDIATELY; AND
 G. SEDIMENT THAT ACCUMULATES AT THE FENCE SHALL BE REMOVED WITH SUFFICIENT FREQUENCY TO PREVENT THE DEPTH OF THE SEDIMENT FROM REACHING ONE-THIRD THE HEIGHT OF THE FENCE;
 H. INSTALL FENCE PER MANUFACTURER'S SPECIFICATIONS.
 I. IF THE FABRIC ON THE SILT FENCE SHOULD DECOMPOSE OR BECOME INEFFECTIVE DURING THE LIFE OF THE FENCE, THE FABRIC SHALL BE PROMPTLY REPLACED.
 J. SEDIMENT DEPOSITS THAT ARE REMOVED OR LEFT IN PLACE AFTER THE BARRIER HAS BEEN DISMANTLED SHALL BE GRADED TO CONFORM WITH THE EXISTING TOPOGRAPHY AND VEGETATED USING THE APPROPRIATE VEGETATIVE BMP.

SILT FENCE DETAIL:

NOT TO SCALE



TAX MAP 12 LOT 11
DETAIL SHEET 2
SOONER-MILFORD
 LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055
 OWNER: LAND BARGE, LLC
 754-756 ELM STREET
 MILFORD, NH 03055
 APPLICANT: SOONER TRANSPORTATION LLC
 3 EXECUTIVE PARK DRIVE, SUITE 2016
 BEDFORD, NH 03110

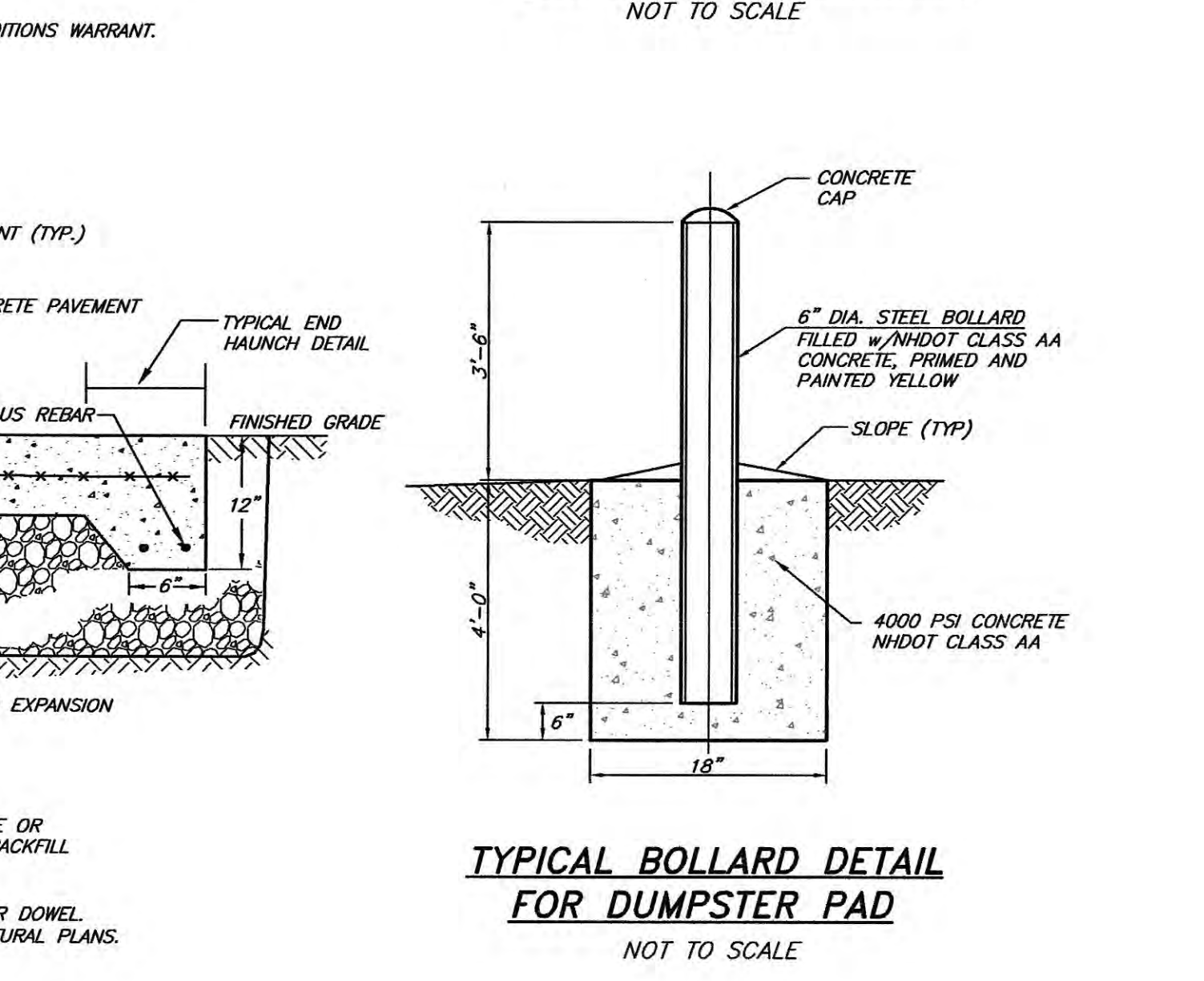
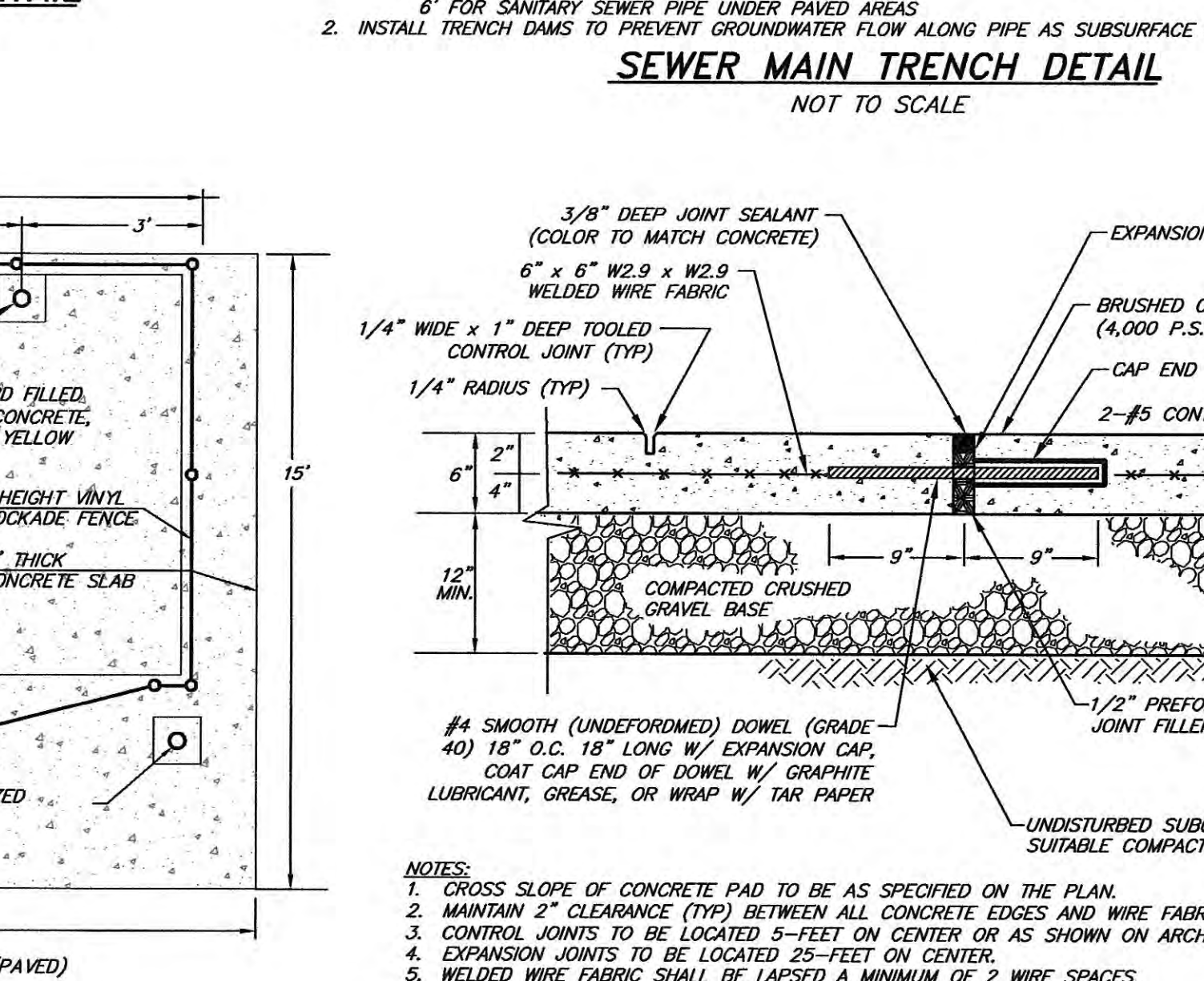
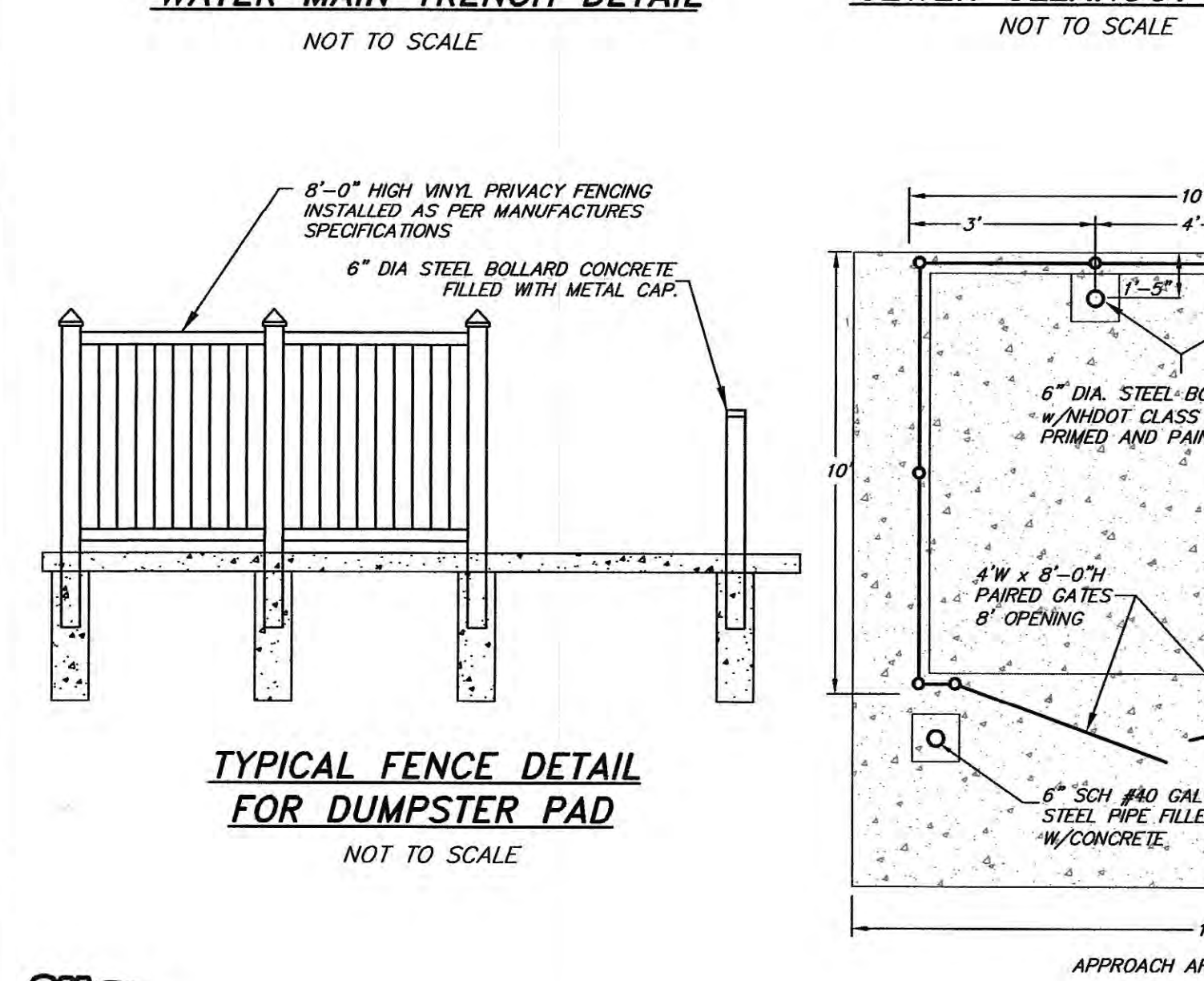
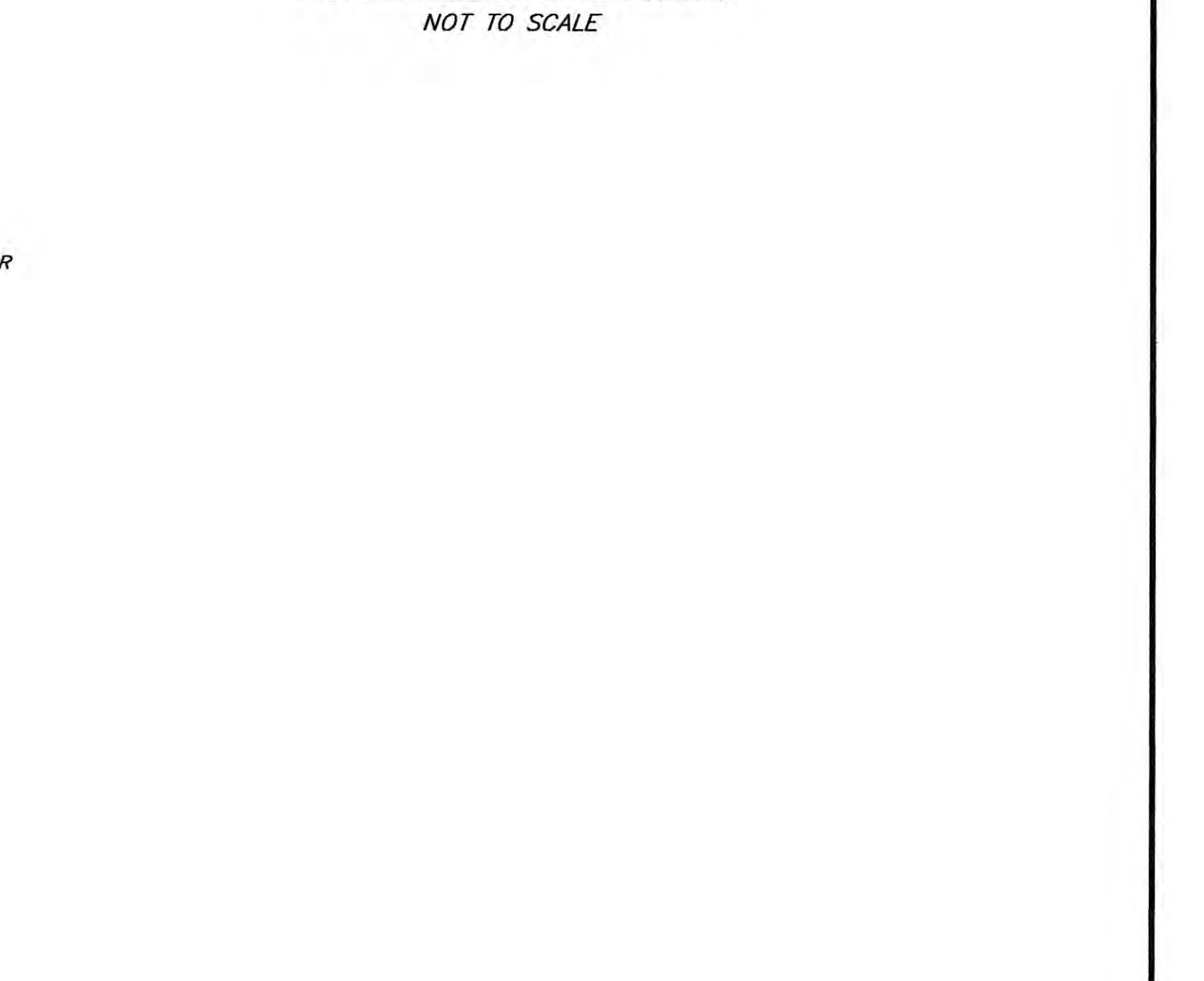
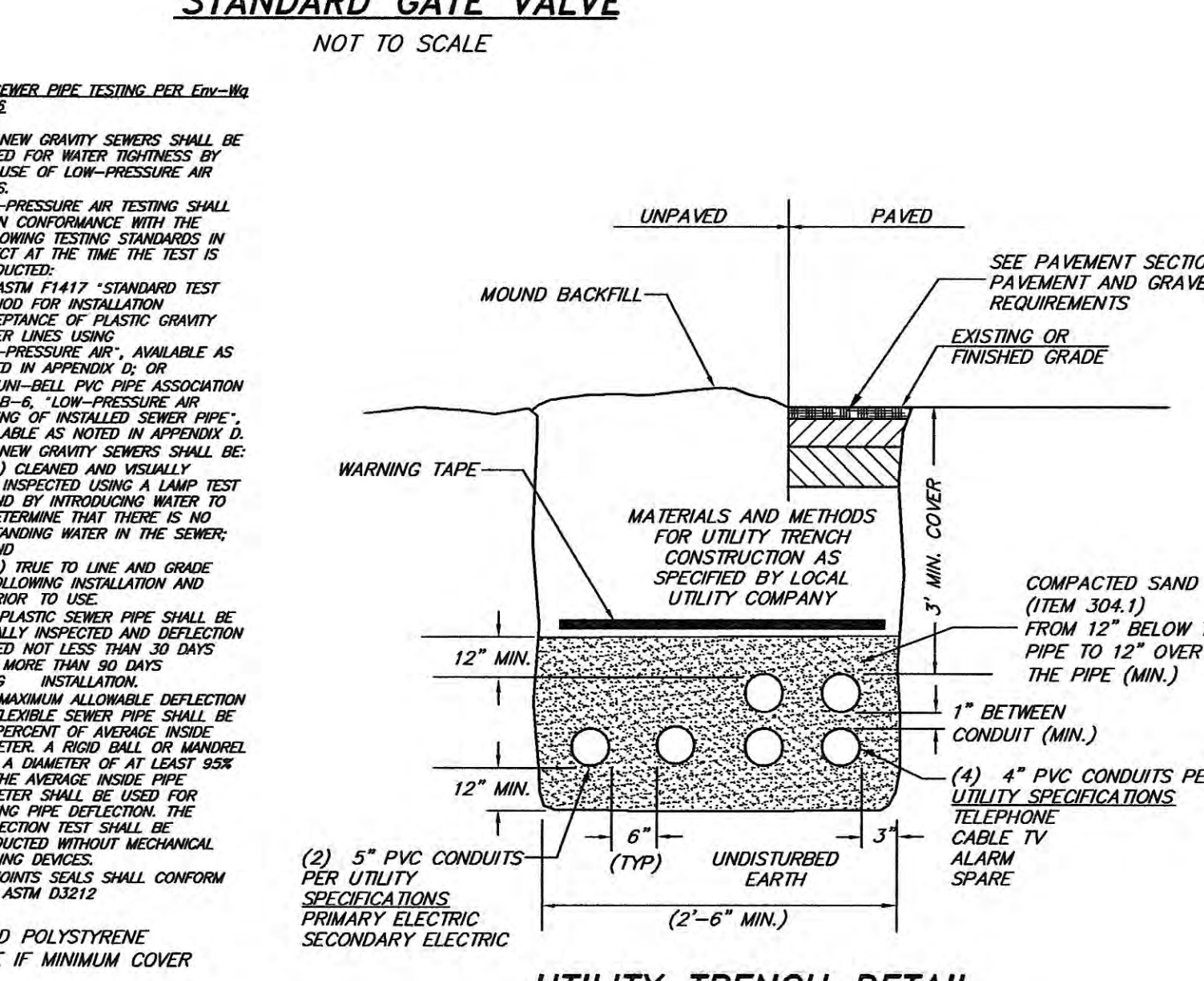
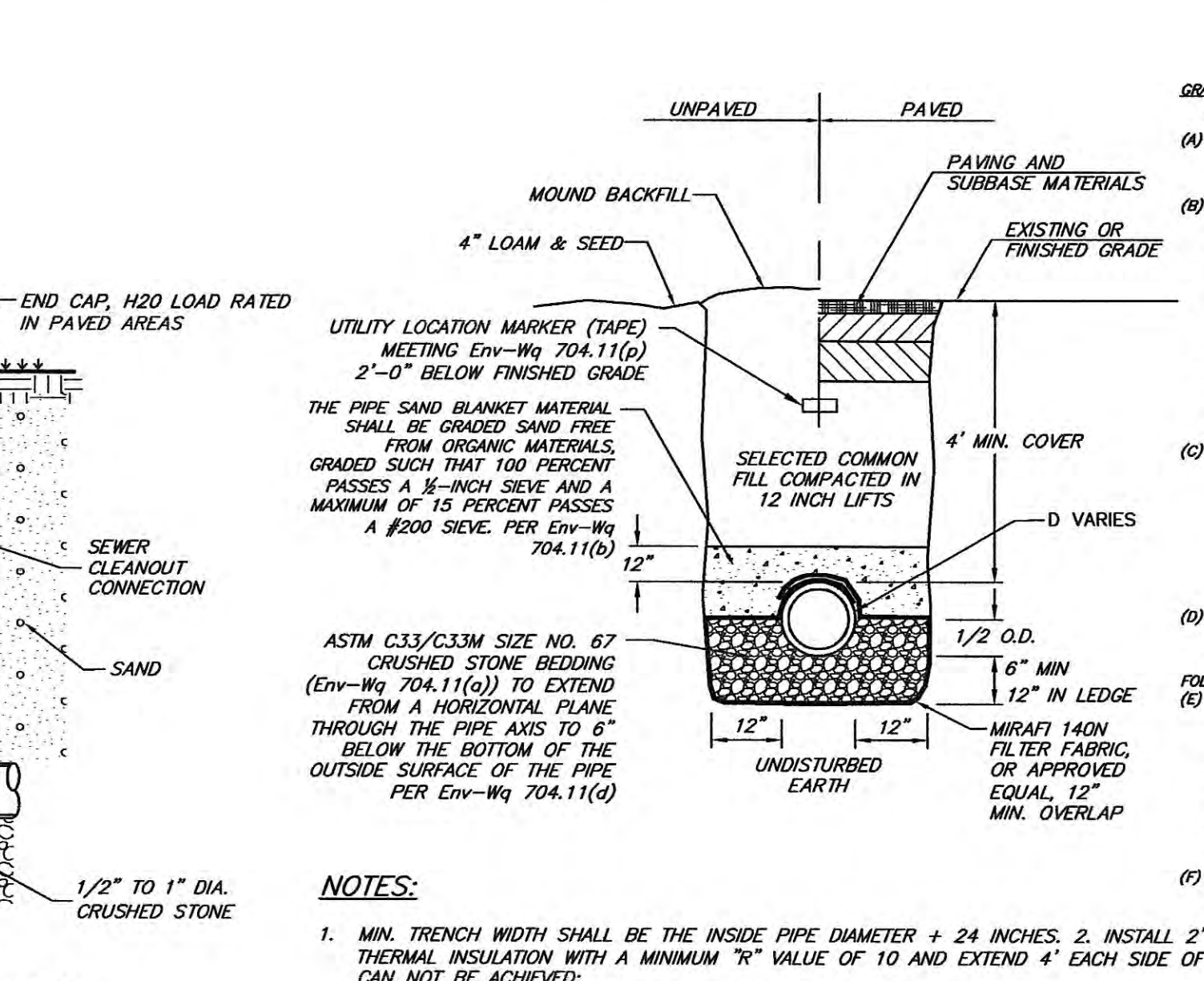
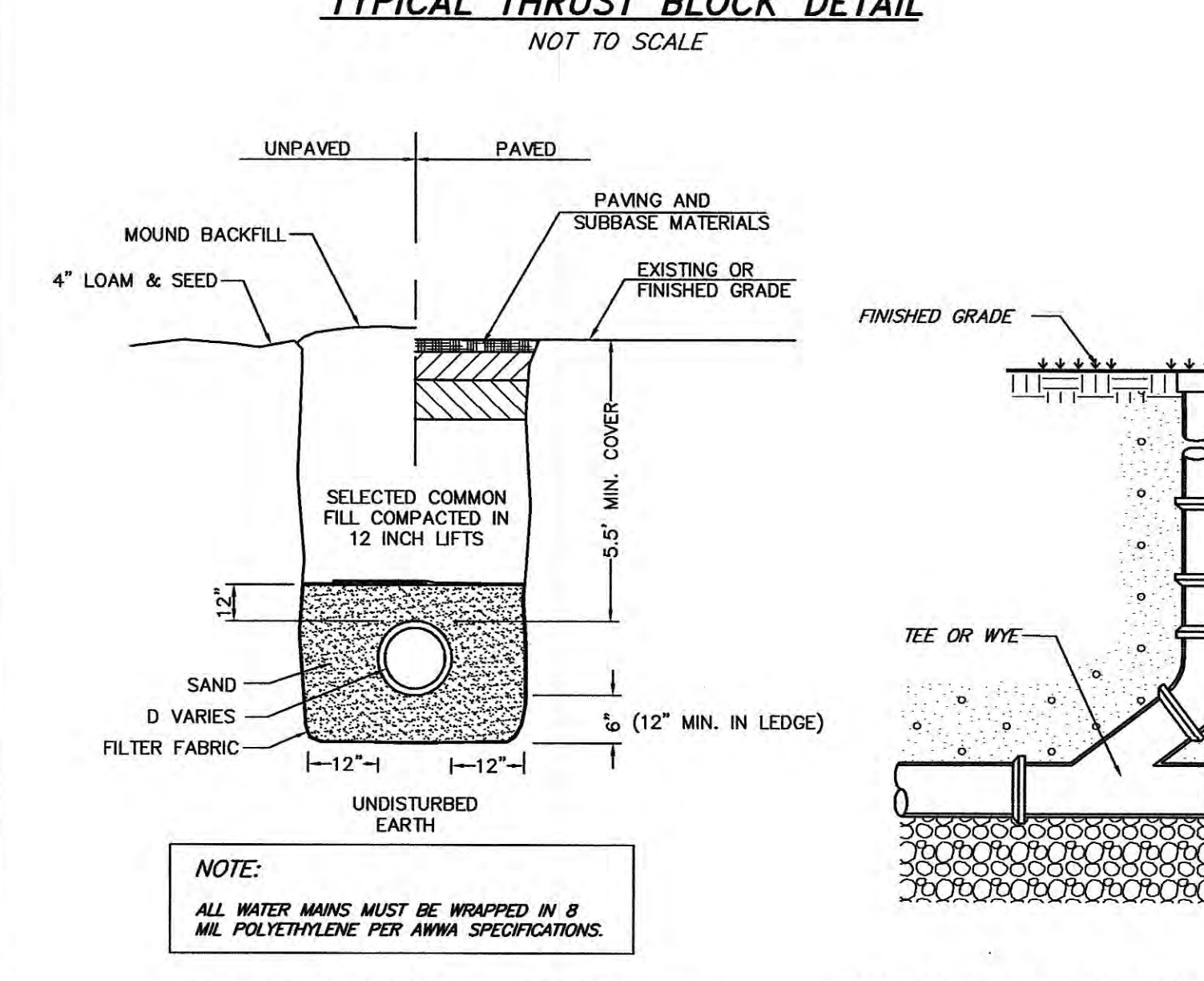
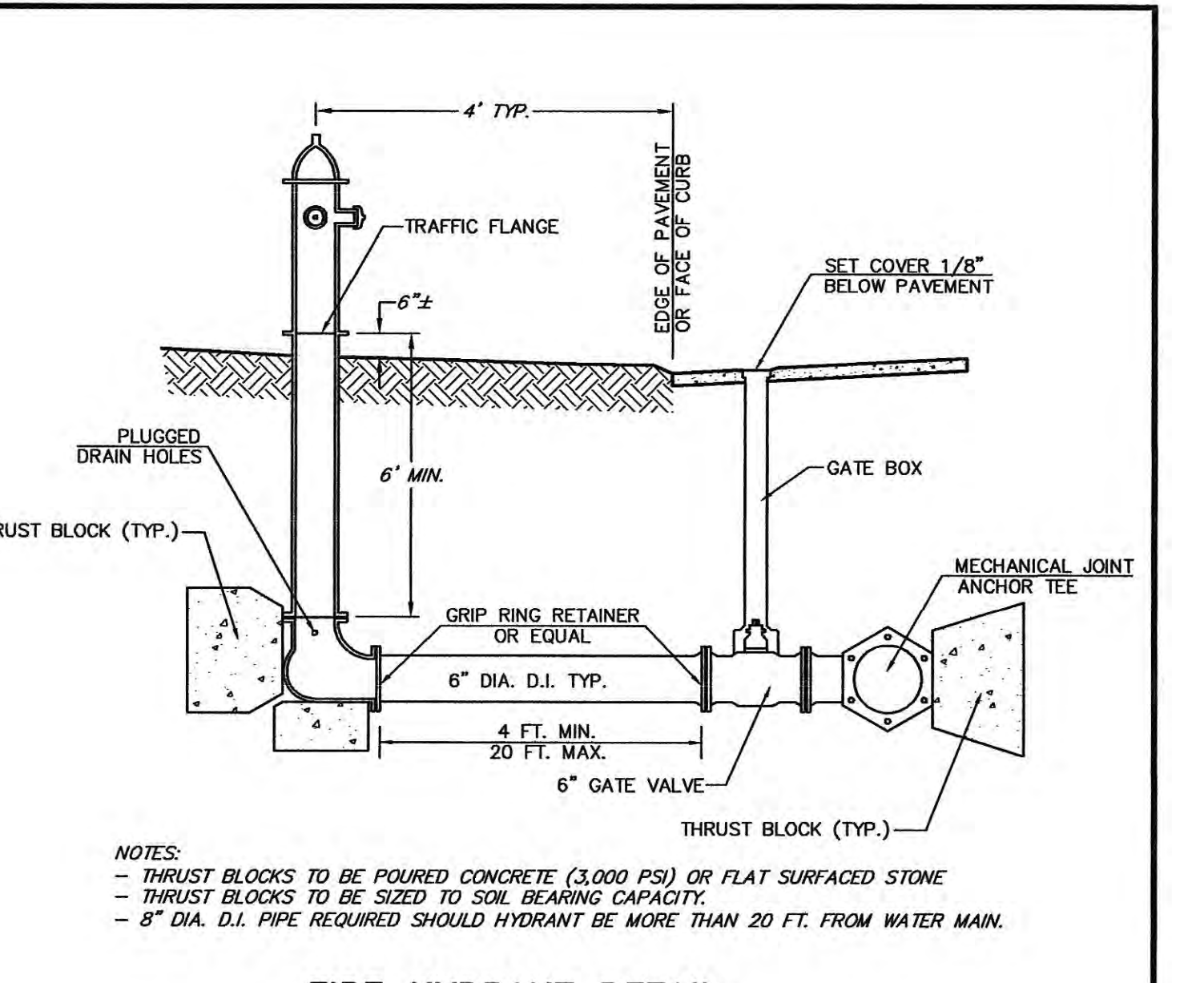
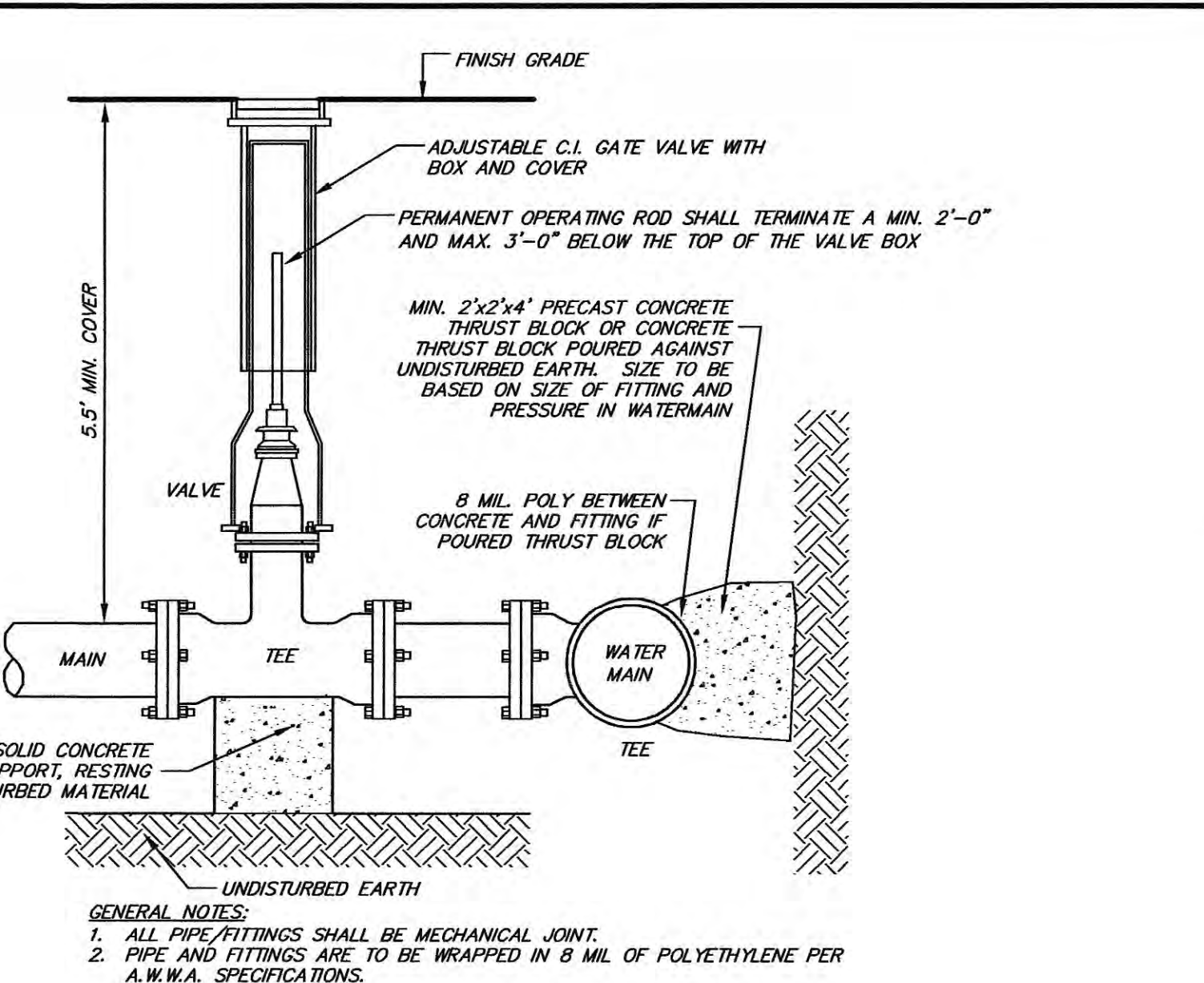
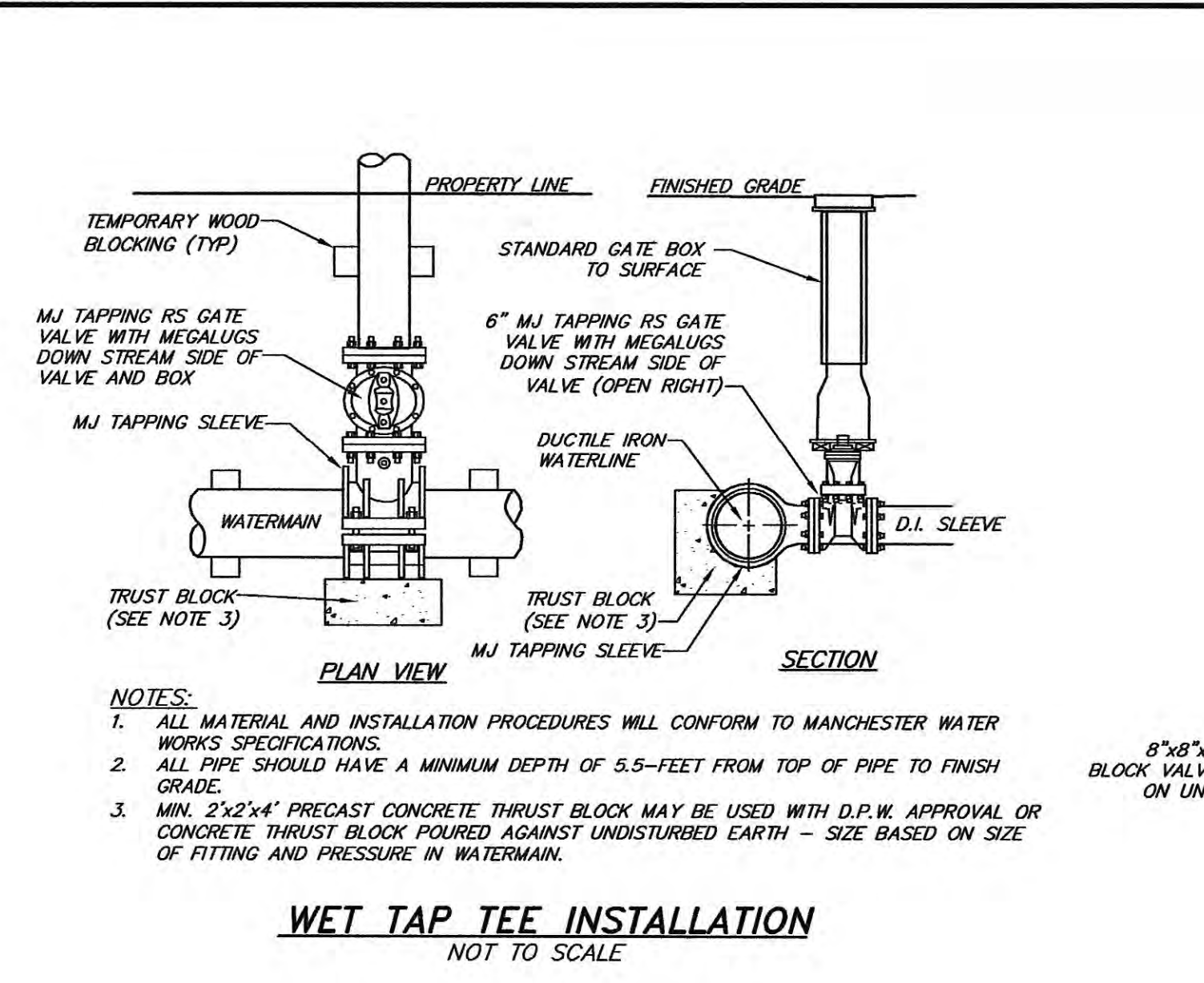
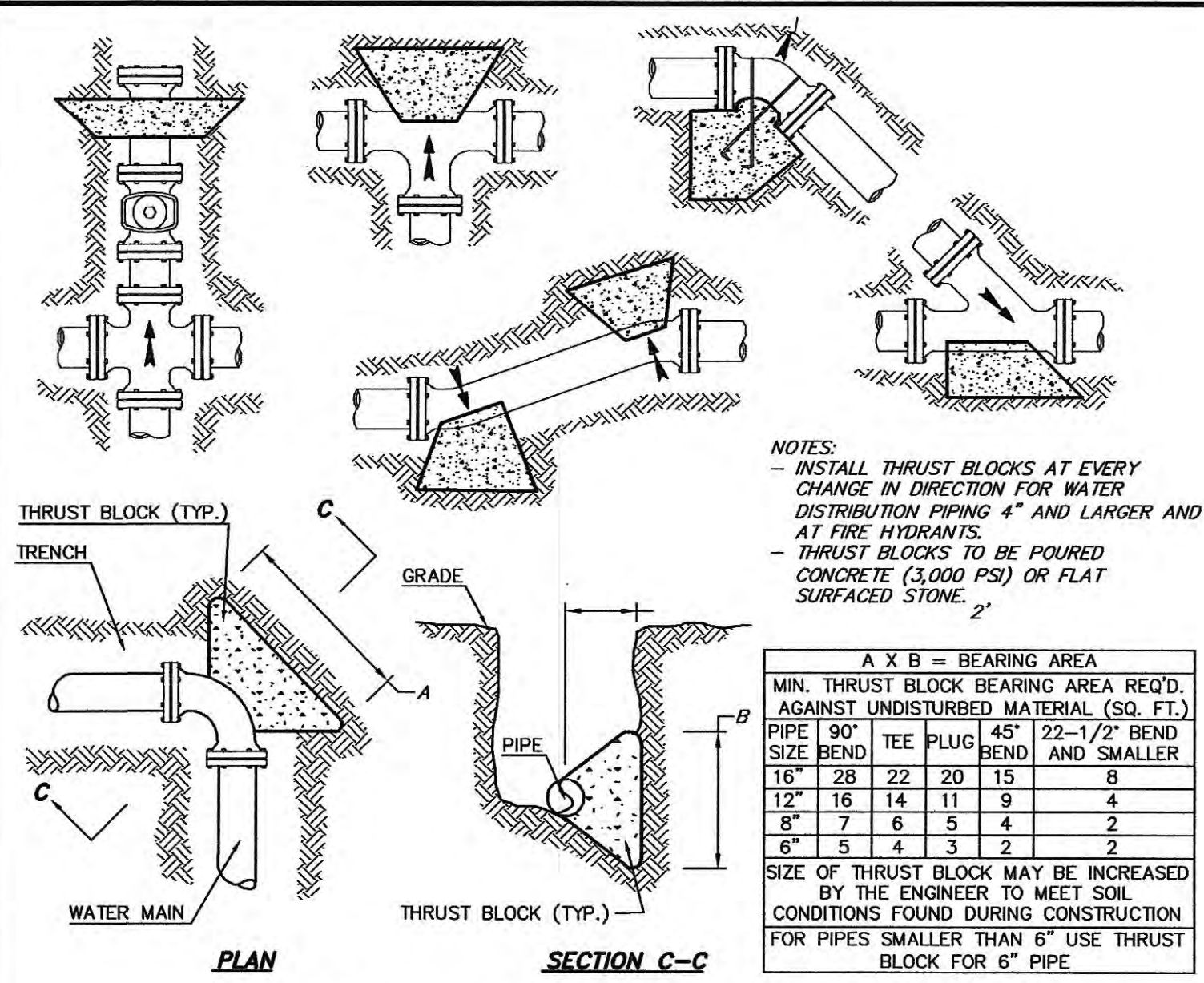
SCALE: 1" = 20'
 APRIL 25, 2022
 SHEET 10 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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Bedford Design Consultants Inc.
ENGINEERS AND SURVEYORS
 592 Harvey Road, Manchester, NH 03103
 Telephone: (603) 622-5533
 www.bedforddesign.com



DATE	DESCRIPTION	BY	REV.
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A



TAX MAP 12 LOT 11

DETAIL SHEET 3
SOONER-MILFORD
LOCATED AT:
754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055

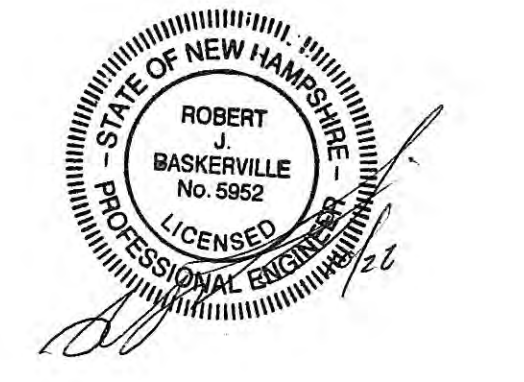
OWNER: LAND BARGE, LLC
754-756 ELM STREET
MILFORD, NH 03055

APPLICANT: SOONER TRANSPORTATION LLC
3 EXECUTIVE PARK DRIVE, SUITE 2016
BEDFORD, NH 03110

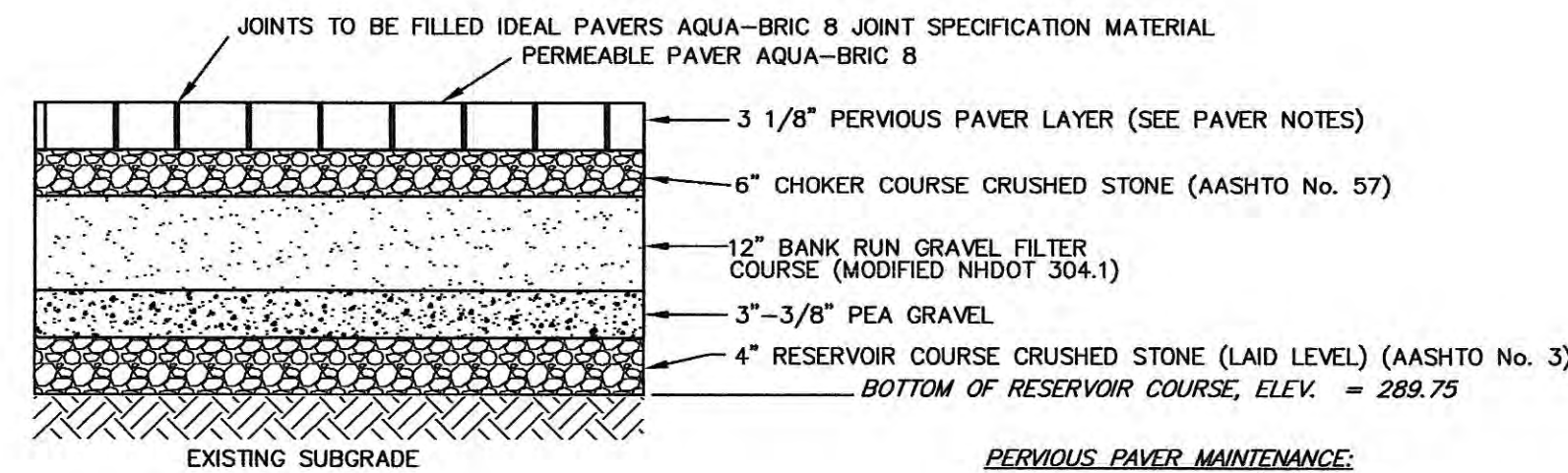
SCALE: 1" = 20' APRIL 25, 2022 SHEET 11 OF 12

DESIGN: KAW	DRAWN: KAW	CHECKED: RJB	FB: 580	PG: 60	1667-01
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PAVER NOTES:
PAVERS SHALL BE PAVERS BY IDEAL AQUA-BRIC 8 FOR HEAVY LOADS IN 'QUARRY BLEND'. THE PATTERN MAY BE 'HERRINGBONE OR RUNNING BOND AT THE DISCRETION OF THE OWNER. PAVERS ARE TO BE INSTALLED PER MANUFACTURER'S INSTALLATION GUIDE.

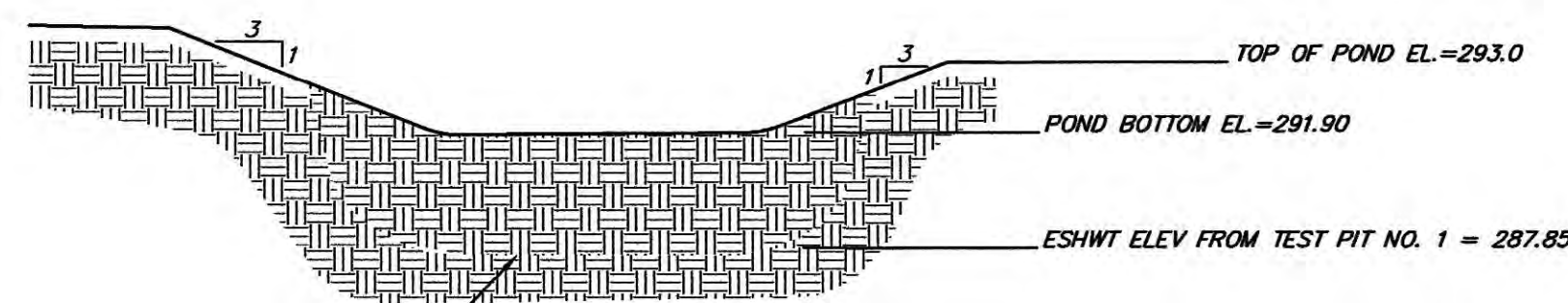
TYPICAL CROSS SECTION FOR PERVIOUS PAVERS FOR HIGH PERMEABILITY SUB SOILS
NOT TO SCALE

BEST MANAGEMENT PRACTICES FOR PERMEABLE PAVERS

1. NO WINTER SANDING OF PERMEABLE PAVEMENT IS PERMITTED.
2. MINIMIZE APPLICATIONS OF SALT FOR ICE CONTROL.
3. NEVER SEAL OR REPAVE PERMEABLE PAVERS.
4. IF A PAVER BECOMES DAMAGED IT CAN BE REPLACED AND THE NEW PAVER RESET.
5. INSPECT ANNUALLY FOR DETERIORATION OR SPALLING.
6. MONITOR PERIODICALLY TO ENSURE THAT PAVER SURFACE DRAINS EFFECTIVELY AFTER STORMS.
7. FOR INTERLOCKING PAVERS, PERIODICALLY ADD JOINT MATERIAL TO REPLACE LOST MATERIAL.
8. MAJOR CLOGGING MAY NECESSITATE REPLACEMENT OF PAVER SURFACE, AND POSSIBLY FILTER COURSE AND SUB-BASE MATERIAL.

PERVIOUS PAVEMENT MAINTENANCE:

- INSPECT ANNUALLY FOR DETERIORATION OR BREAKAGE
- MONITOR PERIODICALLY TO ENSURE THAT THE SURFACE DRAINS EFFECTIVELY AFTER STORMS
- CLEAN PERIODICALLY (2-4 TIMES PER YEAR) TO REMOVE SEDIMENT AND ORGANIC DEBRIS ON THE SURFACE.
- INSPECT ADJACENT VEGETATED AREAS (2-4 TIMES PER YEAR) FOR SIGNS OF EROSION AND RUN-ON TO THE POROUS PAVERS
- MAJOR CLOGGING MAY NECESSITATE REPLACEMENT OF THE SURFACE AND POSSIBLY FILTER COURSE AND SUB-BASE COURSE



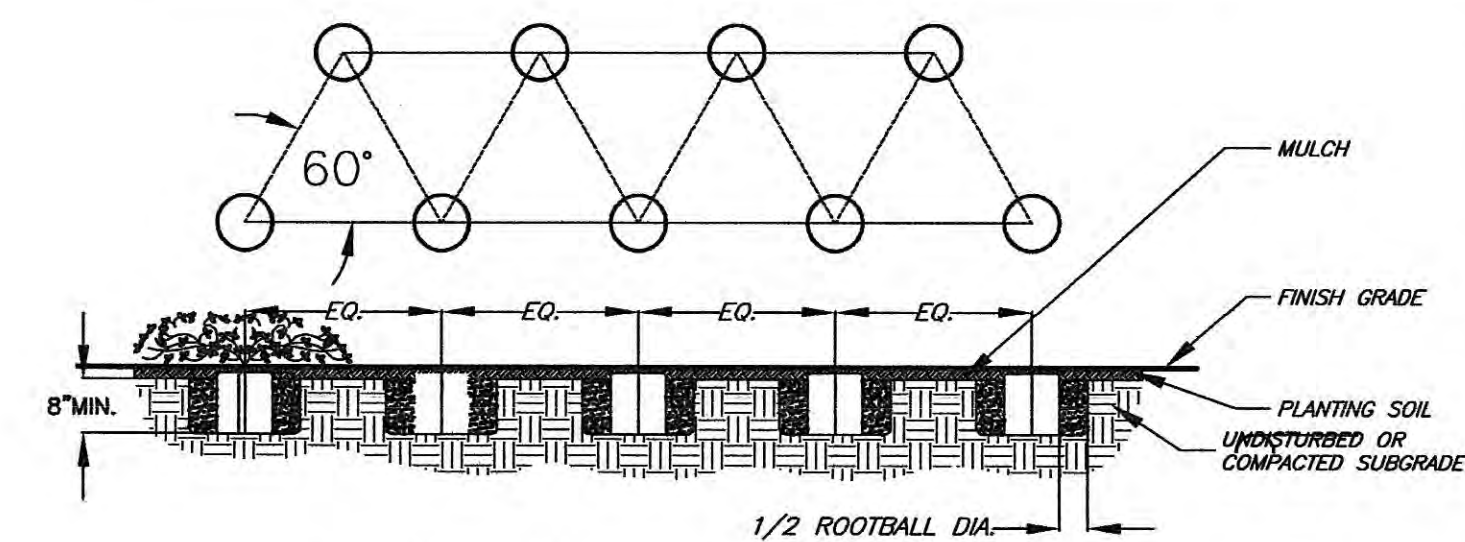
BASIN NOTES:

1. DURING CONSTRUCTION DO NOT RUN HEAVY MACHINERY OVER INFILTRATION SITES. COMPACTION WILL CAUSE THEM TO BECOME INEFFECTIVE. ONCE THE BOTTOM LEVEL OF BASIN IS EXCAVATED, TILL THE SOIL WITH A ROTARY TILLER TO RESTORE INFILTRATION RATES, FOLLOWED BY A PASS BY LEVELING DRAG.
3. BASINS SHALL NOT BE PLACED INTO SERVICE UNTIL THE PRACTICE HAS BEEN PLANTED AND ITS CONTRIBUTING AREAS HAVE BEEN FULLY STABILIZED.
4. SLOPES SHALL BE PROTECTED WITH JUTE MATTING.

INFILTRATION BASIN
NOT TO SCALE

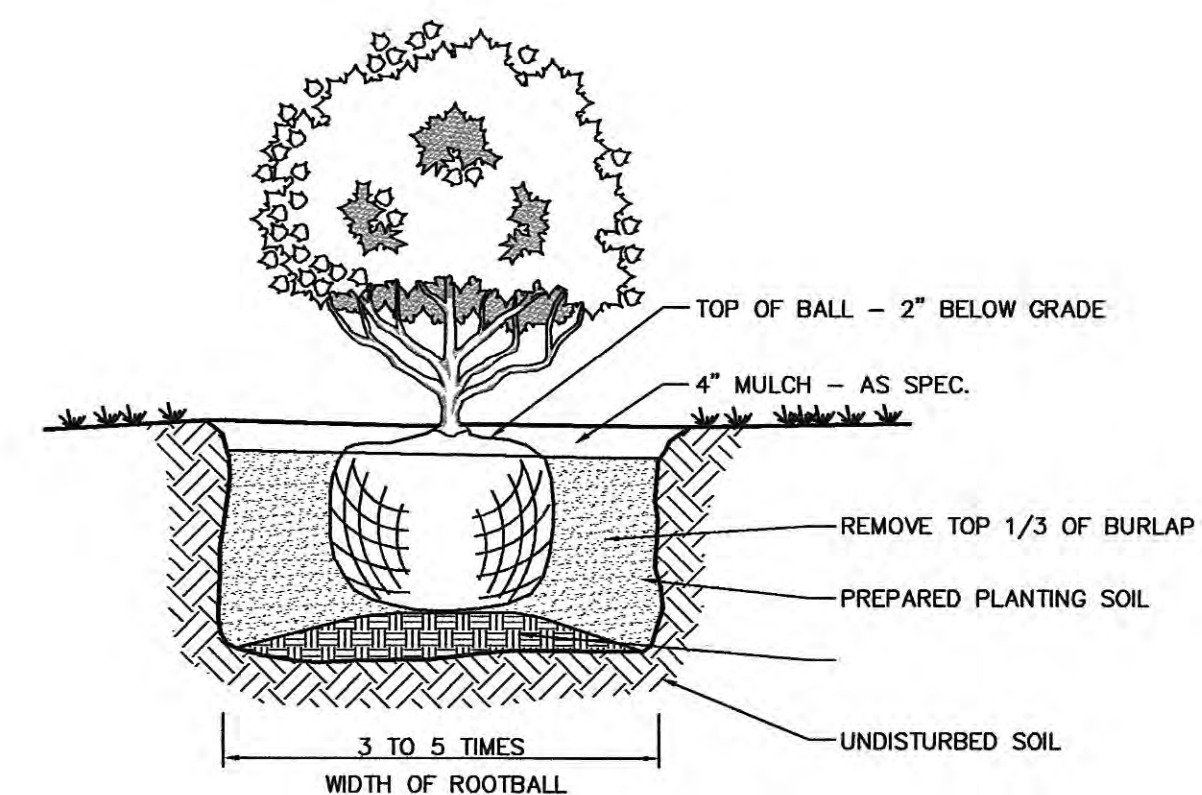
BEST MANAGEMENT PRACTICES FOR INFILTRATION BASINS

1. REMOVAL OF DEBRIS FROM INLET AND OUTLET STRUCTURES.
2. REMOVAL OF ACCUMULATED SEDIMENT.
3. INSPECTION AND REPAIR OF OUTLET STRUCTURES AND APPURTENANCES.
4. INSPECTION OF INFILTRATION COMPONENTS AT LEAST TWICE ANNUALLY, AND FOLLOWING ANY RAINFALL EVENT EXCEEDING 2.5 INCHES IN A 24 HOUR PERIOD, WITH MAINTENANCE OR REHABILITATION CONDUCTED AS WARRANTED BY SUCH INSPECTION.
5. INSPECTION OF PRETREATMENT MEASURES AT LEAST TWICE ANNUALLY, AND REMOVAL OF ACCUMULATED SEDIMENT AS WARRANTED BY INSPECTION, BUT NO LESS THAN TWICE ANNUALLY.
6. PERIODIC MOWING OF EMBANKMENTS.
7. REMOVAL OF WOODY VEGETATION FROM EMBANKMENTS.
8. INSPECTION AND REPAIR OF EMBANKMENTS AND SPILLWAYS.
9. IF INFILTRATION SYSTEM DOES NOT DRAIN WITHIN 72 HOURS FOLLOWING A RAINFALL EVENT, THEN A QUALIFIED PROFESSIONAL SHOULD ASSESS THE CONDITION OF THE FACILITY TO DETERMINE MEASURES REQUIRED TO RESTORE INFILTRATION FUNCTION, INCLUDING BUT NOT LIMITED TO REMOVAL OF ALL ACCUMULATED SEDIMENTS OR RECONSTRUCTION OF THE INFILTRATION TRENCH.

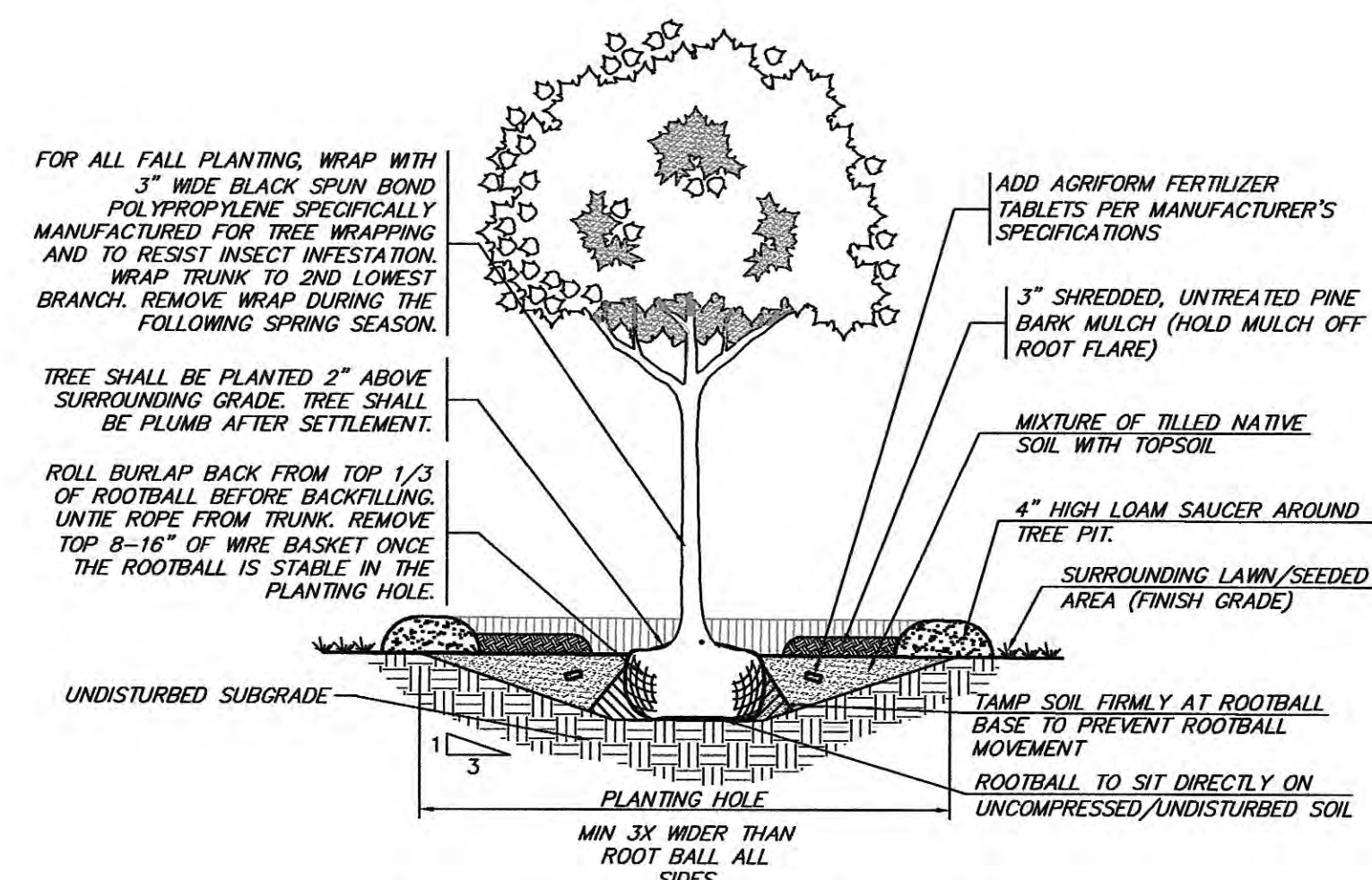


TYPICAL GROUNDCOVER/PERENNIAL PLANTING DETAIL
NOT TO SCALE

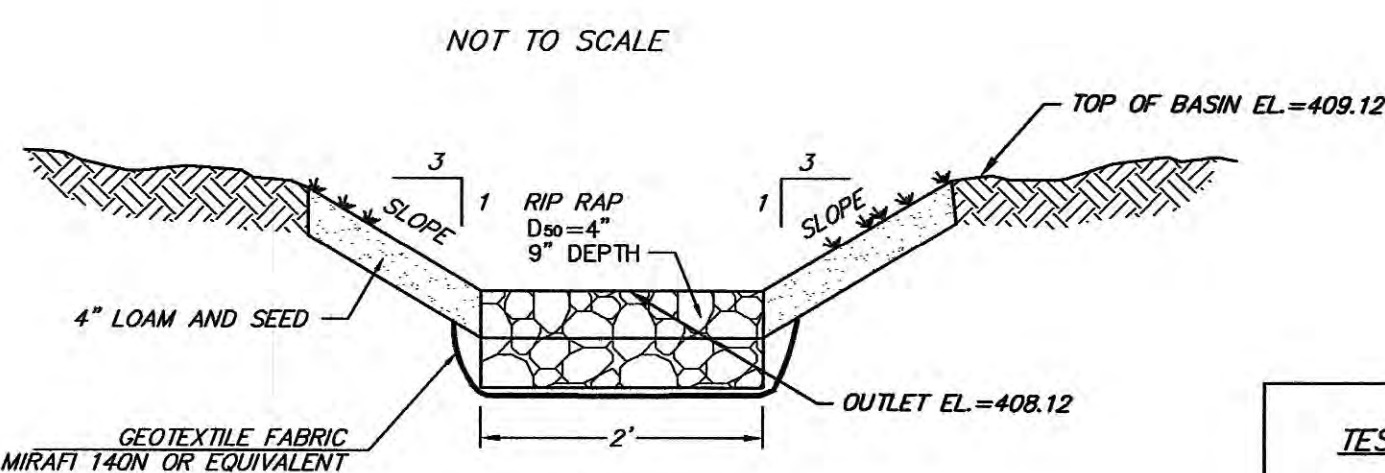
NOTE:
SPACING SHALL BE THE PLANT'S MATURE WIDTH ON CENTER



SHRUB PLANTING DETAIL
NOT TO SCALE



DECIDUOUS TREE PLANTING
NOT TO SCALE



NOTES:
- WIDTH OF SPILLWAY AND/OR WIDTH OF RIPRAP AS SPECIFIED ON THE PLANS.
- REFER TO THE MAINTENANCE AND CONSTRUCTION NOTES FOR ROCK RIP-RAP FOR ADDITIONAL DETAILS.

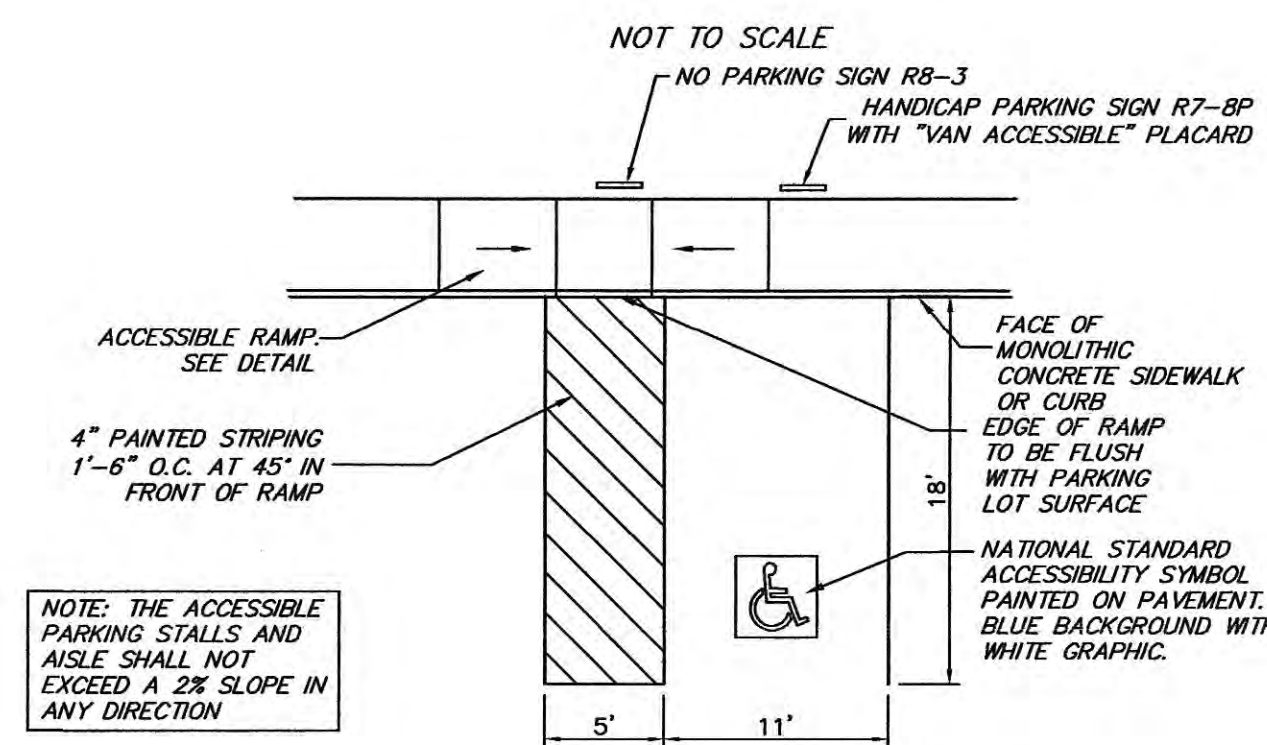
MAINTENANCE:

ROCK RIPRAP SHOULD BE CHECKED AT LEAST ANNUALLY AND AFTER EVERY MAJOR STORM. IF THE RIPRAP HAS BEEN DISPLACED, UNDERMINED OR DAMAGED, IT SHOULD BE REPAIRED IMMEDIATELY BEFORE FURTHER DAMAGE CAN TAKE PLACE. WOODY VEGETATION SHOULD BE REMOVED FROM THE ROCK RIPRAP ANNUALLY BECAUSE TREE ROOTS WILL EVENTUALLY DISLODGE THE ROCK RIPRAP. IF THE RIPRAP IS ON A CHANNEL BANK, THE STREAM SHOULD BE KEPT CLEAR OF OBSTRUCTIONS SUCH AS FALLEN TREES, DEBRIS, AND SEDIMENT BARS THAT MAY CHANGE FLOW PATTERNS WHICH COULD DAMAGE OR DISPLACE THE RIPRAP. REPAIRS MUST BE CARRIED OUT IMMEDIATELY TO AVOID ADDITIONAL DAMAGE TO THE RIPRAP.

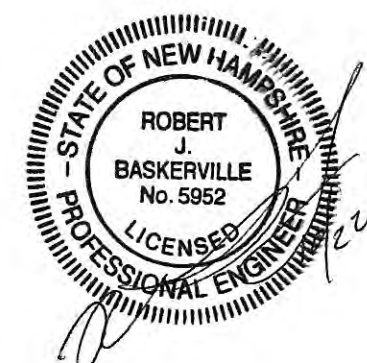
CONSTRUCTION SPECIFICATIONS:

1. THE SUBGRADE FOR THE FILTER MATERIAL, GEOTEXTILE FABRIC OR RIPRAP SHALL BE CLEARED AND GRUBBED TO REMOVE ALL ROOTS, VEGETATION, AND DEBRIS AND PREPARED TO THE LINES AND GRADES SHOWN ON THE PLANS.
2. THE ROCK AND/OR GRAVEL USED FOR FILTER AND RIPRAP SHALL CONFORM TO THE SPECIFIED GRADATION.
3. GEOTEXTILE FABRICS SHALL BE PROTECTED FROM PUNCTURE OR TEARING DURING THE PLACEMENT OF THE ROCK RIPRAP BY PLACING A CUSHION OF SAND AND GRAVEL OVER THE FABRIC. DAMAGED AREAS IN THE FABRIC SHALL BE REPAIRED BY PLACING A PIECE OF FABRIC OVER THE DAMAGED AREA OR BY COMPLETE REPLACEMENT OF THE FABRIC. ALL OVERLAPS REQUIRED FOR REPAIRS OR JOINING TWO PIECES OF FABRIC SHALL BE A MINIMUM OF 12 INCHES.
4. STONE FOR THE RIPRAP MAY BE PLACED BY EQUIPMENT AND SHALL BE CONSTRUCTED TO THE FULL LAYER THICKNESS IN ONE OPERATION AND IN SUCH A MANNER AS TO PREVENT PREVENT DISPLACEMENT OF THE UNDERLYING MATERIALS. HAND PLACEMENT MAY BE REQUIRED TO PREVENT DAMAGE TO ANY PERMANENT STRUCTURES.
5. STONES FOR RIPRAP SHALL BE ANGULAR OR SUBANGULAR. THE STONES SHOULD BE SHAPED SO THAT THE LEAST DIMENSION OF THE STONE FRAGMENT SHALL BE NOT LESS THAN ONE-THIRD OF THE GREATEST DIMENSION OF THE FRAGMENT. FLAT ROCKS SHALL NOT BE USED FOR RIPRAP.
6. VOIDS IN THE ROCK RIPRAP SHOULD BE FILLED WITH SPALLS AND SMALLER ROCKS.

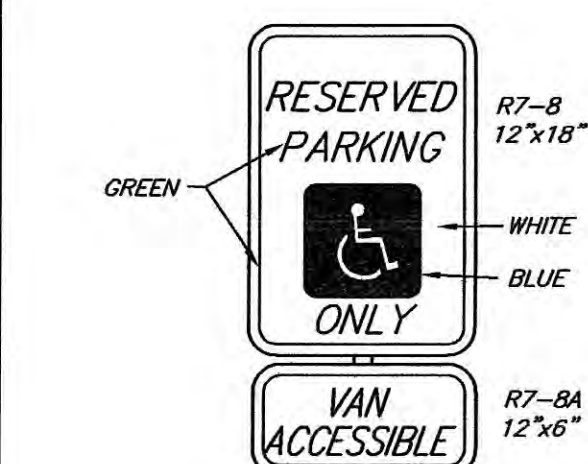
OUTLET SPILLWAY DETAIL
NOT TO SCALE



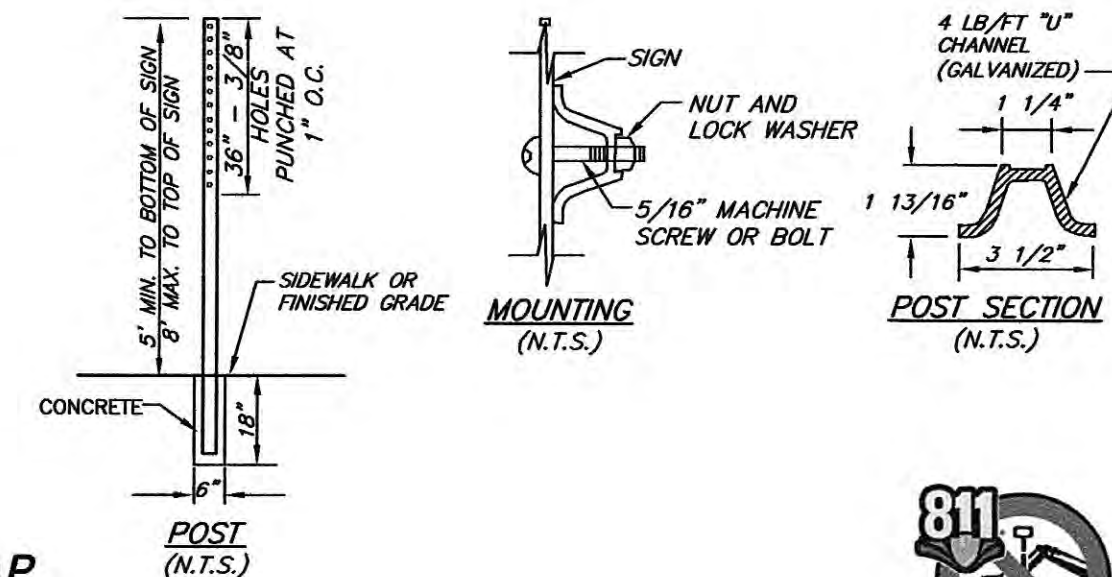
ACCESSIBLE PARKING SPACES
NOT TO SCALE



STRIPED PARKING AREAS AND DRIVES AS SHOWN INCLUDE PARKING SPACES, HANDICAP SYMBOLS AND PAINTED ISLES. ALL MARKINGS TO BE CONSTRUCTED USING WHITE TRAFFIC PAINT, MEETING THE REQUIREMENTS OF AASHTO M248 TYPE F.



VAN ACCESSIBLE HANDICAP PARKING SIGN DETAIL
NOT TO SCALE



DATE	DESCRIPTION	BY	REV.
10-7-22	REVISED PER PLANNING BOARD, STAFF, & DOT COMMENTS	KAW	B
8-9-22	ADDED SIGHT DISTANCE AND SEWER PROFILE SHEETS	KAW	A

TAX MAP 12 LOT 11

**DETAIL SHEET 4
SOONER-MILFORD**
LOCATED AT:
**754-756 ELM STREET
MILFORD, NEW HAMPSHIRE, 03055**
OWNER: LAND BARGE, LLC
754-756 ELM STREET
MILFORD, NH 03055
APPLICANT: SOONER TRANSPORTATION LLC
3 EXECUTIVE PARK DRIVE, SUITE 201G
BEDFORD, NH 03110

SCALE: 1" = 20' APRIL 25, 2022 SHEET 12 OF 12

DESIGN:	DRAWN:	CHECKED:	FB:	PG:	
KAW	KAW	RJB	580	60	1667-01

Bedford Design Consultants, Inc.
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Bedford Design Consultants Inc.
ENGINEERS AND SURVEYORS

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October 7, 2022

Camille Pattison, NRPC Planning Consultant
Town of Milford
1 Union Square
Milford, NH 03055

RE: Comment Replies – Sooner Transportation, LLC
754-756 Elm Street (Horseless Carriage Site)
Milford, NH
Tax Map 12 Lot 11

Ms. Pattison,

Bedford Design has reviewed comments from the Town of Milford dated August 10, 2022 and KV Partners LLC dated August 11, 2022 for the Major Site Plan Application. Below are our responses to each item and attached are revised plans with the requested edits.

We received feedback from NHDOT and have made edits to the plans along the ROW line per their request. Updated drainage plans and calculations are also included. We have also included additional waivers for accessible parking space length, parking within 10' of the Right of Way/Property Line, and the parking lot and building location in the West Elm St Gateway Overlay District.

We also attended a site walk on September 6th with the Board, and presented at a Planning Board Meeting on September 20th to discuss the building and parking location

If you have any questions, please feel free to contact us at 603-622-5533.

Sincerely,
Bedford Design Consultants, Inc.



Katherine A. Weiss, PLA, ASLA
Project Manager

TOWN OF MILFORD COMMENTS

Fire Department:

1. The total square footage of the building is actually 10,320 as proposed.

Reply: Yes, the first and second floor combined is 10,320 SF. The building footprint is 7,200 SF.

2. The Life Safety Code occupancy classification is a Special-Purpose Industrial Occupancy and would need to comply with Chapter 40 of the LSC.

Reply: This has been noted.

3. The Building Code occupancy classification is an S-1 storage occupancy.

Reply: This has been noted.

4. An automatic fire detection and notification (fire alarm) system is required.

Reply: Please see Note 17 on the Cover Sheet.

5. An automatic sprinkler system is required for this building. I did note a proposed 6" fire service on the grading and utility plan, so I'm assuming that they're planning on providing one.

Reply: Please see Note 17 on the Cover Sheet.

6. Milford Fire Department requires a fire hydrant within 100' of fire department connections, which will require a private hydrant to be installed as part of the project.

Reply: We have included a proposed hydrant within 100' of the fire department building connection.

7. The engineer must design the fire department access to accommodate our ladder truck weight, approach/departure angles, and turning radius. Specifications available from MFD upon request.

Reply: We have used a truck turning template for an 8.3' wide ladder truck that is 40' long.

8. Fire lane markings will be required on the fire department access.

Reply: A fire lane has been added to the travel lane along the main access.

Public Works Department:

1. Project will require a NH DOT and Driveway Permit.

Reply: This has been noted.

Water/Sewer Utilities Department:

1. The service needs to be from the main in the street, not stubbed off the fire line. The domestic should be 1 inch CTS running 18 inches from the 6-inch fire line. Please revise plan accordingly.

Reply: We have added an additional line from the street for the domestic water. The line is shown 18" from the fire line.

2. The Sewer once it reaches the Town right of way must turn from 4 inch to 6-inch SDR 35. Please revise plan accordingly.

Reply: We have revised the plan to use the existing line. This line is a 6-inch asbestos/cement pipe. We show a 4-inch line to the property line. We have added a cleanout at the connection.

3. The Water line out in the road is a 12 inch, not 8 inches. Please revise plan accordingly.

Reply: This has been updated on the Grading & Utilities Plan sheet.

Community Development / Planning Department:

1. A discrepancy appears on the plan regarding the lot size. This needs to be addressed and made consistent.

Reply: We have revised the Cover Sheet to match the Existing Conditions Plan.

2. Ensure that all plantings do not impact sight distance for access/egress along Wilton Road.

Reply: Please see the Sight Distance Plan. There are no planting obstructions in the way.

3. The current configuration of the new facility will allow the property owner to maintain current operations during construction, however is not meeting the spirit of the West Elm Street Gateway Overlay District, which aims to orient buildings with the roadway and towards the front of the lot, with parking shielded in the rear and to the side of the property.

Reply: This has been noted. We will continue to discuss this with the Planning Board.

4. The property falls within the Groundwater Protection District, with most of the lot falling within the Level 2 Area and a small portion to the Northwest lies within the Level 1 Area. The parcel must comply with the performance standards of Section 6.01.2 and the Shoreland Protection Zone.

Reply: We will be applying for a Shoreland Permit and have made sure that all water infiltration is 4' from the water table.

5. No additional traffic related information has been submitted as part of this site plan, and anticipated versus existing peak traffic volumes should be reviewed.

Reply: We have included a trip generation analysis with before and after estimated traffic counts.

6. Add a note to the plan indicating that in the event the proposed snow removal areas are not sufficient, snow will be relocated off-site.

Reply: Please see Note 16 on the Site Plan sheet.

7. Please amend the note saying demolition will be completed under the rules of the town of Manchester.

Reply: Note 25 on the Demolition sheet has been updated to state "Milford".

8. Sheet 1, Note 8 – Please update the site plan to include the existing and resulting open space calculations in the *Dimensional Regulations: Commercial* table.

Reply: The calculations have been updated using the correct lot area.

9. Please revise site plan to include all proposed signage for the project.

Reply: We have included the monument sign location on the Site Plan and a wall sign location on the Elevation sheets. We do not have a specific design for the sign at this time.

10. Sheets 1 and 10 – The site plan shows the proposed location of the dumpster and pad encircled by an 8' stockade fence within the 15 foot westerly side dimensional setback. The proposed 8' stockade fence and concrete pad are defined as structures in the Town's Zoning Ordinance and such, would require a Special Exception by the Board of Adjustment for relief of this requirement. The alternative would be amend the plan and relocate the dumpster outside of the dimensional setback.

Reply: The building has been moved to relocate the dumpster outside the setback.

11. Sheet 1 – The plan displays a 12' wide loading area within the state owned right of way. The proposal may effect and impact the traffic conditions on Elm Street and abutting properties. Further as stated in Section 6.05.6.E, all off-street parking and loading or unloading spaces shall be at least ten feet (10') from any public street right-of-way and shall be separated from such right-of-way by a granite curb and landscaping as required in 6.08 LANDSCAPING STANDARDS. The proposal require a waiver of these requirements and require NHDOT approval and dedicated easement to use the area within the right of way.

Reply: We have removed the loading area per the request of the NHDOT and extended the existing island to the property line. We also have moved the display area to exclusively within the lot. The new loading area is in front of the building. We have included a turning radius sheet with this submittal.

Staff suggests further discussion with the applicant to determine if the proposed loading area could be designed to have a dual purpose for a loading area and bus stop (Subject to NHDOT approval).

Reply: The NHDOT had no issues with a bus stop in this area if the Board is still amendable. The stop would now be located within the travelled way and not behind the curbed island. We have run a

turning lane diagram and a trailer can make it out of the site through the adjacent property. Please see the attached turning radius plan.

12. Sheet 1 – The plan shows display parking space partially located off the property and within the State right-of way. As stated in the Zoning Ordinance, Section 6.05.6.E, Parking and loading spaces shall be arranged so that cars will not back into public roads. The plan will need to be amended to relocate said parking spaces within the subject property at least 10 feet from the Elm Street public right-of-way.

Reply: The display spaces have been moved onto the property per the DOT's request. We would argue that the display spaces are not parking spaces but for the display of goods which happen to be cars. Any vehicle movements will be done by employees. Only five display spaces, located along the front property line, can use the Right of Way to maneuver.

13. Sheet 1 - The applicant should explain the internal circulation, access/egress patterns for the 46+ display vehicles. At a minimum, the plan should be revised to include the appropriate aisle widths between the rows of display vehicle parking.

Reply: We have removed a row of display spaces to create a 14' aisle from the main access drive. This will allow for additional maneuvering now that the island has been expanded in the ROW.

14. Sheet 1 – Staff questions if the adequate snow storage areas have been provided. A note should be added to the plan stating that snow shall be removed to an offsite location using best management practices.

Reply: Please see Note 16 on the Cover Sheet.

15. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.1.a, a new structure shall maintain an appropriate street edge in relationship to adjacent structures. The location of the proposed building is set within the rear section of the property, which conflicts with buildings located to the east and west.

Reply: We have discussed this with the Planning Board and believe the nature of the construction timeline for the building and site plan make the building location appropriate.

16. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.2.b, the proposed building should enhance the definition of each floor of the building through terracing, material changes, articulated structural elements, changes in materials and horizontal trim bands. The applicant should explain how the proposed building complies with this section.

Reply: We are in the process of making changes to the architectural plans. Changes include awnings over doors, a stone veneer along the bottom of the building, enhancing the windows and door trim, and clapboard style metal siding.

17. Sheets 1 and 6 – Please explain how the proposed building architectural features and materials comply with the Zoning Ordinance, Section 6.07.6.E.3.

Reply: Awnings will be added to the doorways and a stone veneer to the bottom section of the building.

18. Sheets 1 and 6 – Pursuant to the Zoning Ordinance, Section 6.07.6.E.4.a, the proposed building design should avoid blank walls at ground-floor levels through the use of windows, trellises, material changes, arcades, or other features to increase visual interest. The applicant should be prepared to discuss how the proposed building complies with the section.

Reply: The ground floor level has two doors and windows which we are enhancing with thicker trim.

19. Sheets 1 and 6 - Pursuant to the Zoning Ordinance, Section 6.07.6.E.5.a, parking lots should be located to the rear or side of a building whenever possible. The proposed parking area is located along the front the property. The applicant should be prepared to discuss the alternatives considered.

Reply: We have discussed this with the Planning Board and plan to continue the discussion at subsequent meetings. The display spaces must be out front. This is essential to be used car business on-site. The parking spaces for customers and employees are located 60-feet from the ROW.

KV PARTNERS LLC COMMENTS

1. Test pit information states the “redesign is required” due to the depth to ground water below an infiltration basin in a “groundwater” district. Please clarify the specifics of the proposed basin design.

Reply: We have included our updated drainage calculations and design with this submittal.

2. Provide backup calculations for the 0.15 CFS exfiltration rate used for both infiltration areas (pavers and pond). Note that the same exfiltration rate number was used for two different sized areas.

Reply: We have updated our infiltration rate to use inches per hour from the Hillsborough County Soils Book. The sheet with the new infiltration rate is included in the drainage report. Any exfiltration rate will remain the same across the site because it’s the same soil. The square foot area where the rate is applied to will differ for ponds P1 and P2.

3. Verify that the pavers have adequate permeability in the joints to pass stormwater through the pavers to the aggregates below at a rate consistent with the exfiltration rate used for the subsurface infiltration system (aggregates). Also, the calculations indicate that there will be complete infiltration of the design storm for the watershed flowing to the pavers, however, the pavers are sloping towards the abutting property and there appears to be no high point on the pavement to contain flows to the paver area. Add a small high point in the paving to ensure all flow will be infiltrated into the pavers/subbase without any bypass.

Reply: There is a low point in the pavers and water runs into the area from the north and south. Please see the directional arrows on the Grading & Utilities Plan. We have included the paver information with this submittal in the drainage report. The pavers will infiltrate up to 10” per hour.

4. The post development total watershed area is slightly less than the pre-development area. Please clarify.

Reply: We have reviewed and updated the pre and post development areas.

END COMMENTS

Bedford Design Consultants Inc.

ENGINEERS AND SURVEYORS

592 Harvey Road Manchester, NH 03103
Telephone: (603) 622-5533 • www.bedforddesign.com

August 9, 2022

Brian A. Desfosses, P.E.
Assistant District Engineer
NHDOT – District 5
471 E. Point Drive
Bedford, NH 03110

**RE: Sooner Milford Site Plan –Pre & Post Traffic Counts
754-756 Elm St, Route 101 Milford, NH
Traffic Letter**

Mr. Desfosses,

Bedford Design has supplied this traffic letter to show the estimated pre and post volumes for the Sooner Milford Site Plan. For this letter we have chosen land use codes 841 Automobile Sales (Used) and 942 Automobile Care Center. Attached is a table using the 10th Edition's land use codes for the pre and post Square Footage of the buildings.

The existing use will remain the same. We chose to use the Trip Rate/1000 GFA for our estimate for the Automobile Care Center. However, we could also use the number of bays and number of employees to create an estimate. Both options have a much lower rate than the GFA calculations. The number of bays decreases from 8 bays to 7 bays in the proposed building, and the number of employees would be 9 for the new building.

The Land Use Code for Auto Care Center does not include total weekday trips. The majority of the traffic will be from the Auto Care Center. The morning peak hour currently has an estimate of 12 trips and the proposed building has an estimate of 30 trips. The evening peak hour currently has an estimate of 17 trips and the proposed building has an estimate of 33 trips. Both scenarios show an increase in trips. The am peak hour has an increase of 18 trips and the evening peak hour has an increase of 16 trips.

We have attached an aerial view of the existing intersection, as well as the ITE sheets that were used to determine the truck trips. If you have any questions, please don't hesitate to call us at 603-622-5533.

Sincerely,

Bedford Design Consultants, Inc.

Robert J. Baskerville, PE
President

PRE-TRAFFIC ESTIMATION
4,970 s.f. Automobile Care Center (Land Use 942)

	<u>Rate/1,000 GFA</u>	<u>Total Trips</u>		<u>Entering Site</u>		<u>Exiting Site</u>
<u>Weekday</u>	N/A					
<u>AM Peak Hour</u>	2.25	11.18	66%	7.38	34%	3.80
<u>PM Peak Hour</u>	3.11	15.50	48%	7.44	52%	8.06

225 SF Automobile Sales - Used (Land Use 841)

	<u>Rate/1000 GFA</u>	<u>Total Trips</u>		<u>Entering Site</u>		<u>Exiting Site</u>
<u>Weekday</u>	27.06	6.09	50%	3	50%	3
<u>AM Peak Hour</u>	2.13	0.48	76%	0.36	24%	0.12
<u>PM Peak Hour</u>	3.75	0.84	47%	0.39	53%	0.45

POST TRAFFIC ESTIMATION
10,040 SF Automobile Care Center (Land Use 942)

	<u>Rate/1000 GFA</u>	<u>Total Trips</u>		<u>Entering Site</u>		<u>Exiting Site</u>
<u>Weekday</u>	N/A					
<u>AM Peak Hour</u>	2.25	22.59	66%	14.91	34%	7.68
<u>PM Peak Hour</u>	3.11	31.22	48%	14.99	52%	16.23

280 SF Automobile Sales - Used (Land Use 841)

	<u>Rate/1000 GFA</u>	<u>Total Trips</u>		<u>Entering Site</u>		<u>Exiting Site</u>
<u>Weekday</u>	27.06	7.58	50%	3.79	50%	3.79
<u>AM Peak Hour</u>	2.13	0.60	76%	0.46	24%	0.14
<u>PM Peak Hour</u>	3.75	1.05	47%	0.49	53%	0.56



The current site is mostly paved and the existing building is close to the street.